



HISTORIC DISTRICT COMMISSION APPLICATION FOR WORK APPROVAL

City of Detroit - Planning & Development Department
2 Woodward Avenue, Suite 808
Detroit, Michigan 48226

APPLICATION ID

HDC2025-00024

PROPERTY INFORMATION

ADDRESS(ES): 14528 Ashton Rd.

HISTORIC DISTRICT: Rosedale Park

SCOPE OF WORK: (Check ALL that apply)

- | | | | | | |
|--|---|--|---|---|--------------------------------|
| <input type="checkbox"/> Windows/ Doors | <input type="checkbox"/> Walls/ Siding | <input type="checkbox"/> Painting | <input type="checkbox"/> Roof/Gutters/ Chimney | <input checked="" type="checkbox"/> Porch/Deck/Balcony | <input type="checkbox"/> Other |
| <input type="checkbox"/> Demolition | <input type="checkbox"/> Signage | <input type="checkbox"/> New Building | <input type="checkbox"/> Addition | <input type="checkbox"/> Site Improvements (landscape, trees, fences, patios, etc.) | |

BRIEF PROJECT DESCRIPTION:

Renovation of front porch including: Replace existing wood decking in-kind; replace existing wood railing with metal railing, replace existing wood porch stair in-kind. Reconstruct concrete walkway at the front of the house. Reconstruct concrete driveway between sidewalk and front of house.

APPLICANT IDENTIFICATION

TYPE OF APPLICANT: Architect/Engineer/Consultant

NAME: Joshua R. Maddox

COMPANY NAME: 4545 Architecture

ADDRESS: 2761 E. Jefferson Ave. Suite 302

CITY: Detroit

STATE: MI

ZIP: 48207

PHONE: +1 (313) 670-6377

EMAIL: Josh.maddox@4545architecture.com

I AGREE TO AND AFFIRM THE FOLLOWING:

- I understand that the failure to upload all required documentation may result in extended review times for my project and/or a denied application.
- I understand that the review of this application by the Historic District Commission does not waive my responsibility to comply with any other applicable ordinances including obtaining appropriate permits (building, sign, etc.) or other department approvals prior to beginning the work.
- I hereby certify that the information on this application is true and correct. I certify that the proposed work is authorized by the owner of record and I have been authorized to make this application as the property owner(s) authorized agent.

DocuSigned by:

Joshua R. Maddox

01/21/2025

SIGNATURE

DATE

NOTE: Based on the scope of work, additional documentation may be required. See www.detroitmi.gov/hdc for scope-specific requirements.

PROJECT DETAILS – TELL US ABOUT YOUR PROJECT

Instructions: Add project details using the text box in each section. If your details exceed the space provided, attach the details via the attachment icon for that section.

ePLANS PERMIT NUMBER:

(only applicable if you've already applied for permits through ePLANS)

N/A

GENERAL

1. DESCRIPTION OF EXISTING CONDITION

Please tell us about the current appearance and conditions of the areas you want to change. You may use a few sentences or attach a separate prepared document on the right. (For example, "existing roof on my garage is covered in gray asphalt shingles in poor condition.")



2. PHOTOGRAPHS

Help us understand your project. Please attach photographs of all areas where work is proposed.



3. DESCRIPTION OF PROJECT

In this box, tell us about what you want to do at the areas described above in box #1. (For example, "Install new asphalt shingle roofing at garage.")



4. DETAILED SCOPE OF WORK

In this box, please describe all steps necessary to complete the work described in box #3. (For example, "remove existing shingles, replace wood deck as necessary, replace wood eaves, install roof vents, replace rotted fascia boards, paint, clean worksite.")



5. BROCHURES/CUT SHEETS

Please provide information on the products or materials you are proposing to install. For example, a brochure on the brand and color of the shingles proposed.



ADDITIONAL DETAILS

| | |
|--|--|
| | |
| | |
| | |
| | |



January 21, 2025

14528 Ashton Rd.,
Detroit, MI 48223

RE: 14528 Ashton Rd. Porch Renovations, District Historic Commission Submission

Project Narrative

14528 Ashton Rd. is an existing two-story home in Detroit's Rosedale Park Historic Neighborhood. Constructed in 1926, the home is wood framed with lap siding. The home is square in footprint and features a hipped roof with a front-facing dormer. From the street, a major feature of the home is its front porch spanning across the full width of the house. The porch consists of three brick piers supporting robust square columns and a low-slopped hip roof. The porch surface is currently wood decking, and railings spanning between the brick piers and the porch stairs are also wood. None of the wood elements are finished with any noticeable paint or stain and are in a very worn and deteriorating condition. The owner of the home has contracted CHN Housing solutions to perform accessibility upgrades to the home to suit her needs. These items include reconstruction of the wood stairs, wood decking, installation of new railings, and reconstruction of the existing concrete pedestrian walkway and driveway at the front of the property. No other work on the exterior of the home itself is being proposed as part of this project.

Proposed Work

The proposed work consists of demolition and reconstruction of all existing deteriorated elements of the porch and hardscape that are creating a dangerous condition for the owner.

Beginning with the porch itself, the wood decking, wood handrails, wood stairs, and wood siding on the North and South elevations of the porch will be removed. The decking will be replaced in-kind with 5/4 x 6" wood planks to create a consistent and level walking surface. The wood stairs will be replaced in-kind with new wood stringers, 5/4 wood treads, and wood risers of equal tread depth and riser height to meet code and accessibility requirements. The existing unfinished T1-11 siding at the north and south elevations of the porch will be removed, and new framing/blocking installed as required to support new James Hardie Lap Siding to enclose the area under the porch. This siding, along with the new wood stair and wood decking will be painted B:19, Black, according to Detroit HDC's Color System E Chart.

The existing wood railing will be removed and replaced with a new pre-finished aluminum railing in the same locations as the existing rail, i.e. between the brick piers along the West elevation of the porch, between the brick piers and house along the North and South elevations, and on both sides of the new wood stair. The railing will be



36" tall to meet current code, and will feature intermediate posts, top and bottom rails, and thin vertical balusters. All elements are generally square in profile. The specified manufacturer of the railing is Ultra Fencing-Railing, and the specific product is the Signature Series UAR-200-TR in black.

In addition to the porch work, the existing hardscape in the front of the property will be replaced. The current concrete walk between the porch and sidewalk features a low, awkwardly placed step-up near the sidewalk that poses a tripping hazard to the owner. The proposal is to remove the existing walk and replace it in-kind with a new concrete walk that slopes consistently from the sidewalk elevation up to the bottom of the porch stairs. Width and finish of the new walk will match the old.

The existing driveway along the North property line is currently a "two-track" design from the back of the sidewalk to the rear yard. The differences in surfaces and deteriorated state of the existing concrete pose tripping hazards to the owner. The proposal is to remove the "two-track" driveway between the back of the sidewalk and front (Northwest) corner of the house, and replace it with a full-width concrete driveway. The continuous surface will make it much safer for the owner to walk across when coming to and from their vehicle, and make it easier to maintain.

4545 architecture

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Existing Photos



West Elevation

2761 E. Jefferson, Suite 302, Detroit, MI 48207

e | tim.flintoff@4545architecture.com • c | 248.320.6098 • w | 4545architecture.com

4545 architecture



Google Street View from 2009 showing front porch with black stairs and railings, black wood decking, and black and white porch railings.

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North Elevation

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South Elevation

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Scope of Work - Front Porch Renovation

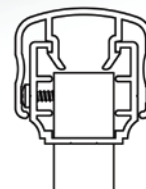
- Remove existing wood porch decking and perimeter wood railings
- Remove existing wood stair and railings
- Remove existing T1-11 siding on north and south elevations of porch

- Demolish existing concrete walkway
- Demolish existing “two-track” driveway between back of sidewalk and northwest corner of house

- Install new wood decking on porch
- Construct new wood stair
- Install new James Hardie lap siding at north and south elevations of porch
- Install new prefinished aluminum railings at perimeter of porch and each side of stair
 - Product: Ulta Fencing-Railing Signature Series UAR-200-TR, black

- Construct new concrete walkway
- Construct new concrete driveway between back of sidewalk and northwest corner of house.

| Components | Post-To-Post |
|--|--|
| Square Picket | 0.75" Sq. x .050" Thickness |
| Top Cap Adams | 1.700" H x 1.716" W 0.080" Thickness |
| Bottom Rail | 1.320" H x 1.352" W 0.070" Thickness |
| Boss Screw Posts | 2.5" x 2.5" x 0.100" 3.0" x 3.0" x 0.125" |
| Spacing Between Pickets | 3.625" |
| Nominal Panel Length* | 4', 5', 6', & 8' |
| Heights Available | 37' & 43" |
| Signature [®] Columns and Newel Posts | 6" Square 0.100" Thickness 6" Square 0.100" Thickness |



Adams Profile

Ultra Aluminum™ Warranty



Ultra Aluminum™ fencing products are warranted against defects in workmanship and/or materials. The Ultra powder coat finish is guaranteed against cracking, peeling or chipping.

Ultra Aluminum Alloys



Ultra's high strength, aluminum alloy is as strong as steel, but will never rust. Our 6005-T5 alloy has a minimum strength of 35,000 psi.

Environmentally Responsible



Ultra Aluminum™ is proud to use recycled aluminum in our products, and our powder coat coating process is environmentally friendly and virtually pollution-free.

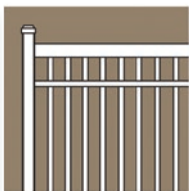
Textured Finishes



Our advanced powder coat finish meets AAMA 2604 allowing us to produce a high-quality, long-lasting finish.



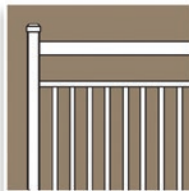
UAR-200-2R Two Rail



UAR-200-3R Three Rail



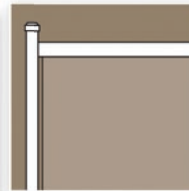
UAR-220-3R Three Rail



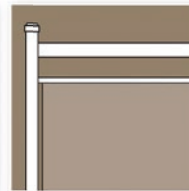
UAR-270-3R Three Rail
*6' Max Panel



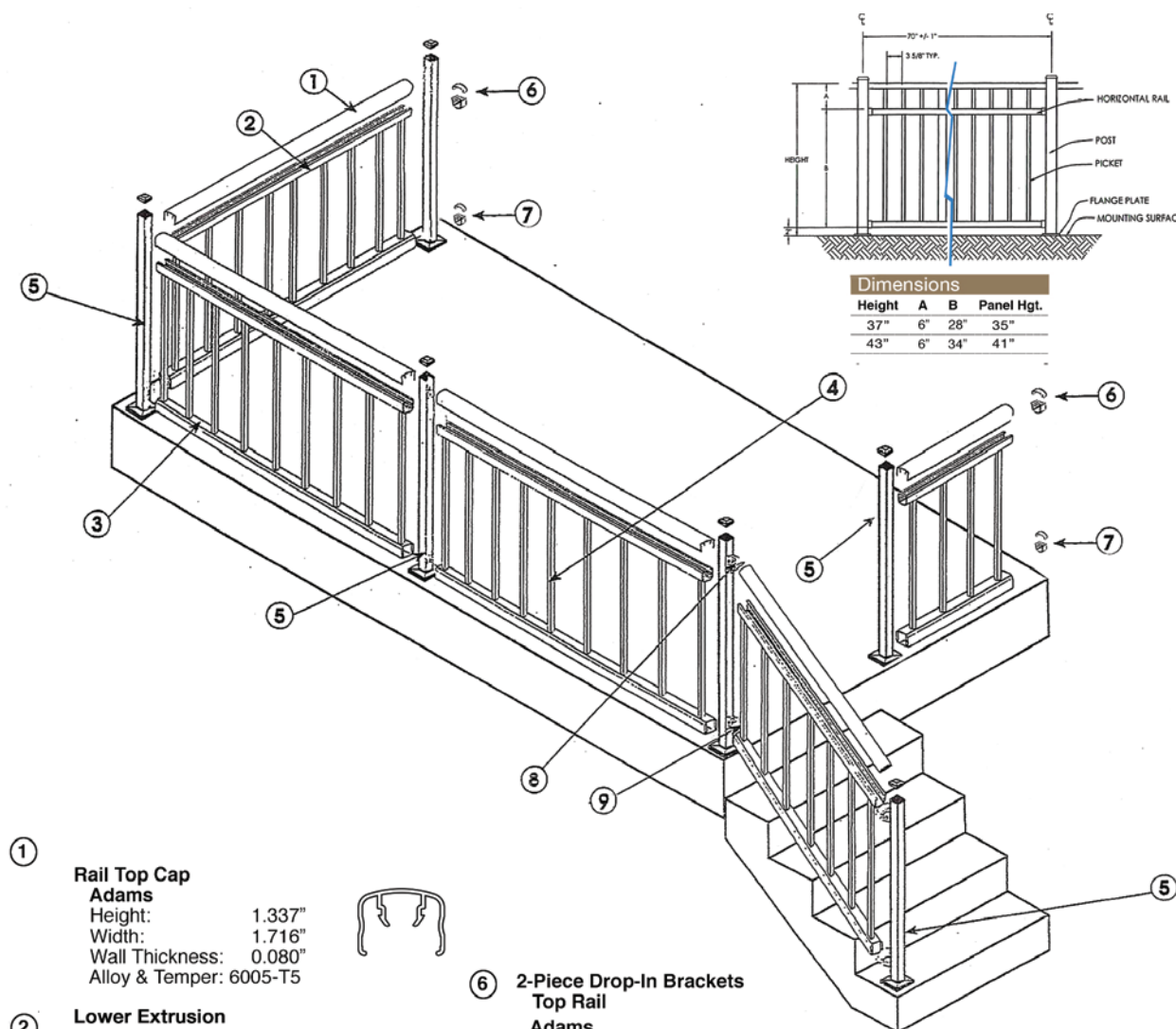
UAR-250-3RS Three Rail
with Spears



UAG-200-2R Glass Railing
*4' Max Panel

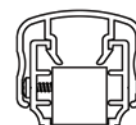


UAG-270-3R Glass Railing
*4' Max Panel

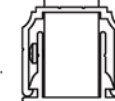


| Dimensions | | | |
|------------|----|-----|------------|
| Height | A | B | Panel Hgt. |
| 37" | 6" | 28" | 35" |
| 43" | 6" | 34" | 41" |

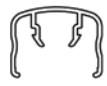
Adams Top-Rail Profile



Adams Bottom-Rail Profile



- ① **Rail Top Cap**
Adams
Height: 1.337"
Width: 1.716"
Wall Thickness: 0.080"
Alloy & Temper: 6005-T5
- ② **Lower Extrusion for Top Rail**
Height: 1.235"
Width: 1.352"
Wall Thickness: 0.070"
Alloy & Temper: 6005-T5
- ③ **Bottom Rail**
Height: 1.375"
Width: 1.375"
Wall Thickness: 0.070"
Alloy & Temper: 6005-T5
- ④ **Square Picket**
Wall Thickness: 0.050"
Alloy & Temper: 6005-T5
- ⑤ **Post**
2 1/2" Sq. x 2 1/2" Sq.
Wall Thickness: 0.100"
Alloy & Temper: 6005-T5
3" Sq. x 3" Sq.
Wall Thickness: 0.125"
Alloy & Temper: 6005-T5



- ⑥ **2-Piece Drop-In Brackets Top Rail**
Adams
Wall Thickness: 0.200"
Alloy & Temper: A-360
- ⑦ **2-Piece Drop-In Brackets Bottom Rail**
Adams
Wall Thickness: 0.200"
Alloy & Temper: A-360
- ⑧ **Adjustable Rail End Vertical or Horizontal Top Rail**
Wall Thickness: 0.200"
Alloy & Temper: A-360
- ⑨ **Adjustable Rail Ends-Vertical or Horizontal Bottom Rail**
Wall Thickness: 0.200"
Alloy & Temper: A-360

| Standard Ultra Aluminum Colors | | | | | | Premium* |
|--------------------------------|----------------|--------------|-----------------|----------------|-------------|-------------------|
| BL | TB | BR | TZ | TK | WH | SI |
| Gloss Black | Textured Black | Gloss Bronze | Textured Bronze | Textured Khaki | Gloss White | Silver (RAL-9006) |

*Colors shown represent an approximate comparison and may vary slightly from actual product color. *Premium Colors are available at an extra charge and may require additional lead times. Ask your Ultra Representative for details.

Visit our website for more information and a copy of the warranty.



ULTRARAILING.COM
800-656-4420



FRONT PORCH RECONSTRUCTION CHN HOUSING PARTNERS

14528 ASHTON RD.
DETROIT, MI 48223

ARCHITECT
4545 ARCHITECTURE | DESIGN
TIMOTHY FLINTOFF
2761 E. JEFFERSON, SUITE 302
DETROIT, MI 48207

PROJECT DATA

BUILDING CODE AUTHORITY: CITY OF DETROIT

APPLICABLE CODES:

BUILDING CODE
ALSO KNOWN AS THE "MICHIGAN RESIDENTIAL BUILDING CODE"
2015 MICHIGAN RESIDENTIAL CODE (MRC) AS AMENDED

BARRIER FREE REQUIREMENTS
AMERICANS WITH DISABILITIES ACT (ADA)
MBC-2015, CHAPTER 11
ICC / ANSI 117.1 - 2010, EXCEPT SECTION 611 & 707

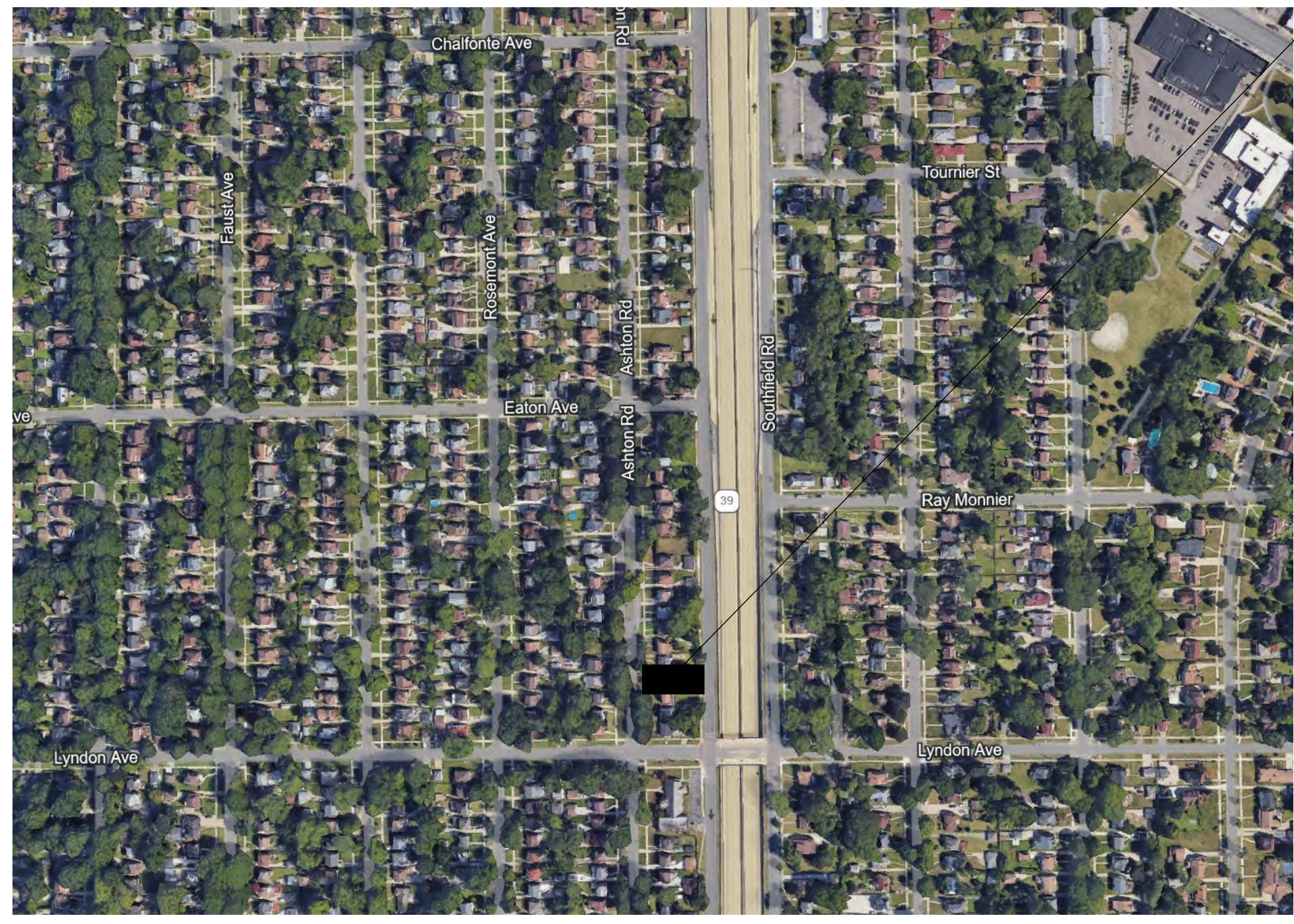
2009 MICHIGAN ACCESSIBILITY CODE

PROJECT DESCRIPTION

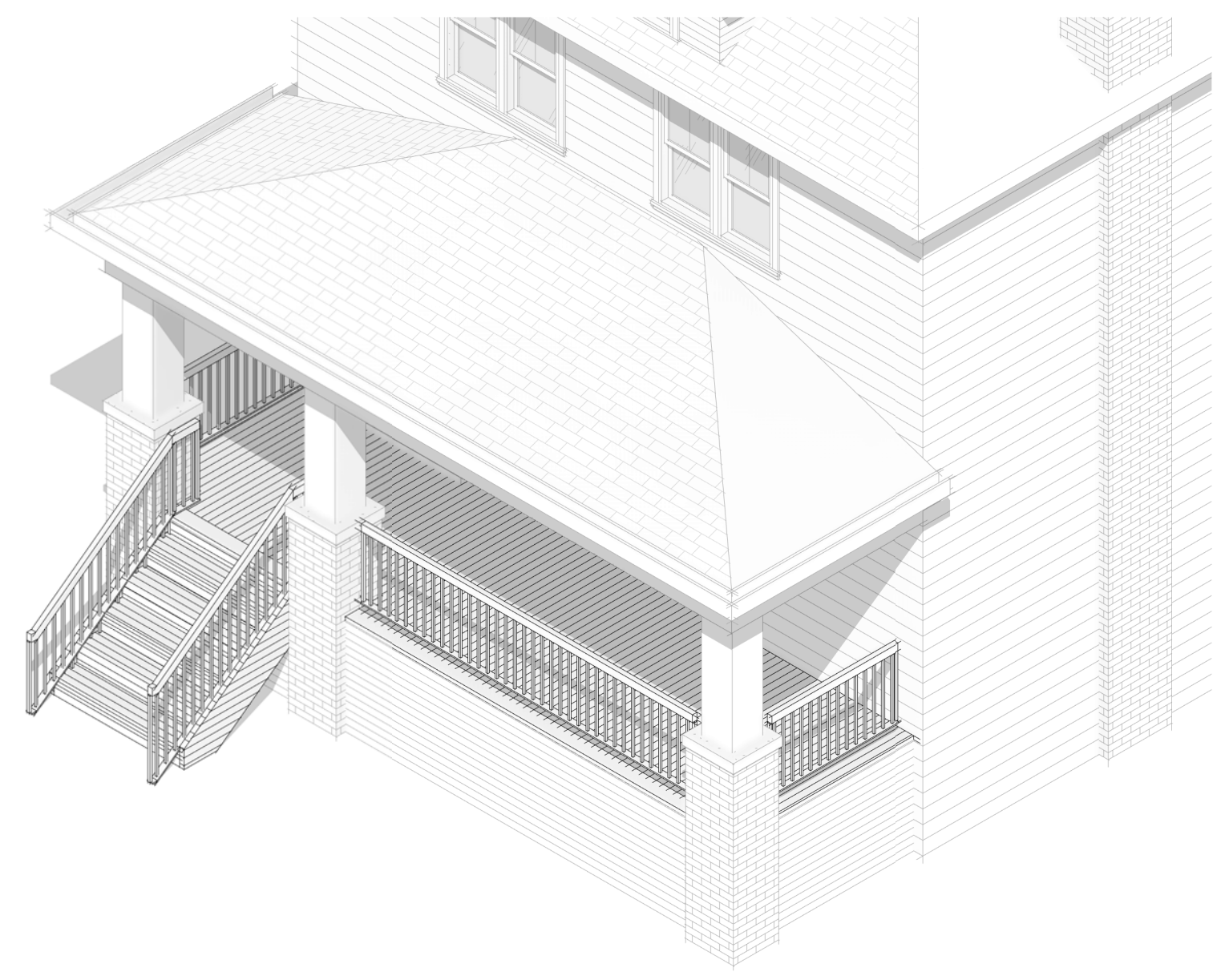
THE PROJECT CONSISTS OF DEMOLISHING EXISTING WOOD DECKING, WOOD STAIR, AND WOOD RAILINGS AT THE FRONT PORCH OF 14528 ASHTON RD. THE CURRENT PORCH ELEMENTS HAVE DETERIORATED TO A POINT THAT THEY ARE UNSAFE FOR THE RESIDENT WHO HAS ACCESSIBILITY REQUIREMENTS. NEW WOOD DECKING, A WOOD STAIR MEETING CURRENT RESIDENTIAL CODE AND ADA REQUIREMENTS, AND A NEW METAL RAILING WILL BE INSTALLED. IN ADDITION, THE EXISTING CONCRETE WALK FROM THE SIDEWALK TO THE PORCH STAIR WILL BE DEMOLISHED AND RE-CONSTRUCTED TO REMOVE A DANGEROUS STEP AT THE SIDEWALK. ALSO, THE EXISTING "2-TRACK" DRIVEWAY WILL BE DEMOLISHED BETWEEN THE SIDEWALK AND THE FRONT CORNER OF THE HOUSE AND WILL BE REPLACED WITH AN 8'-WIDE CONCRETE DRIVEWAY MATCHING THE WIDTH OF THE EXISTING CURB CUT.

SHEET INDEX

- A1.0 ARCHITECTURAL SITE PLAN AND DETAILS
- A1.1 PORCH DEMO AND NEW WORK PLANS
- A1.2 PORCH DEMO AND NEW WORK ELEVATIONS AND DETAILS



PROJECT LOCATION:
14528 ASHTON RD.
DETROIT, MI 48223



1 PROJ. IMAGE
SCALE:

| SYMBOL LEGEND | ABBREVIATIONS |
|---|---|
| <p>Drawing Navigation Symbols</p> <p>Elevation Callout</p> <p>Sheet Number Drawing Number</p> <p>Section Callout</p> <p>"SIM" (if present) means this condition is similar to the drawing referenced. Drawing Number Sheet Number</p> <p>Detail Callout</p> <p>"SIM" (if present) means this condition is similar to the drawing referenced. Drawing Number Sheet Number</p> <p>Other Symbols</p> <p>Keynote Tag - refer to keynotes on that sheet</p> <p>Wall Tag - refer to Wall Schedule</p> <p>Door Tag - refer to Door Schedule</p> <p>Window Tag - refer to Window Schedule</p> <p>Name Elevation Floor Level Elevation</p> | <p>@ Acoustic Ceiling Tile</p> <p>ACT Acoustic Ceiling Tile</p> <p>ADJ Adjacent</p> <p>AFF Above Finish Floor</p> <p>ALUM Aluminum</p> <p>ANOD Anodized</p> <p>BD Board</p> <p>BLDG Building</p> <p>BLK Block</p> <p>BLKG Blocking</p> <p>CEM Cement</p> <p>CJ Control Joint</p> <p>CLG Ceiling</p> <p>CL Centerline</p> <p>CO Clean Out</p> <p>COL Column</p> <p>CONC Concrete</p> <p>CG Corner Guard</p> <p>CONST Construction</p> <p>CONT Continuous</p> <p>CORR Corrugated</p> <p>CPT Carpet</p> <p>CT Ceramic Tile</p> <p>DET Detail</p> <p>DIA Diameter</p> <p>DM Dimension</p> <p>DN Down</p> <p>DO Door Opening</p> <p>DR Door</p> <p>DWG Drawing</p> <p>EA Each</p> <p>ELEV Elevation</p> <p>EW Each Way</p> <p>EXG Existing</p> <p>EXIST Existing</p> <p>EXP Expansion, Exposed</p> <p>FD Floor Drain</p> <p>FDN Foundation</p> <p>FRP Fiber Reinforced Panels</p> <p>FIN Finish</p> <p>FLR Floor</p> <p>FO Face Of</p> <p>FOS Face of Stud</p> <p>FR Frame</p> <p>FTG Fasting</p> <p>FV Field Verify</p> <p>GA Gauge</p> <p>GALV Galvanized</p> <p>GYP Gypsum</p> <p>HDW Hardware</p> <p>HM Hollow Metal</p> <p>HORIZ Horizontal</p> <p>HT Height</p> <p>ID Inside Diameter</p> <p>INSUL Insulation</p> <p>INT Interior</p> <p>JT Joint</p> <p>LAV Lavatory</p> <p>LG Long</p> <p>LLO Long Leg Outstanding</p> <p>LLV Long Leg Vertical</p> <p>MAX Maximum</p> <p>MECH Mechanical</p> <p>MET Metal</p> <p>MEZZ Mezzanine</p> <p>MI Miscellaneous Iron</p> <p>MIN Minimum</p> <p>MISC Miscellaneous</p> <p>MO Masonry Opening</p> <p>NIC Not In Contract</p> <p>NTS Not To Scale</p> <p>OC On Center</p> <p>OD Outside Diameter</p> <p>OPNG Opening</p> <p>OPP Opposite</p> <p>PLG Plate Glass</p> <p>PLS Plate Steel</p> <p>PLAM Plastic Laminate</p> <p>PLAS Plaster</p> <p>PREFAB Prefabricated</p> <p>PROJ Project, Projection</p> <p>PSF Pounds per Square Foot</p> <p>PT Paint, Point, Pressure Treated</p> <p>R Riser</p> <p>RA Return Air</p> <p>RB Rubber Base</p> <p>RC Roof Conductor</p> <p>RCP Reflected Ceiling Plan</p> <p>RD Roof Drain</p> <p>RF Rubber Flooring</p> <p>REINF Reinforced, Reinforcing</p> <p>REOD Required</p> <p>RFG Roofing</p> <p>RM Room</p> <p>RS Roof Sump</p> <p>RT Rubber Tile</p> <p>SAN Sanitary</p> <p>SCHED Schedule</p> <p>SHT Sheet</p> <p>SIM Similar</p> <p>SPEC Specification</p> <p>SS Service Sink</p> <p>STL Steel</p> <p>STD Standard</p> <p>STOR Storage</p> <p>STRUCT Structural</p> <p>SUSP Suspended</p> <p>SW Switch</p> <p>SYM Symmetrical</p> <p>T Tread</p> <p>T&B Top and Bottom</p> <p>TEL Telephone</p> <p>TERR Terrazzo</p> <p>T&G Tongue and Groove</p> <p>THK Thick, Thickness</p> <p>THRESH Threshold</p> <p>TOS Tag Of Steel</p> <p>TYP Typical</p> <p>UC Undercut</p> <p>UNO Unless Noted Otherwise</p> <p>VB Vinyl Base</p> <p>VCT Vinyl Composition Tile</p> <p>VIF Verify In Field</p> <p>W Wide</p> <p>VERT Vertical</p> <p>WAINS Wainscot</p> <p>WC Water Closet</p> <p>WD WIN Wood Window</p> <p>WT Weight</p> <p>WWF Welded Wire Fabric</p> |

ARCHITECT:

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2761 E. JEFFERSON
SUITE 302
DETROIT, MI 48207
P. 313.450.4545
TM.FLINTOFF@4545ARCHITECTURE.COM

CONSULTANT:

Project :

CHN HOUSING PARTNERS ASHTON RD. FRONT PORCH RENOVATION 14528 ASHTON RD. DETROIT, MI 48223

Issued for :

HDC 01/21/2025

STATE OF MICHIGAN
TIMOTHY R. FLINTOFF JR.
ARCHITECT
No. 1304054083
LICENSED ARCHITECT

Drawn by : JRM
Checked by : JRM

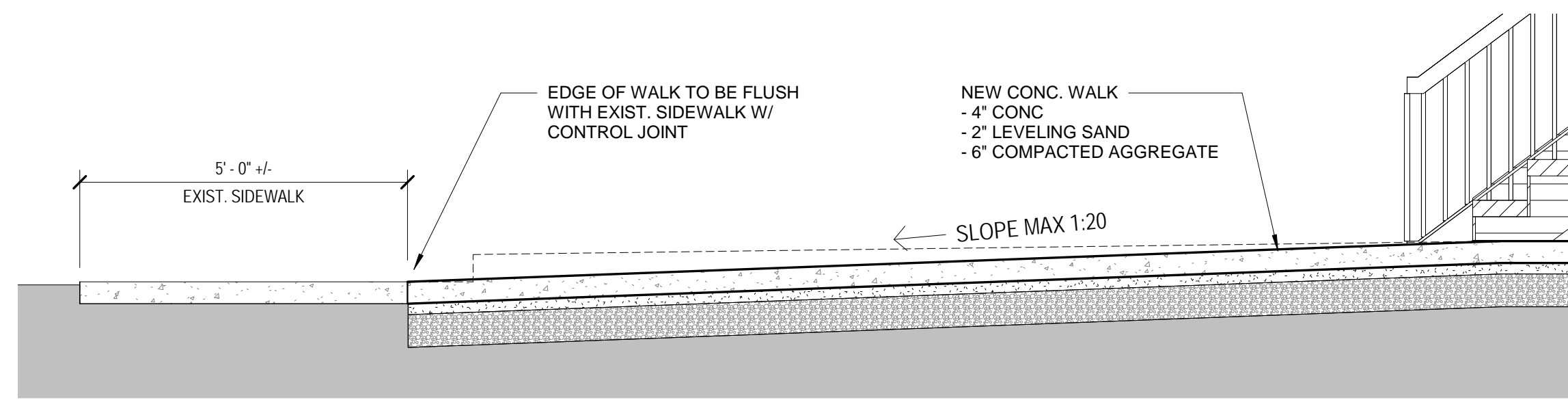
Sheet Title : TITLE SHEET

Project No. : 2024041

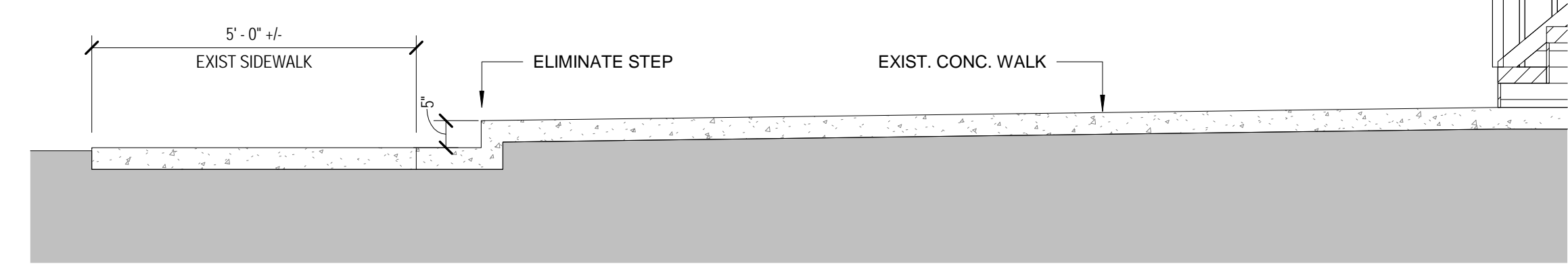
Sheet No. : TS1.1

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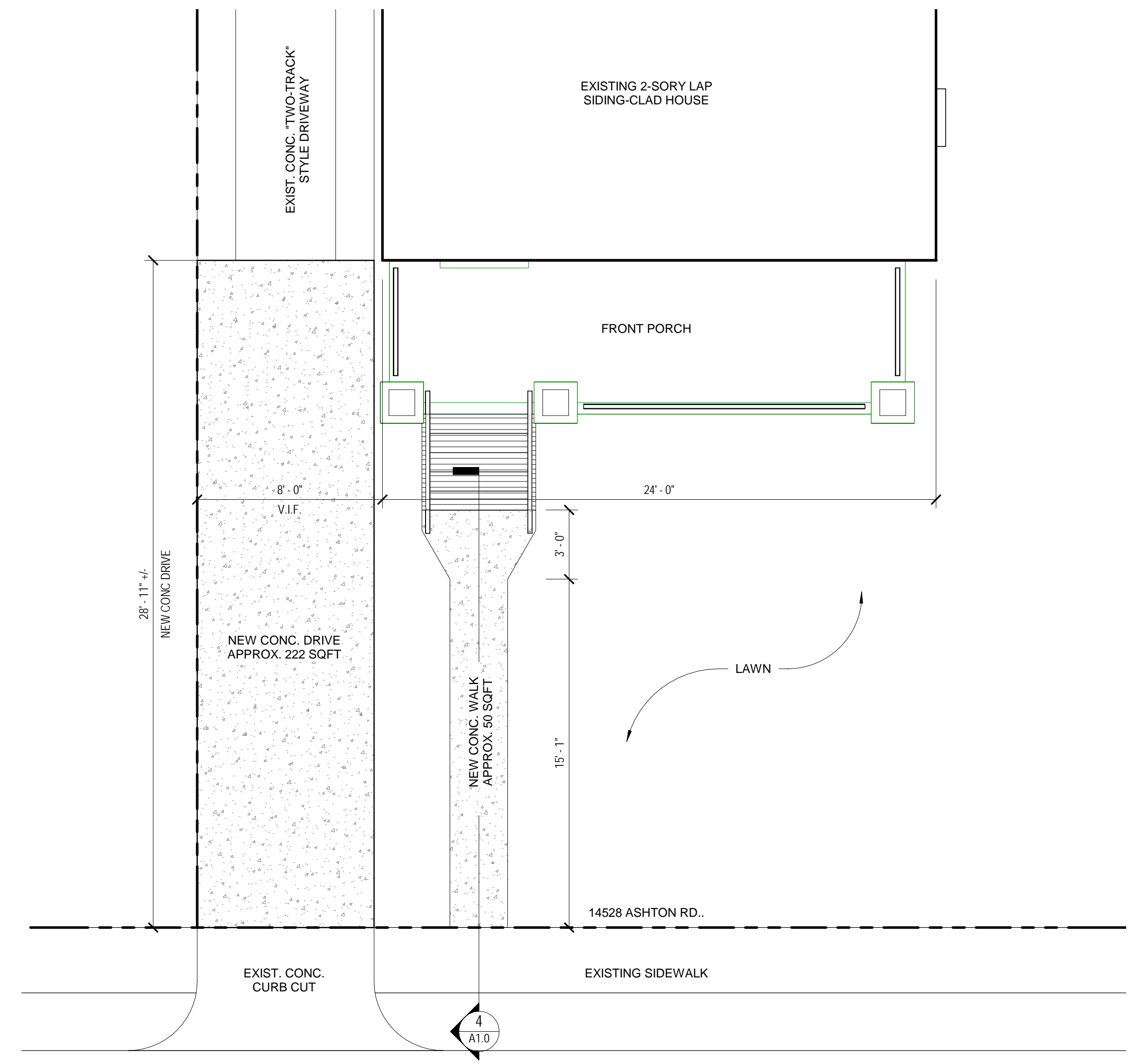
2 LOCATION PLAN
SCALE: 1" = 100'-0"



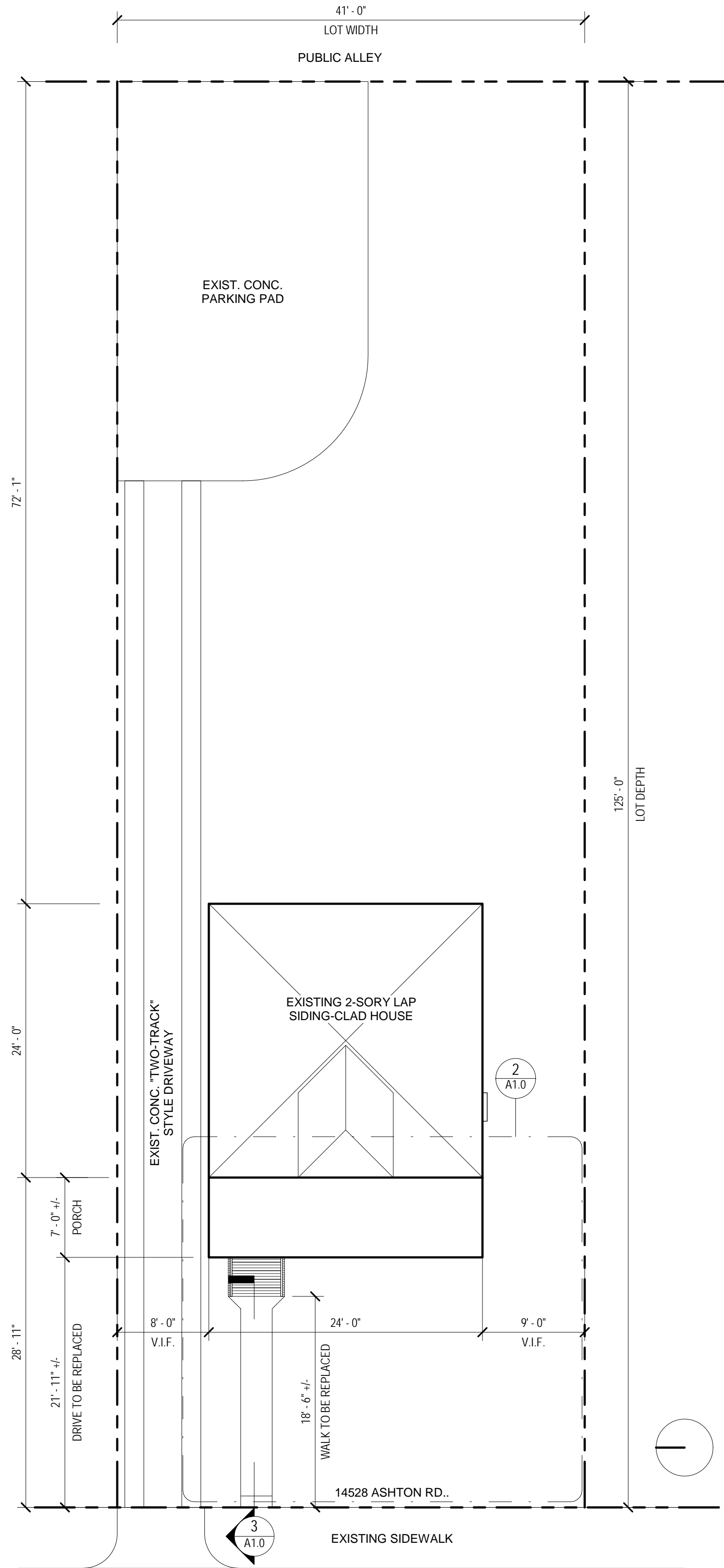
4 SECTION @ NEW CONC. WALK
SCALE: 1/2" = 1'-0"



3 SECTION @ EXIST CONC WALK
SCALE: 1/2" = 1'-0"



2 ENLARGED ARCHITECTURAL SITE PLAN - NEW WORK
SCALE: 1/4" = 1'-0"



1 ARCHITECTURAL SITE PLAN - EXISTING
SCALE: 1/8" = 1'-0"

GENERAL NOTES:

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- ALL DIMENSIONS ARE SHOWN FROM FINISH FACE TO FINISH FACE OF PARTITION UNLESS OTHERWISE NOTED.
- WALL THICKNESS' ARE NOMINAL NOT ACTUAL DIMENSIONS. SEE WALL SCHEDULE FOR ACTUAL DIMENSIONS.
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W ELEVATION OF HOME



- CONC. WALK IS CRACKED AND HEAVING
- WOOD STAIRS ARE NOT EQUAL RISER HEIGHTS
- SETTLING HAS CAUSED TREADS TO NOT BE LEVEL
- HANDRAILS ARE NOT SECURE

EXISTING CONC. WALK



- AWKWARD 5" STEP AT SIDEWALK IS TRIPPING HAZARD

EXISTING "2-TRACK" DRIVEWAY



- CONC. IS DETERIORATED DIFFICULT TO MAINTAIN, ESPECIALLY IN WINTER

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Issued for :
HDC 01/21/2025



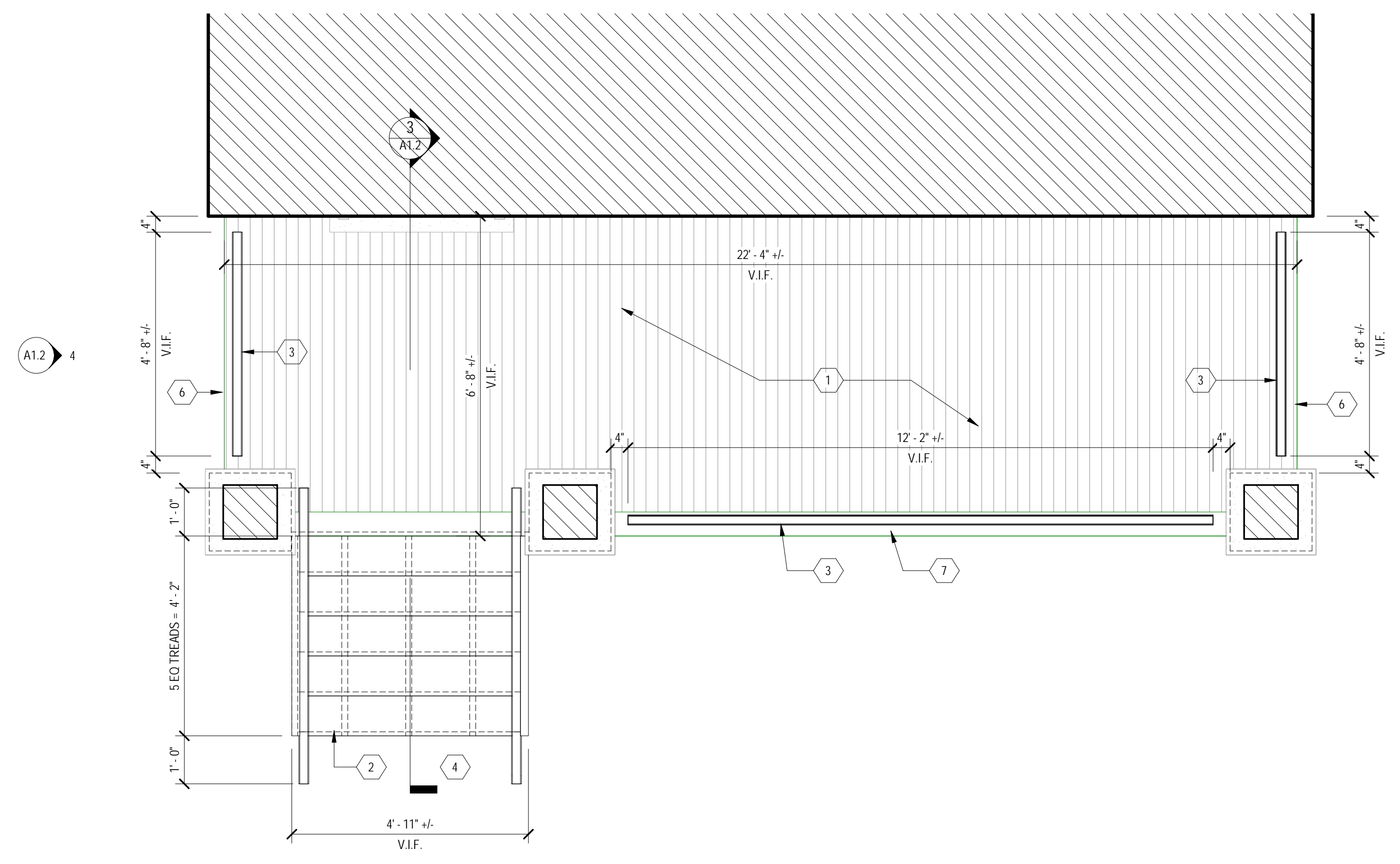
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JRM
Checked by :
JRM
Sheet Title :
ARCHITECTURAL SITE PLAN AND DETAILS

Project No. :
2024041

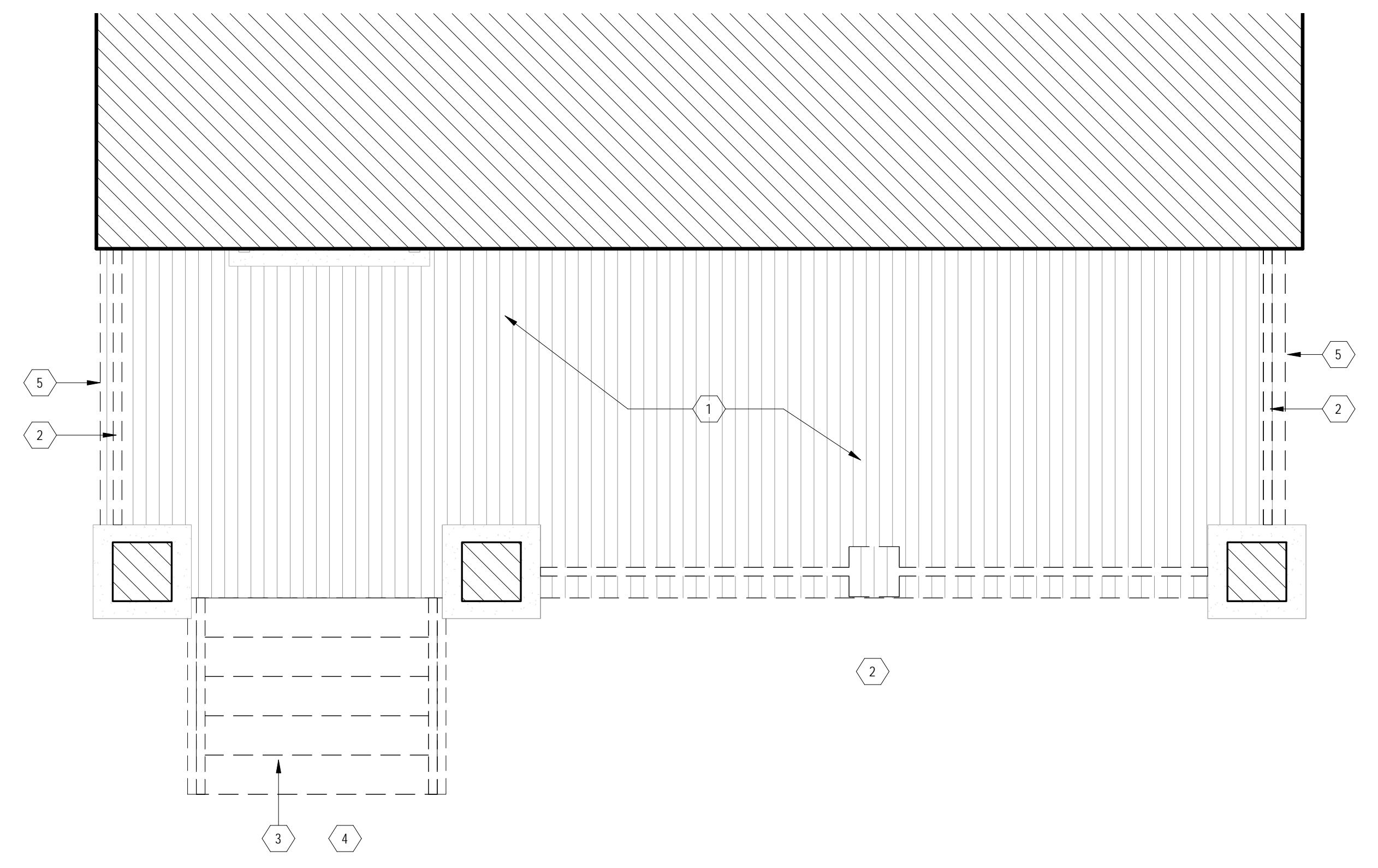
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BINDER STRIP LOCATION



2 NEW WORK PORCH PLAN
 SCALE: 1/2" = 1'-0"



1 EXISTING PORCH PLAN
 SCALE: 1/2" = 1'-0"

ON PORCH LOOKING SW



- RAILINGS ARE IMPROVISED 2x CONSTRUCTION, 30" TALL, NOT TO CODE
- ALL WOOD IS UNFINISHED/UNPROTECTED AND IS VERY WEATHERED

W ELEVATION OF STAIR



- CONC. WALK IS CRACKED AND HEAVING
- WOOD STAIRS ARE NOT EQUAL RISER HEIGHTS
- SETTLING HAS CAUSED TREADS TO NOT BE LEVEL
- HANDRAILS ARE NOT SECURE

N ELEVATION OF PORCH AND RAILING



- AT 30" TALL, RAILING IS NOT TO CODE
- UNFINISHED "1-11" SIDING IS NOT VENTED AND NOT WEATHER RESISTANT

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DEMOLITION KEY NOTES:

- (TYPICAL THIS SHEET ONLY)
- 1 REMOVE WOOD PORCH DECKING
 - 2 DEMOLISH WOOD RAILING
 - 3 DEMOLISH WOOD STAIR AND STRINGERS AND ANY SUPPORT FRAMING
 - 4 DEMOLISH CONC. WALK COMPLETE
 - 5 DEMOLISH PLYWOOD PANELING ON PORCH SIDE-WALLS

NEW WORK KEY NOTES:

- (TYPICAL THIS SHEET ONLY)
- 1 NEW PT DECK BOARDS ON EXISTING STRUCTURE, PAINT B:19, BLACK
 - 2 NEW WOOD STAIR: PT WOOD TREADS AND RISERS ON PT 2x STRINGERS, PAINT B:19, BLACK
 - 3 NEW PREFINISHED ALUM RAILING, 36" HIGH, BASES OF DESIGN: ULTRA RAILING SIGNATURE SERIES UAR-200-TR, BLACK
 - 4 NEW CONC. WALK, PROVIDE THICKENED SLAB UNDER FIRST STAIR RISER
 - 5 INSTALL SIMPSON STRINGER HANGERS AND BASE BRACKETS AS REQ'D
 - 6 JAMES HARDIE LAP SIDING W/ 3.5" TRIM ON PT 2x FRAMING TO INFILL OPEN PORCH CRAWLSPACE ENDS, PAINT B:19, BLACK, VENT AS REQ'D.
 - 7 SET FIRST DECK BOARD PARALLEL TO FRONT EDGE OF PORCH TO CREATE BORDER
 - 8 LOUVERED WALL-VENTS EA. SIDE, AS REQ'D, PAINT B:19, BLACK
- NOTE: ALL NEW WOOD ELEMENTS TO BE PAINTED B:19, BLACK BASED ON DETROIT HDC'S COLOR SYSTEM E CHART UNLESS NOTED OTHERWISE.

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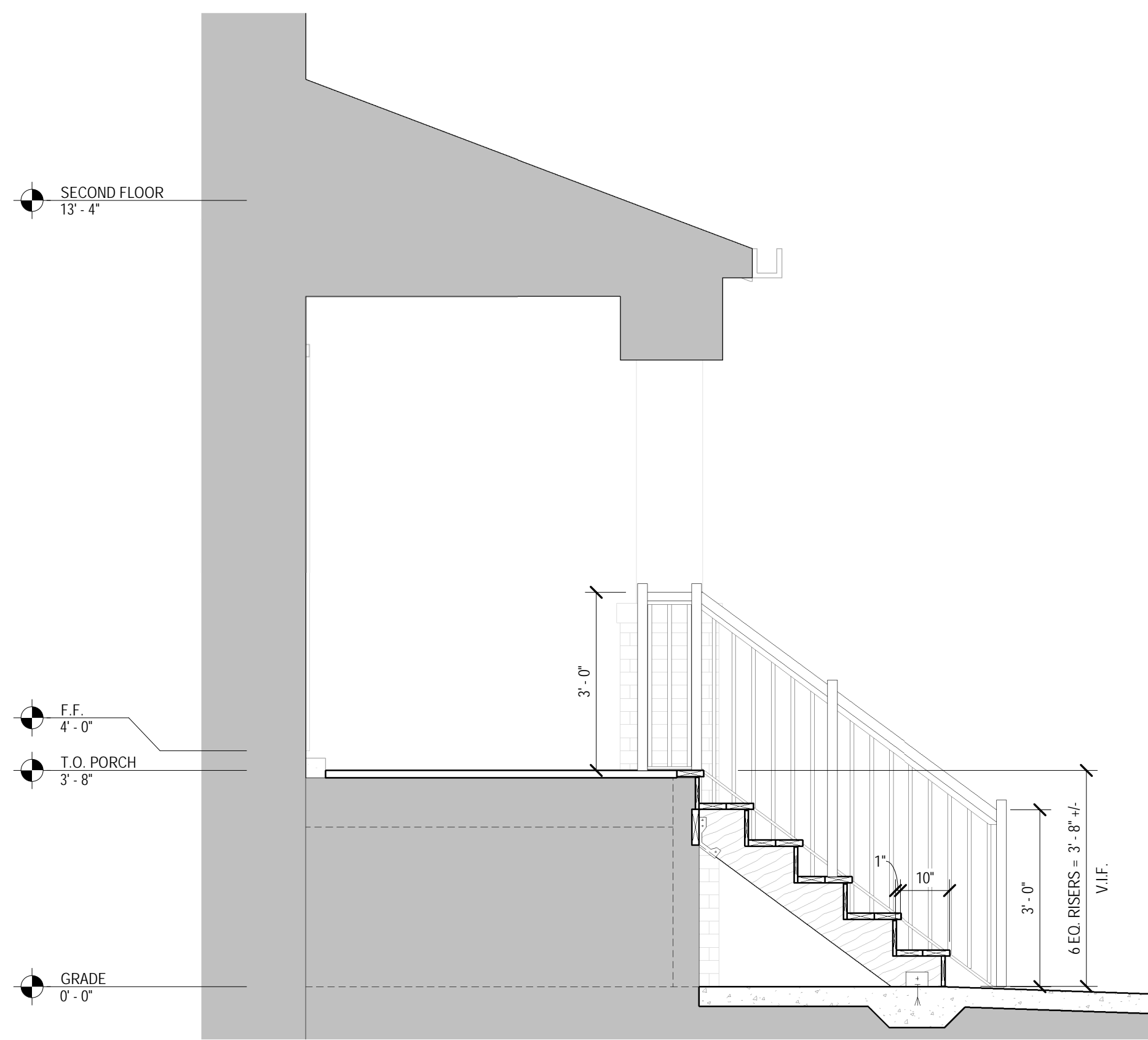
Sheet Title :
 PORCH DEMO AND NEW WORK PLANS

Project No. :
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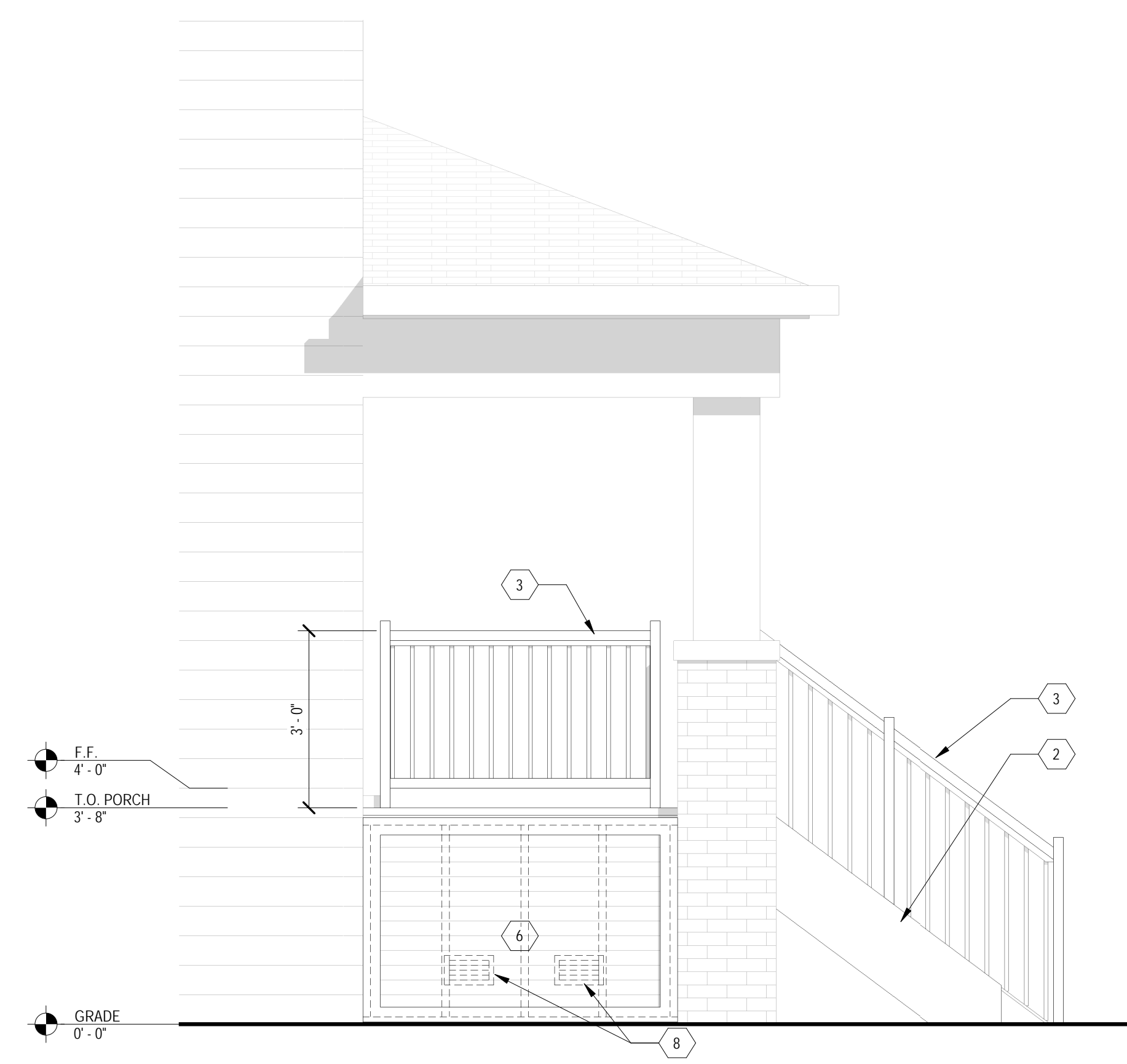
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3 NEW WORK PORCH STAIR SECTION
SCALE: 1/2" = 1'-0"



4 NEW WORK NORTH ELEVATION
SCALE: 1/2" = 1'-0"



2 OVERALL NEW WORK SOUTH ELEVATION
SCALE: 3/8" = 1'-0"



1 OVERALL EXISTING SOUTH ELEVATION
SCALE: 3/8" = 1'-0"

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 - DEMOLISH WOOD STAIR AND STRINGERS AND ANY SUPPORT FRAMING
 - DEMOLISH CONC. WALK COMPLETE
 - DEMOLISH PLYWOOD PANELING ON PORCH SIDE-WALLS

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PORCH DEMO AND NEW WORK ELEVATIONS AND DETAILS
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2024041

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