



LIVERNOIS AND MCNICHOLS CORRIDOR

# PUBLIC REALM PLAN





# TABLE OF CONTENTS

1	EXECUTIVE SUMMARY	05
2	INTRODUCTION	07
	<i>History of the Area</i>	
	<i>Vision Statement for the Corridors</i>	
3	GOALS OF THE PLAN	10
	<i>The Public Realm Plan</i>	
4	PAST PLANNING EFFORTS	13
	<i>Planning Efforts Timeline</i>	
	<i>Past Plans Review</i>	
5	CIVIC ENGAGEMENT	18
	<i>Civic Engagement</i>	
	<i>Community Engagement Efforts</i>	
	<i>Better Block Demonstrations</i>	
6	EXISTING CONDITIONS ANALYSIS	29
	<i>Overview of Analysis</i>	
	<i>Existing Streetscape Character</i>	
7	PLAN RECOMMENDATIONS	38
	<i>Overall Area Design Strategies</i>	
	Study Area A: McNichols Neighborhood Retail	45
	<i>Snap Shot: Study Area A</i>	
	<i>Existing Conditions</i>	

<ul style="list-style-type: none"> <li><i>Design Process</i></li> <li><i>Final Design Proposal</i></li> </ul>	52
Study Area B: McNichols at Marygrove College	
<ul style="list-style-type: none"> <li><i>Snap Shot: Study Area B</i></li> <li><i>Existing Conditions</i></li> <li><i>Design Process</i></li> <li><i>Final Design Proposal</i></li> </ul>	59
Study Area C: Livernois Avenue	
<ul style="list-style-type: none"> <li><i>Snap Shot: Study Area C</i></li> <li><i>Low-Density Livernois and Livernois at the University of Detroit Mercy</i></li> <li><i>Livernois at the Avenue of Fashion</i></li> <li><i>Design Process</i></li> <li><i>Final Design Proposal - Option A</i></li> <li><i>Final Design Proposal - Option B</i></li> </ul>	
8 STREETScape DESIGN GUIDELINES	76
<ul style="list-style-type: none"> <li><i>Guidelines For Design Details</i></li> <li><i>Protected Bike Lanes</i></li> <li><i>Bumpouts</i></li> <li><i>Chicanes</i></li> <li><i>Seasonal Parklets</i></li> <li><i>Green Alleys</i></li> <li><i>Street Furnishings</i></li> <li><i>Livernois - Option A</i></li> <li><i>Livernois - Option B</i></li> <li><i>McNichols Retail</i></li> <li><i>McNichols At Marygrove</i></li> </ul>	

9	OVERALL COST ESTIMATES	93
	<i>Cost Estimates</i>	
10	APPENDIX	95
	Livernois Streetscape Survey 1	96
	<i>Better Block Installation Feedback</i>	
	Livernois Streetscape Survey 2	102
	Livernois Streetscape Survey Raw Data	109
	<i>Summary</i>	
	<i>Impacts of Streetscape Demonstration</i>	
	<i>Commercial Development Questions</i>	



# EXECUTIVE SUMMARY

1

## > DEVELOPMENT OF THE PUBLIC REALM PLAN

The two corridors in this study form the link between a number of historic neighborhoods of Detroit and tie together two major Detroit institutions—the University of Detroit Mercy and Marygrove College. Although these corridors currently show some signs of vacancy and neglect, the foundation of these once highly productive and lively thoroughfares is still intact. The revitalization process of the corridors—and the surrounding neighborhoods—are in the beginning stages, anchored by successful pockets of commercial activity along the corridor, including the Avenue of Fashion and adjacent local businesses. A strategic investment in the streetscape would be a major catalyst for continued growth along the corridors and in the surrounding neighborhoods. The City of Detroit is in a unique position to lead this effort and continue the revitalization underway in this community.

With growing interest from the community and signs of positive change in the neighborhoods, it is the opportune time for the City of Detroit to think critically about providing support for public realm improvements. The purpose of this study is to engage the community in the planning process, and to analyze the most effective approaches for public investment in the streetscape.

The study area is generally comprised of the neighborhoods that border Livernois Avenue between Lodge Freeway and 8 Mile Road. Two corridors within this area, Livernois Avenue itself, and W McNichols Road between the University of Detroit Mercy and Marygrove College, are the focus of the design recommendations. A primary goal of the public realm plan is to provide a set of strategic design interventions—focused on streets and sidewalks—for the City of Detroit to consider for implementation.

The public realm plan is a product of extensive community engagement. As a first step in the process, past planning efforts for the area were analyzed. This was followed by a series of community engagement meetings, a survey of local business owners and residents, and a full scale mock-up of some

of the potential design interventions. From the community engagement process, three overarching goals were developed:

- [Improve the Pedestrian Experience](#)
- [Enhance Local Businesses](#)
- [Build on the Unique Identity of the Area](#)

An analysis of the existing conditions of the two corridors and surrounding neighborhoods and institutions was conducted. The McNichols Road and Livernois Avenue corridors were then partitioned into three study areas:

- [Study Area A: McNichols Neighborhood Retail](#)
- [Study Area B: McNichols at Marygrove College](#)
- [Study Area C: Livernois Avenue](#)

Each study area has unique conditions and opportunities. The design proposals reflect the local conditions within the neighborhood, and address the businesses and recreational opportunities along that part of the corridor.

The final design proposals for each of the study areas were developed with extensive community input. The proposals were reviewed by City agencies—such as fire, police, and engineering departments—for feasibility. This study concludes that if implemented, these proposals will greatly improve the walkability and bikeability of these corridors, increase the opportunity for an economic resurgence for businesses in the area, and contribute to a thriving residential community.

# INTRODUCTION

2



# HISTORY OF THE AREA

## > 20TH CENTURY HISTORY

The Livernois and McNichols area was first surveyed in 1816 and was farmland until the area was annexed by the City of Detroit in 1922 and subsequently subdivided for urban home construction in the 1920s. In 1927 both Marygrove College and the University of Detroit opened their new campuses at their current locations near the intersection of Livernois Avenue and McNichols Road. The area was also impacted by Henry Ford's nearby Highland Park Plant, which spurred suburban development in neighborhoods around Livernois and McNichols neighborhoods. Commercial land was largely developed during a WWII boom, contributing to the development of the Livernois corridor as a primary retail destination, along with McNichols, Puritan and other thriving neighborhood commercial streets. The John C. Lodge Freeway was built in 1959. In the 1960s, the area became a destination for African American middle and upper class families, contributing to today's strong communities and the arts and business culture along Livernois and McNichols.

## > THE AVENUE OF FASHION

From the 1950s to the 1970s the stretch of Livernois from 7 Mile to 8 Mile became known as the Avenue of Fashion as shops bustled and a pedestrian retail corridor thrived. Businesses like B. Siegel, Woolworths, and Winkelmans were destination shopping locations known for quality and customer service. Baker's Keyboard Lounge, America's oldest operating jazz club, opened in 1934 and continues to anchor Livernois at the corner of Eight Mile, leading a rich musical legacy in the area.



*Livernois Avenue at McNichols Road, mid 1960s. (University of Detroit Mercy)*



*Winkelman's department store on McNichols Rd. at San Juan Dr., 1940's. (Flicker user Howard258)*

## > TODAY'S COMMERCIAL CORRIDORS

Retail activity along Livernois began to decline in the 1970s, as did the businesses along McNichols that bridged Detroit Mercy, Marygrove and the neighborhoods. This trend is still evident today with a large number of vacant properties along both corridors. Nevertheless, some new businesses opened along Livernois in the 1980s and 1990s, and retail activity in the 2000s set the stage for today's resurgence. Joe's Gallery, 1917 American Bistro and Simply Casual are among the businesses that jump-started the current retail revitalization along the Avenue of Fashion. In 2006, the median along Livernois was constructed, which greatly reduced speeds and accidents but remains unpopular among local residents and businesses primarily due to inconvenient turn-arounds and circulation changes. In the last several years, the Livernois corridor has benefited from revitalization efforts led by the City of Detroit, strong business leaders, and support from local organizations and institutions. This effort has been guided by the University Commons Organization and a bevy of partners, and including initiatives like a facade improvement program, Revolve Detroit's pop-up-to-permanent retail program, the Livernois Community Storefront, and annual events like Jazz on the Ave. The commercial corridors also benefit from dense surrounding neighborhoods that are well-positioned to support new business.

## > A PLAN TO GENERATE EXPONENTIAL INCREASES IN INVESTMENT

As seen through the history of the Livernois and McNichols corridors, this area has served the people of Detroit in a multitude of ways for almost a century. For local business owners, investors, and entrepreneurs, these streets have provided attractive places of commerce, business establishment, and growth. Local retailers prominent in the 1950's to 1970's found success in running their businesses along a main retail-pedestrian hub of the city. Like most successful businesses and retailers, they followed one of the most foundational business strategies of “going where the people are.”

And again today, the same strategy proves to be key. New local businesses are reviving the corridors of Livernois and McNichols in places which currently have the most pedestrian traffic and the most potential for commercial growth and increased pedestrian movement. New businesses have been opened within the last few decades, and even within the last few years, on Livernois within the high-density retail hub known as the Avenue of Fashion, and along McNichols in places with a density of existing buildings and other businesses near by that attract pedestrians.

It could be understood then, that private investment in the form of business and retail development is usually targeted within areas of busy activity and movement, places that attract the presence of customers, create foot traffic, and provide visibility. The first premise of real estate investment has been explained for decades as “location, location, location.” Where is my business going to be seen and noticed? Where do the target customers already go to spend their time and money?

It could be said that people are attracted by the businesses and not the other way around. But when people are only attracted by businesses then there is solely justification for people to come, obtain goods or services and leave. This is the ultimate result of strip centers, big-box stores and low-density commercial development. These developments cater to vehicular use over

pedestrian use. The effect is quick, efficient, one-stop shopping with half empty parking lots and completely empty sidewalks. Local businesses and restaurants have little contact with people and are not given the opportunity to attract new customers. Already established businesses with name recognition and a known service are able to thrive in this environment, while new local businesses struggle to attract the customer base that they need.

On the other hand, investment given to streetscapes, to reinforce them as a key interface in the neighborhood, between the people of the community and the local businesses, will produce an exponential increase in additional investment in the area over time. With enhancements to pedestrian and bicycle infrastructure, residents will be drawn into the public spaces of the community. The character of the corridors will be transformed into a series of inviting, pedestrian-oriented destinations, rather than a group of places to quickly pass through. This new streetscape character will foster investment from local businesses who are looking to be located in places with high foot traffic and visibility.

Necessary upfront investment costs will become seemingly negligible in comparison to the booms in private investment and development along the streets and within the neighborhoods. The vision for this area is for it to become a hot-spot of growth, attracting home-owners, university students, entrepreneurs, developers, and visitors. Local businesses will not be the only ones who will be positively impacted by investment in the streetscape of these two corridors. The improved streetscape will also provide amenities to the people of the community, while an increase in commerce, activity, and interaction will support the health, safety, and increased opportunity for the whole community. This ripple-effect of community enhancement and investment will directly improve the public realm experience and quality of life of the community as a whole.

# GOALS OF THE PLAN

3



## > THE PURPOSE OF THE PLAN

The streets of a neighborhood are its lifeblood. Healthy and vibrant communities are centered around streets that are full of life, streets that attract people to walk, shop, and visit with neighbors, and streets that are safe for cyclists and pedestrians. Cities with great streets offer more opportunity for economic growth, a better quality of life for the surrounding neighborhoods, and support the development of an empowered, active and connected community. The purpose of this study is to propose ways for the City of Detroit and the local community to invest in streets and public open space, and create the foundation for an economically vibrant and socially cohesive community. With this purpose in mind, the Public Realm Plan has been uniquely crafted with three overarching goals as standards for the improvement of the streetscapes and public realm spaces of the study area.

## > GOAL #1: IMPROVE PEDESTRIAN EXPERIENCE

The first overarching goal of the plan is the improvement of the pedestrian experience along the Livernois and McNichols corridors. The objective is to make the streets safe places for pedestrians, young and old. Crossing the street should feel safe. The sidewalks should be pleasant places to stroll under shady street trees, while including places to sit in between shopping or dining. There should be convenient, well-marked, safe places to cross in the middle of the block. Slowing cars down through traffic calming measures will make standing at the corner waiting to cross the street a more human experience, and make getting in and out of your car while parking less daunting.

To achieve this richer pedestrian-oriented street life, the plans propose to redesign the intersections to include “curb bumpouts” which will make the distance between one side of the street and the other much shorter. The plan also proposes increases in mid-block crossings with better markings so people always feel like it is convenient to cross at a designated cross walk. To keep people from speeding, slightly narrowing the travel lanes has been

proven to slow cars down and keep them closer to the speed limit. Street trees are proposed to line both sides of the streets to provide shade to pedestrians, while benches and other site furnishings will provide places for people to sit and inhabit the public realm spaces.

## > GOAL #2: ENHANCE LOCAL BUSINESSES

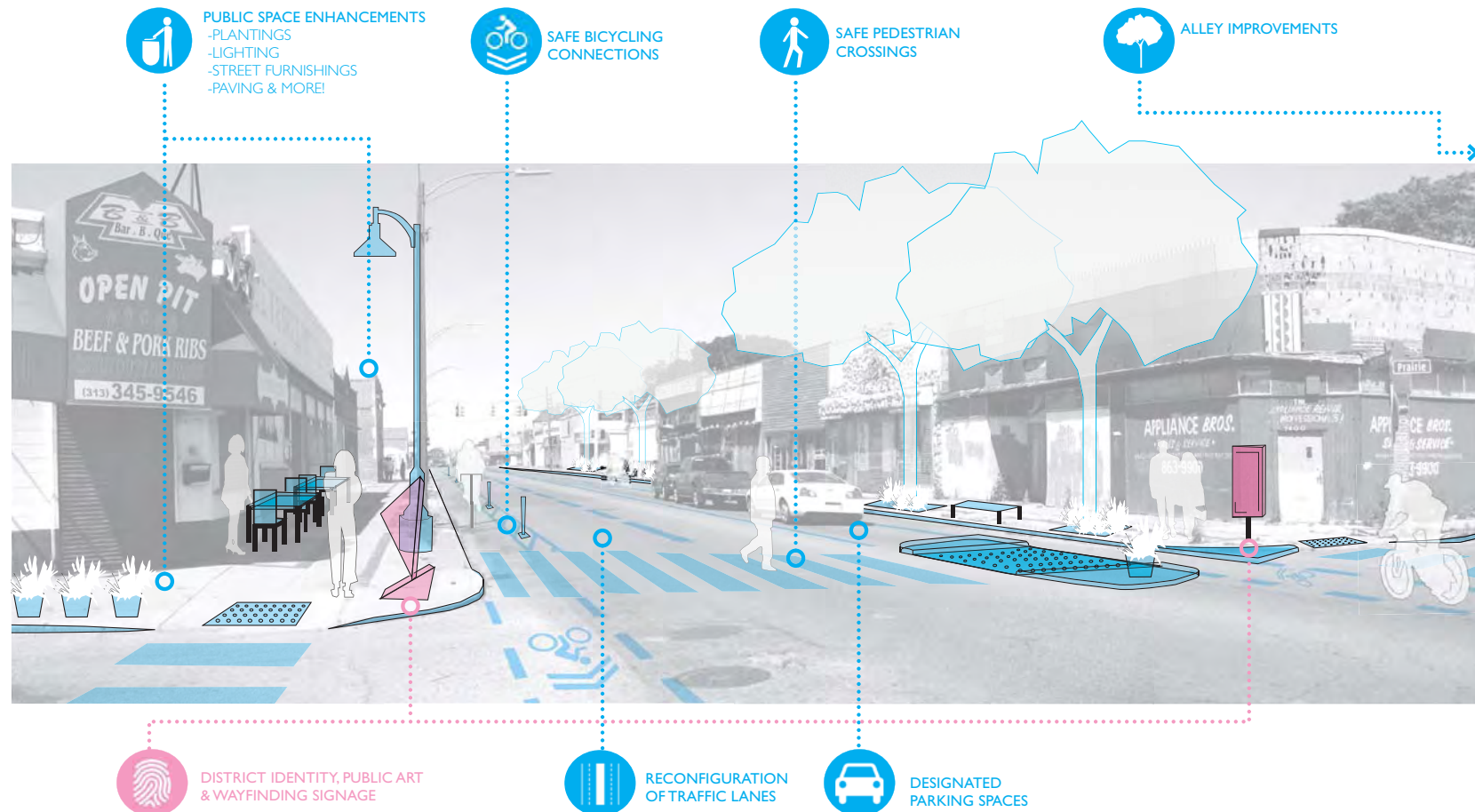
The second overarching goal of the plan is to enhance the local businesses along the corridors. Making the streets safer and more pedestrian friendly is a first step. Once you get to Livernois and McNichols it needs to be easy to walk around and enjoy the restaurants, cafés and local shops. But how you get there is important as well. It should be easy to drive your car to the area and find a parking spot. It should also be easy to hop on your bike and make your way to a local coffee shop. Also, local businesses should be able to expand onto the sidewalks and have cafe tables and clothing racks on the street to bring the dining and shopping experience into the public realm.

To improve the business climate along the Livernois and McNichols corridors, the plan suggests eliminating the J-turns, or Michigan lefts, from Livernois Avenue. This will make it easier for local traffic, as well as for people coming to the neighborhood with the purpose of dining or shopping, to arrive at their desired destination along a more direct route. Maintaining the street parking along both corridors makes visiting a local retailer that much easier. The plan also proposes protected bike lanes along the entire length of both corridors. Being able to hop on a bike at Detroit Mercy or McNichols and safely make your way to the Avenue of Fashion for lunch, or vice versa, would be a real benefit for all of the businesses along the corridors. Wider sidewalks at certain key areas, along with the other pedestrian improvements, will make introducing dining and shopping onto the street easier, and make strolling the street to shop more attractive.

### > GOAL #3: BUILD ON THE UNIQUE IDENTITY OF THE AREA

The third overarching goal of the plan is to build on, and enhance, the unique identity of the area and its history. Reintroducing tree-lined streets with amenities such as benches, special paving, and local art installations is fundamental to reinforcing the character of the streets and the community. The experience of the street at night should also be a priority. People should

feel safe and the sidewalks should be well-lit in the evenings. Unique signage, lit to add excitement and vitality to the street in the evenings, is key to promoting the businesses along the corridors as well as enhancing the identity of the corridors.



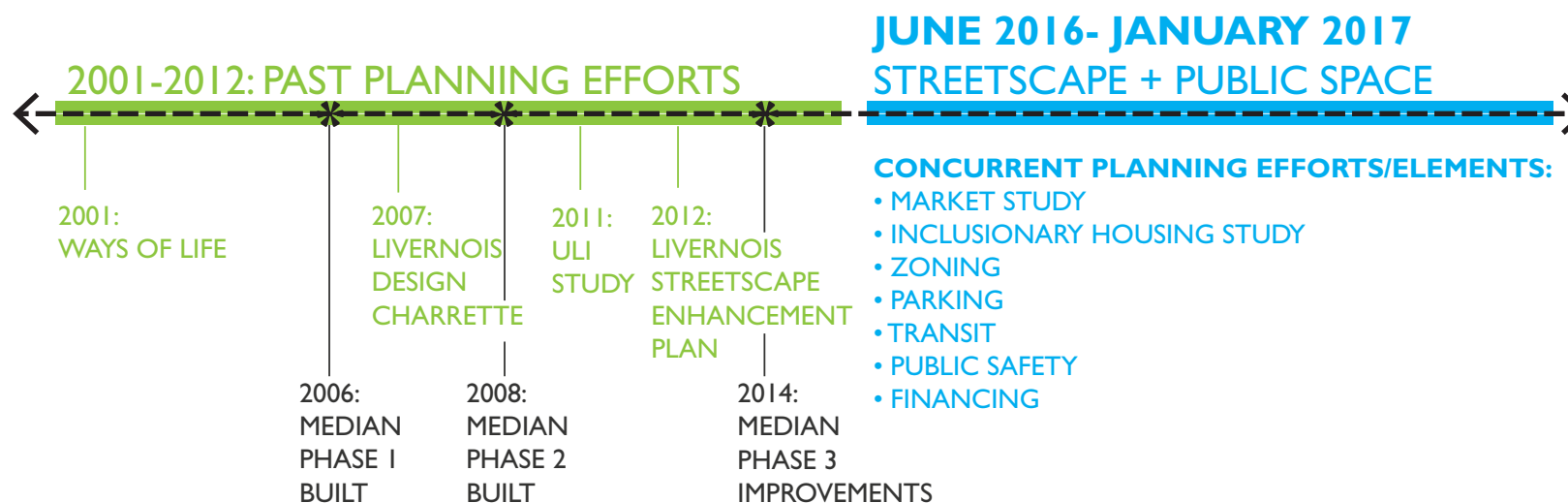
**FIG 01** *project scope diagram*

# PAST PLANNING EFFORTS

4



## > PLANNING EFFORTS TIMELINE



During the initial phase of the planning process, a review was conducted of past planning efforts in the Livernois and McNichols area that have taken place within the past 15 years. This review was done in an effort to better understand our current planning context and the planning work that has come before, as residents, business owners, and institutions invested considerable time and energy in past planning efforts. This plan builds upon key ideas from past planning efforts that still remain relevant. These include the desire to coordinate investments to create vibrant, walkable commercial corridors and public spaces that support commercial and residential development, and better connect local institutions with the adjacent community.

As a whole, the past planning efforts lacked funding for implementation, were limited in scope, and were not comprehensive. Instead, efforts were largely focused on addressing immediate issues at hand through short-term responses. For example, re-configuring the median was not considered to be part of the LSEP scope in 2012.

Many of the past planning efforts did not include bicycle infrastructure at all, or only

proposed minimal investments, such as sharrows. All of the past planning efforts reviewed called for further planning and studies for the McNichols and Livernois corridors, which previously have not had funding, capacity, and/or coordination to carry them out.

Detroit has changed a lot in recent years. There are now more resources available for business development, and many new businesses have opened along the Livernois and McNichols corridors. The City has come a lot further in developing a plan for incorporating bicycle infrastructure across the entire city. In recent years, the need for development of green stormwater infrastructure has become more evident, and some federal funding has become available. Overall, there is now more capacity and coordination within City departments and local institutions, including Live 6 and University of Detroit Mercy. We are now in a better position to work together to address larger, longer term issues and develop a more comprehensive, holistic plan for the Livernois and McNichols corridors.

# PAST PLANS REVIEW

## > WAYS OF LIFE: WAYNE COUNTY - UNIVERSITY COMMONS, 2001

PROJECT AREA: Livernois: Lodge to 8 Mile & McNichols: Livernois to Wyoming.

OVERVIEW: Wayne County's Ways of Life concept uses the extensive network of existing roadways as a framework for crafting sustainable development strategies to improve quality of life for greater Detroit's residents and visitors. This plan for the Livernois-McNichols area utilizes branding, physical design and economic strategies to re-establish aging commercial corridors through promotional development of business and community interests.

### KEY RECOMMENDATIONS:

- > Link the UDM campus, commercial frontage, and community on Livernois.
- > Calm traffic and create a stronger pedestrian orientation and visual appeal.
- > Recommendations for Livernois: 30-foot wide landscaped median with curb bumpouts at intersections and mid-block pedestrian crossings, street trees, special paving, pedestrian lighting and traffic signals.
- > Similar recommendations were made to improve the pedestrian environment on McNichols.
- > Create a corridor "brand," establish the University Commons Organization, create gateways at the Lodge Freeway and 8 Mile.
- > Additional redevelopment opportunities, such as housing across from Marygrove on McNichols and a UDM mixed use welcome center on Livernois.



*University Commons branding strategy*



*Redevelopment strategy on McNichols*



*Livernois boulevard concept*



*Livernois-Lodge gateway concept*

## > LIVERNOIS BUSINESS DISTRICT: CHARRETTE & PRELIMINARY RECOMMENDATIONS 2007

PROJECT AREA: Livernois: John C. Lodge Fwy. to 8 Mile Road

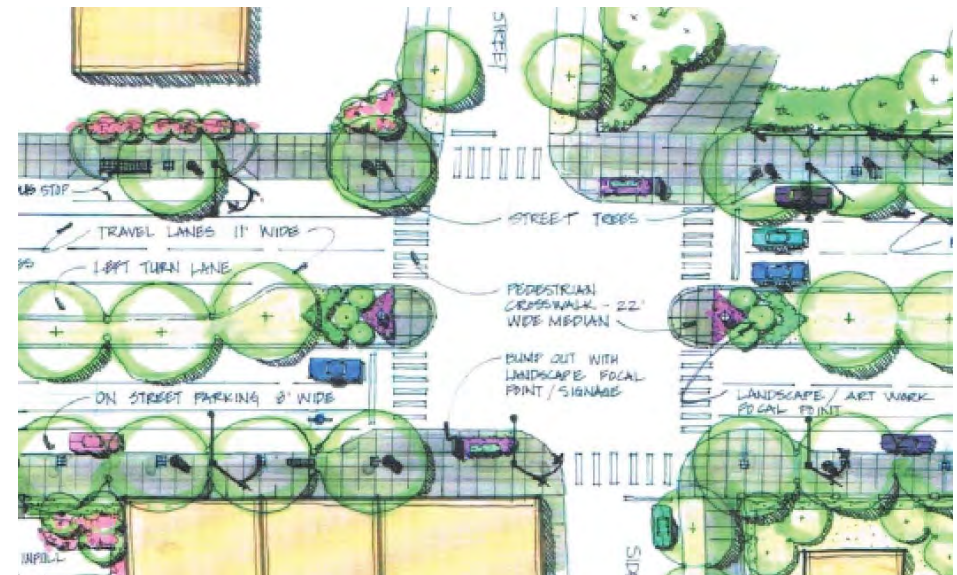
OVERVIEW: This planning effort led by Rainy Hamilton Jr. aimed to support University Commons Association in creating a viable business district that supports adjacent neighborhoods, improves pedestrian circulation and parking, and addresses the issues created by the newly constructed median. While it was understood that the district needed a comprehensive Master Plan, this effort was focused on identifying and prioritizing concerns and suggesting a course of immediate, implementable actions. The group agreed that policy and programming actions were most important, followed by physical improvements and aesthetics.

### RECOMMENDATIONS:

- > Add irrigation, power, signage and landscaping to the Livernois medians.
- > Conduct further traffic studies and modify the median accordingly with enhancements and/or removal.
- > Five foot unprotected bike lanes were proposed, as well as a variety of pedestrian amenities.
- > The development of concise and enforceable facade and signage guidelines was recommended.
- > The plan discouraged surface parking adjacent to the right-of-way and proposed knee-walls and landscaping to buffer existing lots.



Section sketch



Typical intersection sketch

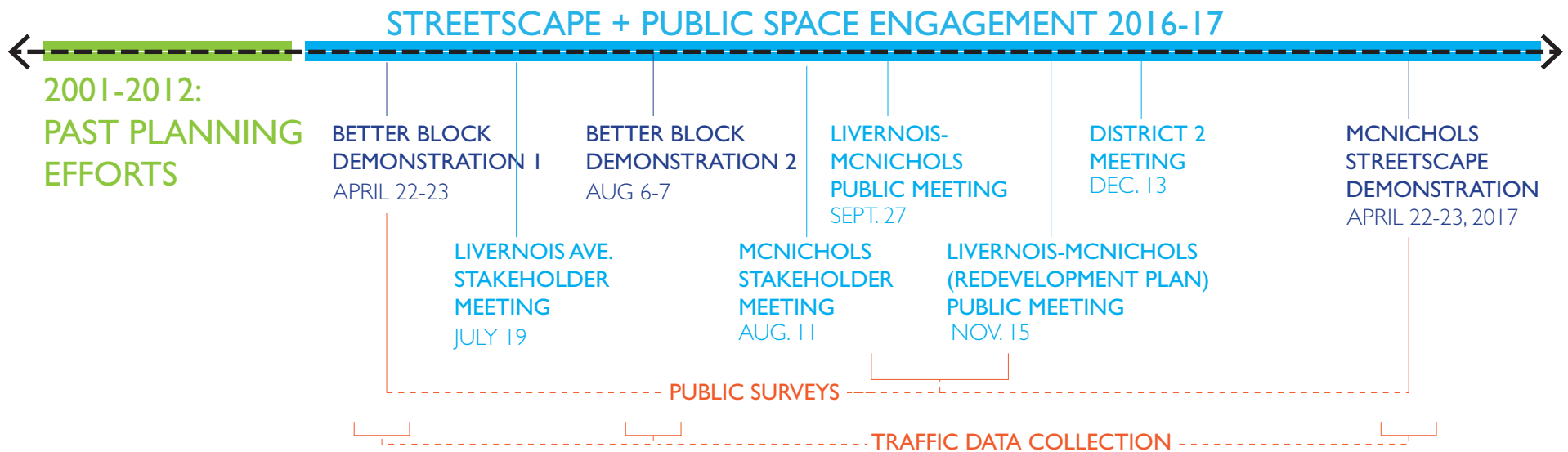




# CIVIC ENGAGEMENT

5

## > TIMELINE OF COMMUNITY ENGAGEMENT ACTIVITIES



Civic engagement for this planning process builds upon what was heard and documented during previous planning projects and the past efforts of the Livernois and McNichols communities. This phase of engagement for the streetscape and public space plan began in April 2016 with the first Better Block installation. The Better Block Foundation (BBF) employs rapid prototyping to reshape and reactivate built environments in the service of creative placemaking and support of public life. The BBF, City and DCDC implemented two pop-up temporary streetscape demonstrations on Livernois in 2016, and the DCDC, Live6 and the City led a third popup on McNichols in April of 2017. These temporary demonstrations allow the City and planning team to test streetscape strategies such as bike lanes and bump outs, and gauge appetite for more active street life along the commercial corridors. Better Block events included surveys and other feedback mechanisms to track people's experiences with the altered streetscape. Traffic and bike data collection coincided with the installations.

In addition to the pop-up streetscape demonstrations a series of community meetings informed this work. These meetings began with stakeholder meetings for Livernois and McNichols property and business owners. Three larger public meetings, including one collaborative meeting with the economic development planning team, occurred throughout Fall 2016, culminating at the District 2 holiday meeting in December.

Throughout this process, community and stakeholder feedback informed the planning trajectory. Key examples include the development of streetscape configurations for both roads and focused conversations about both emergency vehicle access and side street spillover traffic. Intentional interdepartmental collaboration also contributed to these conversations. It is important to note that civic engagement by the City will continue as this project moves toward implementation.

## > INTER-AGENCY + DEPARTMENTAL COLLABORATION

The following agencies worked collaboratively on the planning team to provide technical insight to help guide the planning process. An interdepartmental and interagency core team met on a bi-weekly basis and other departments were consulted for key decisions. The planning team provided regular briefings to the Mayor's staff.

- Planning and Development Dept. (PDD)
- Dept. of Neighborhoods (DON)
- Housing and Revitalization Dept. (HRD)
- Dept. of Public Works (DPW)
- Detroit Water and Sewerage Dept. (DWSD)
- Detroit Dept. of Transportation (DDOT)
- Detroit Fire Dept. (DFD)
- Detroit Police Dept. (DPD)
- Detroit Economic Growth Corp. (DEGC)





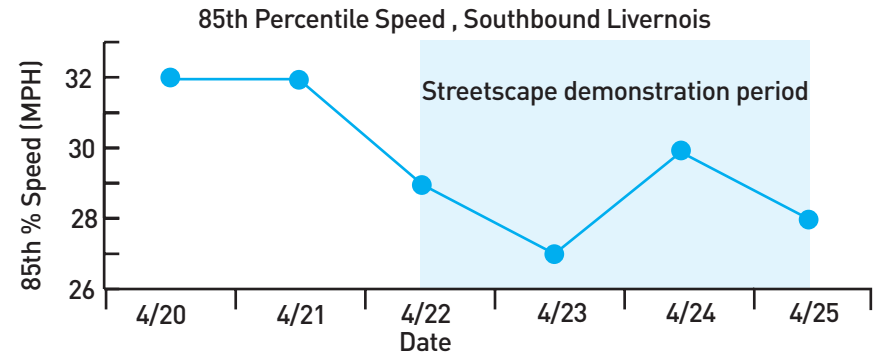
# COMMUNITY ENGAGEMENT EFFORTS

CITY OF DETROIT

## > BETTER BLOCK 1 / SOUTH LIVERNOIS // APRIL 22-23, 2016

This temporary streetscape demonstration kicked off engagement efforts for the public realm planning process. It was located on Livernois Ave, just south of Grove Street across from the University of Detroit Mercy, and tested out several different street design strategies, including a two-way cycle track on the northbound side of Livernois Ave, and angled parking on the southbound side. Livernois was reduced to one travel lane in each direction. The event also sought to physically and socially bridge the divide between the Detroit Mercy campus and the community. A temporary pocket park featuring local food and artisan market vendors was set up in the University-owned empty lot, which has spurred plans to develop a more regular artisan market in that location.

Traffic data gathered prior to and during the event showed that the installation decreased 85th percentile speeds by over 10%. Traffic flow kept moving and dangerous speeding was mitigated. Forty-two people filled out a feedback survey, which provided insight into community preferences for corridor improvements.



TOP STRATEGIES TO MAKE LIVERNOIS 'MORE VIBRANT':



"MORE SHOPS"



"MAKE IT WALKABLE"

TOP FOUR ELEMENTS THAT RESPONDENTS WANTED TO SEE MADE PERMANENT:

1. BIKE LANES

2. CROSSWALKS

3. ANGLED PARKING

4. POCKET PARK/MARKET



Two-way cycle track on Livernois



Pocket park with local vendors and entertainment

**FEEDBACK SURVEY**  
BUILDING A BETTER BLOCK DETROIT

1. What neighborhood/city do you live in?  
Bakery - Detroit, MI

2. Was the event what you expected? Better? Worse?  
YES. GREAT TO SEE LOCAL BUSINESSES

3. If one part of the block pop-up was going to remain permanent on this block, what should it be? (For example: bike lanes, pocket park, seating, crosswalk, diagonal parking, etc.)  
BIKE LANES!

4. What are other block improvements you'd like to see? (For example: lighting, working bike art, businesses, building improvements, etc.)  
HISTORIC markers along walking path to tell story of Livernois history

5. What is one thing that would make Livernois more vibrant?  
Restaurants w/ outdoor seating, more like public little marketplace

6. If there was a farmer's market in the vacant lot across from UDM, when would you most likely visit?  
☐ Wednesday 4-7pm  
☐ Friday 4-7pm  
☒ Saturday 12-3pm

Name: DESHA  
Email:  
Zip code: 48221

Example completed survey



## > LIVERNOIS AVE. STAKEHOLDER MEETING // JULY 19, 2016

This meeting introduced the Livernois-McNichols Streetscape and Public Space Plan to Livernois business and property owners. 18 people attended, largely from the Avenue of Fashion. Feedback was gathered during a [Q&A and discussion period](#) as well as a [mapping activity](#) during which attendees noted and illustrated ways to improve the Livernois streetscape.



*Mapping activity at the Livernois Avenue stakeholder meeting*

## KEY TAKEAWAYS:

**PARKING:** Having adequate parking is a major priority for many businesses. Some people are concerned about losing parking spaces, others advocated for encouraging visitors to park in centralized parking areas and walk to destinations. There was an interest in working with large property owners to create a shared neighborhood parking strategy (for example, New Prospect) and strategies to enhance the aesthetics of surface parking lots. New development would warrant additional parking considerations. More parking data would be helpful to determine actual use and need.

**TRAFFIC & EMERGENCY ACCESS:** Traffic counts and speeding data from the first Better Block were shared and found are very informative to many. There was concern about how much traffic will be diverted due to slower speeds and how that will impact businesses, and also questions about whether transitioning to one lane in each direction would provide adequate emergency vehicle access. There was a strong interest in a local shuttle linked to centralized parking. **Note: The fire department was consulted following this meeting, which influenced the proposed streetscape designs.**

**MEDIAN:** Some of the initial issues with the median's aesthetics have been remedied, but there is still room for improvement to create a more welcoming streetscape. The primary issue with the median is that it prohibits left-hand turns in many areas, forcing drivers to do long turnarounds. There was interest in altering the median if feasible.

**STREETSCAPE & DEVELOPMENT ZONES:** There was interest in a unifying identity and treatment, while acknowledging different types of businesses and conditions along the corridor. Suggestions were made to increase density with second floor housing and new development near Detroit Mercy.

**STREETSCAPE ELEMENTS:** There was general support for crosswalks, bump outs, and pedestrian-friendly features, and more planters coupled with benches and sidewalk cafes. Concerns were stated about tree roots and ongoing maintenance. More frequent pedestrian lighting and signage could be unifying elements along Livernois.

**POLICY:** There is a need to both increase and implement security measures such as the Green Light program and security patrols, as well as increase code enforcement, especially for signs. Trash and cleanliness is a major immediate need. There is a need for more discussion around green infrastructure and the impact of the new drainage fees.

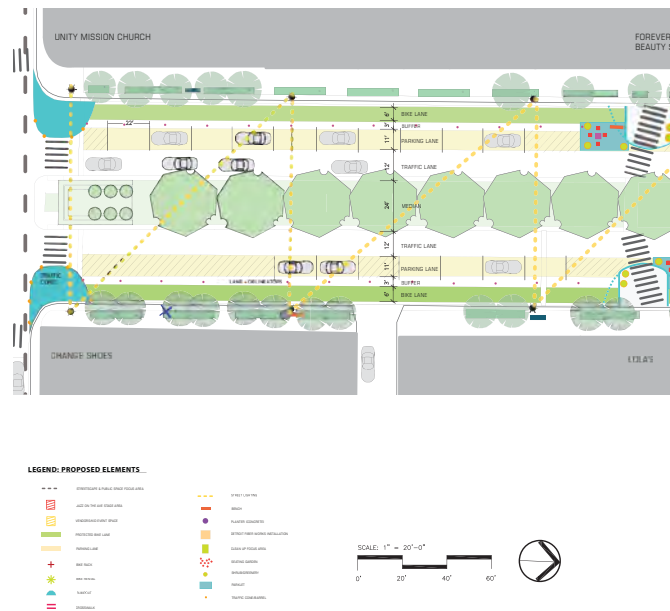
This ambitious streetscape installation covered three blocks on the Avenue of Fashion, from 7 Mile Rd. to St. Martins Ave. It included single direction protected bike lanes along the curb on both sides of the street, painted bumpouts, corner and midblock bumpouts, seating, planters, string lighting and an art installation in the median. The configuration reduced the travel lanes to one lane in each direction. The kickoff of the temporary installation coincided with the annual Jazz on the Ave event, which brings the largest crowds of the whole year to the district for music, sidewalk sales, food and community. The planning team gathered lots of feedback on the streetscape installation at this event. The City decided to extend the streetscape demonstration project, and the bike lanes were left up until late September.



*Midblock crosswalks and bike lane use.*



### Bumpouts and creative crosswalk painting



*Better Block installation map during Jazz on the Ave festival.*

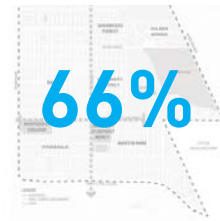


*Temporary bike lane on the inside of the parking lane.*

## > BETTER BLOCK 2 SURVEY RESPONSES

A survey was developed to capture resident and business owner feedback following the completion of the second streetscape installation. The planning team worked with University Commons Organization and the Avenue of Fashion Business Association to get the word out about this survey, which was distributed digitally to residents, community and business associations, as well as via hard copies to Livernois businesses. The survey was open from Oct. 14th- Nov 2, 2016. There was an incredible response to the survey, which captured a broad spectrum of opinions within the community, and helped the planning team understand and address key issues, adjust and prioritize recommendations.

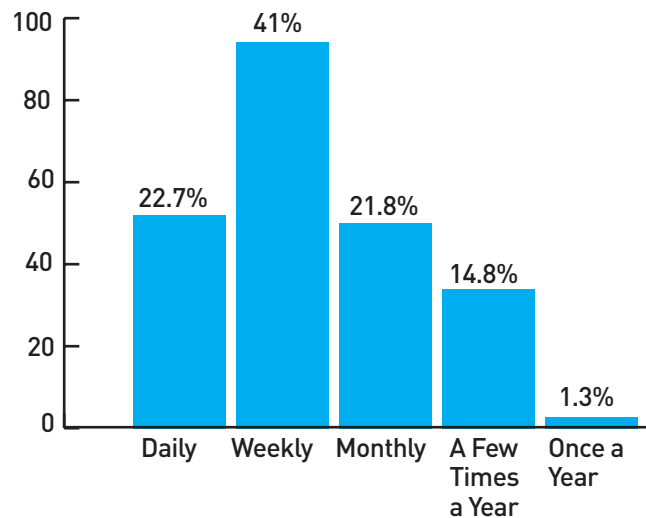
**242** TOTAL RESPONSES



**66%**

FROM ADJACENT  
NEIGHBORHOODS

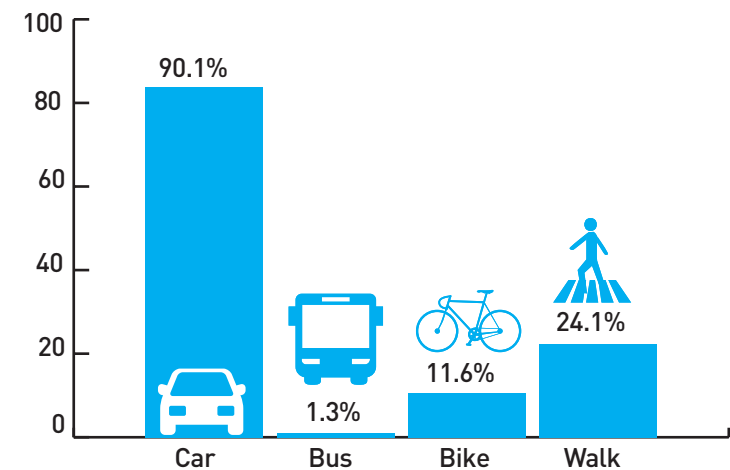
### MOST RESPONDENTS VISIT LIVERNOIS BUSINESSES ON A WEEKLY BASIS



*"I walked from my house to Livernois with my two grandchildren. We visited new shops that I had never entered and I have lived in the neighborhood for 30 years."*

*"The street demo forced traffic to slow down and for people to be able to see my business."*

### MOST PEOPLE DRIVE TO GET TO LIVERNOIS, BUT NEARLY A QUARTER ALSO WALK!

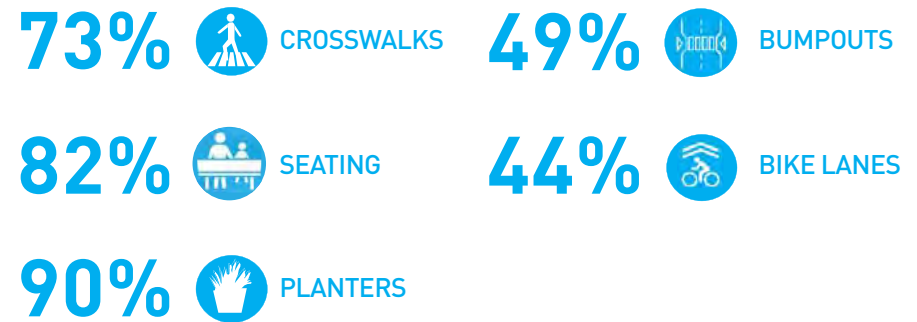




**45%** WOULD WALK OR BIKE MORE IF  
SAFETY IMPROVEMENTS WERE  
MADE

*"I was able to bike on Livernois rather than on the side street. I ride with my son on my bike daily to his preschool in Ferndale and normally have to use the sidewalks until I cross Eight Mile."*

Percentage of respondents that think the following elements are a **GOOD OR GREAT IDEA!** (circled 4 or 5 on scale survey) Please see Appendix for a complete survey results summary.



There were multiple ways to leave feedback at the streetscape headquarters during Jazz on the Ave



A couple fills out the streetscape survey



Feedback sticker method on the streetscape elements

## > MCNICHOLS RD. STAKEHOLDER MEETING // AUGUST 11, 2016

This meeting introduced the Livernois-McNichols Streetscape and Public Space Plan through focused outreach to McNichols business and property owners. Approximately 30 people attended. Feedback was gathered during a [Q&A and discussion period](#) as well as a [mapping activity](#) during which attendees noted and illustrated ways to improve the McNichols streetscape.



*Mapping activity at the McNichols Rd. stakeholder meeting*

### KEY TAKEAWAYS:

**SPEEDING AND AGGRESSIVE DRIVING:** The speed limit on McNichols is 25 MPH, but this is rarely followed. Drivers often use the parking lane to pass on the right. Dangerous driving creates unsafe conditions for pedestrians, bicyclists, other drivers, and property, and was voiced as a major concern. An official traffic study to determine counts/speeds would be useful.

**TRAFFIC CALMING STRATEGIES:** There was a discussion about techniques such as curb bumpouts, more clearly delineated drive and parking lanes, improved crosswalks and tree canopy and how could contribute to traffic calming. Stakeholders were eager to try out these strategies.

**SAFE ALLEYWAYS:** The alleys north and south of McNichols are not well lit, overgrown in some areas, and sometimes used as a public bathroom. Some business owners have taken the initiative to do board-ups, clear the alleyway and install lighting behind their shops. There was a desire for the City to provide incentives for more businesses to make private improvements like this, and also to begin enforcing building codes.

**STREET FURNISHINGS:** The need for additional pedestrian lighting and seating, such as benches and cafe tables was noted many times during the mapping activity.

**BIKE LANES:** There was generally positive support for bike lanes on McNichols. Some people thought having bike lanes might help bring in more business to their shops. Attendees were willing to consider having on-street parking on just one side of the street to accommodate bike lanes, especially if alleyways were improved to provide safe parking options.



## > LIVERNOIS-MCNICHOLS PUBLIC MEETING // SEPTEMBER 27, 2016

This public meeting had over 70 residents, business and property owners, and other stakeholders in attendance. The scope of the project, site analysis, community engagement to date, and overall design strategies including three options for Livernois street design were presented. There was a **Q&A period** and **breakout groups** allowed for facilitated discussion.



*Planning director Maurice Cox speaks at the September 27 community meeting*

### KEY TAKEAWAYS:

**PROCESS IS IMPORTANT:** There were many questions and comments about the planning process. People requested a project website to easily share information, and wanted clarity on the timeline, engagement process and funding mechanisms. People were open to change, but also need time to build trust.

**BIKE LANES:** Bike lanes were supported by many, although not by all. Avid resident cyclists pushed for highly protected bike lanes and were excited for bicycling connections across the corridors and to Palmer Park and beyond. Others wanted to know more about how bike lanes would benefit them and local businesses. Some people were opposed to bike lanes if they took away driving lanes.

**THE MEDIAN:** There was broad support for narrowing, or in some cases removing the median, as well as removing the Michigan left-hand turns and restoring 90-degree left hand turns.

**ONE LANE VS. TWO LANE:** Although the extended sidewalk option was attractive to some, many people questioned whether Livernois Avenue would function well with only one lane in each direction at current traffic levels. Primary concerns related to emergency vehicle access and side street traffic, and were later addressed by the planning effort.

**CROSSWALKS AND BUMPOUTS:** There was broad support for improving and adding crosswalks with curb bumpouts, as well as mid-block bumpouts on longer blocks.

**SIDE STREET TRAFFIC:** Concerns were raised that the transition from two lanes to one lane during the Better Block installation caused an increase in diverted traffic onto residential side streets, on Warrington and Stoepel in particular.

**STREET FURNISHINGS:** Benches, trash cans, and pedestrian lightings were widely supported. In general, attendees thought that street furnishings and plantings should first be prioritized in areas that have thriving businesses to encourage growth of those commercial nodes.

## > LIVERNOIS-MCNICHOLS (REDEVELOPMENT PLAN) PUBLIC MEETING // NOVEMBER 15, 2016

This meeting was led by HR&A Advisors and Hamilton Anderson Associates, who presented their initial findings for the Livernois-McNichols Market Study and Commercial Corridor Redevelopment Plan. Following the presentation, facilitated breakout groups discussed the types of development they would like to see in the corridor. DCDC provided a brief update on planning work, including coordination with the Detroit Fire Department and side street traffic calming strategies. A board that outlined the function, pros and cons the following strategies was presented for feedback: speed humps, curb bumpouts and chicanes. Residents were in support of all of the strategies, but most expressed preference for chicanes, thinking they would be most effective. Several residents also wanted to include gateway islands as a strategy for consideration.

### TRAFFIC CALMING STRATEGIES FOR SIDE STREETS

#### Speed Humps



##### Advantages

- Causes vehicles to slow down.
- Minimal physical changes to street, relatively inexpensive.

##### Disadvantages

- Materials may still speed between humps.
- Need to create snow removal strategy.



#### Bump-out



##### Advantages

- Cars are forced to make slower turns onto side streets.
- Pedestrian crossings are more visible to vehicles.
- Reduces the crossing distance for pedestrians.
- Stormwater management can be incorporated.

##### Disadvantages

- Additional signage may be needed to prevent cars from driving into the bump-out.
- If a few bicycles use the corner, it may be necessary to relocate it to maintain access.



#### Chicane



##### Advantages

- Cars are forced to slow down, if designed properly.
- Emergency response vehicles may prefer chicanes to speed humps.
- Increases the amount of pedestrian space.
- More room for landscaping plantings.

##### Disadvantages

- Reduces the number of on-street parking spots.
- Need to create snow removal strategy.



Side street traffic calming strategies presented at the November 15th community meeting

## > DISTRICT 2 MEETING // DECEMBER 13, 2016

At the District 2 holiday meeting, the City Planning Department and Spackman Mossop & Michaels Landscape Architects presented near-final drafts of proposed design recommendations to approximately 150 people. Boards with existing and proposed sections and perspective renderings and protected bike lane options allowed for one-on-one conversations. The planning team was on hand to answer questions before and after the presentation. In general feedback was very positive, and residents were excited to see the renderings. Questions were asked about timeline and funding mechanisms, and it was requested that the presentation be made publicly available.



District 2 residents and stakeholders listen to the presentation on December 13th

# EXISTING CONDITIONS ANALYSIS

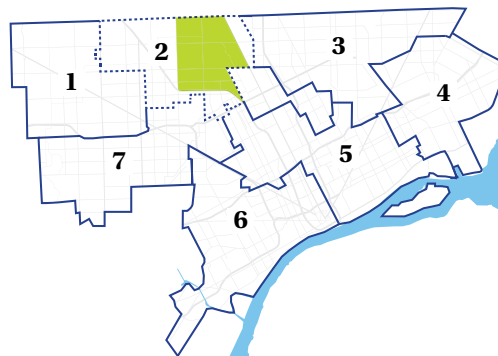


# OVERVIEW

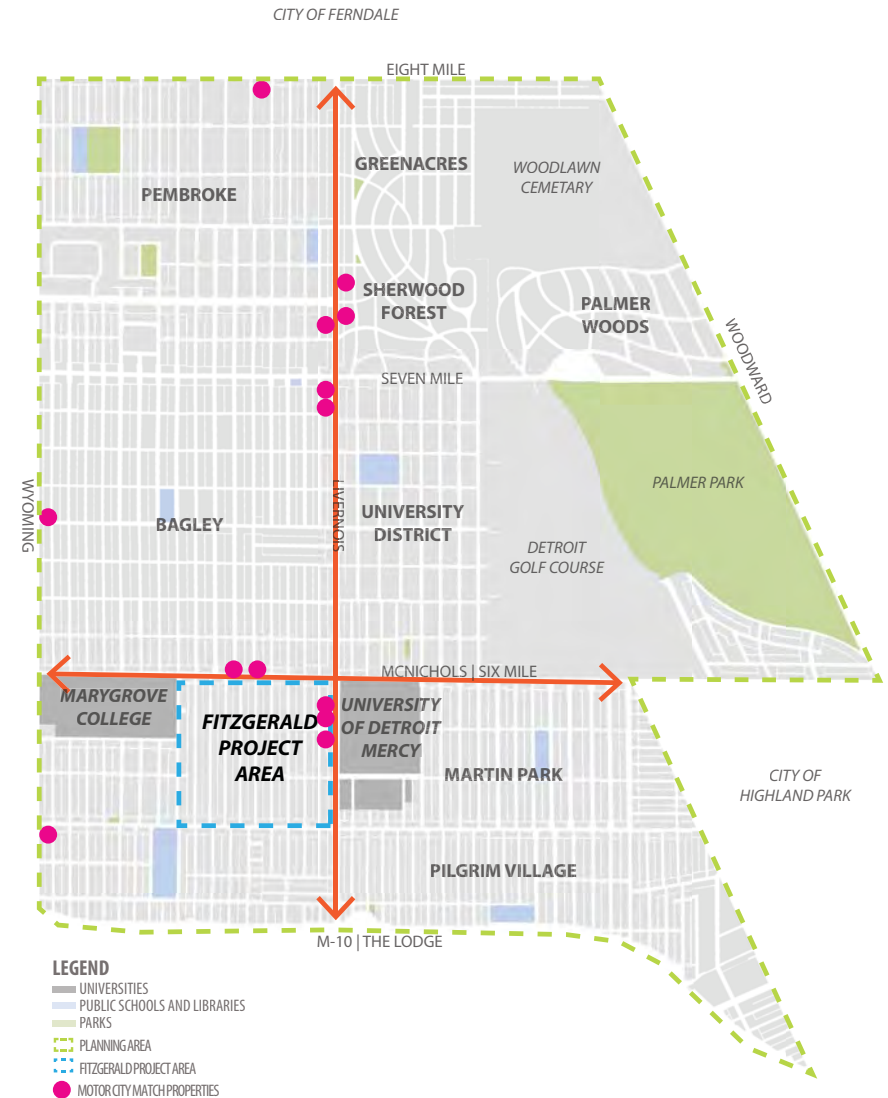
## > PROJECT AREA

This streetscape and public space planning effort is one piece of the City of Detroit's Livernois/McNichols Corridor Revitalization Plan, which is a comprehensive planning strategy focused on implementing a coordinated range of transformative initiatives to address physical, social, and economic challenges in this area of Northwest Detroit. The planning area boundaries are: Eight Mile to the north, M-10/Lodge Freeway to the south, Woodward and Hamilton to the east, and Wyoming to the west. The goal of the plan is to create and sustain a vibrant and attractive community by focusing on the following six key initiatives: Safety and Public Services; Planning and Placemaking; Multifamily Residential Development; Neighborhood Stabilization; Small Business and Retail Development; and Transit and Mobility. This work coincides with an interdepartmental focus in the neighborhoods, which includes Motor City Match investments, as well as, other public and private philanthropic contributions.

This streetscape and public space plan is focused on the commercial corridors of Livernois Avenue and West McNichols Road. These corridors are anchored by two longstanding academic institutions, Marygrove College and the University of Detroit Mercy. Strong and historic neighborhoods, Sherwood Forest, University District, Green Acres, Palmer Woods, Bagley, Fitzgerald, Pembroke, Martin Park, and Pilgrim Village are within the planning area as well. The Detroit Golf Course, Woodlawn Cemetery, and Palmer Park exist adjacent to these neighborhoods. The Fitzgerald Revitalization Project, currently underway, aims to stabilize a quarter-square mile of the Fitzgerald residential neighborhood through housing rehabilitation, blighted structure demolition, the implementation of low maintenance landscapes on every publicly-owned vacant lot, and also includes the construction of a new neighborhood park and greenway.



*Detroit City Council Districts*



**FIG 02** Planning Area



## > LAND USE

Livernois Avenue and McNichols Road are commercial corridors flanked by ten residential neighborhoods. The commercial corridors include a mix of occupied businesses, vacant buildings and lots, institutional and faith-based uses. University of Detroit Mercy (UDM) and Marygrove College are the predominant institutions. Other schools and religious institutions are scattered, and Palmer Park is significant.

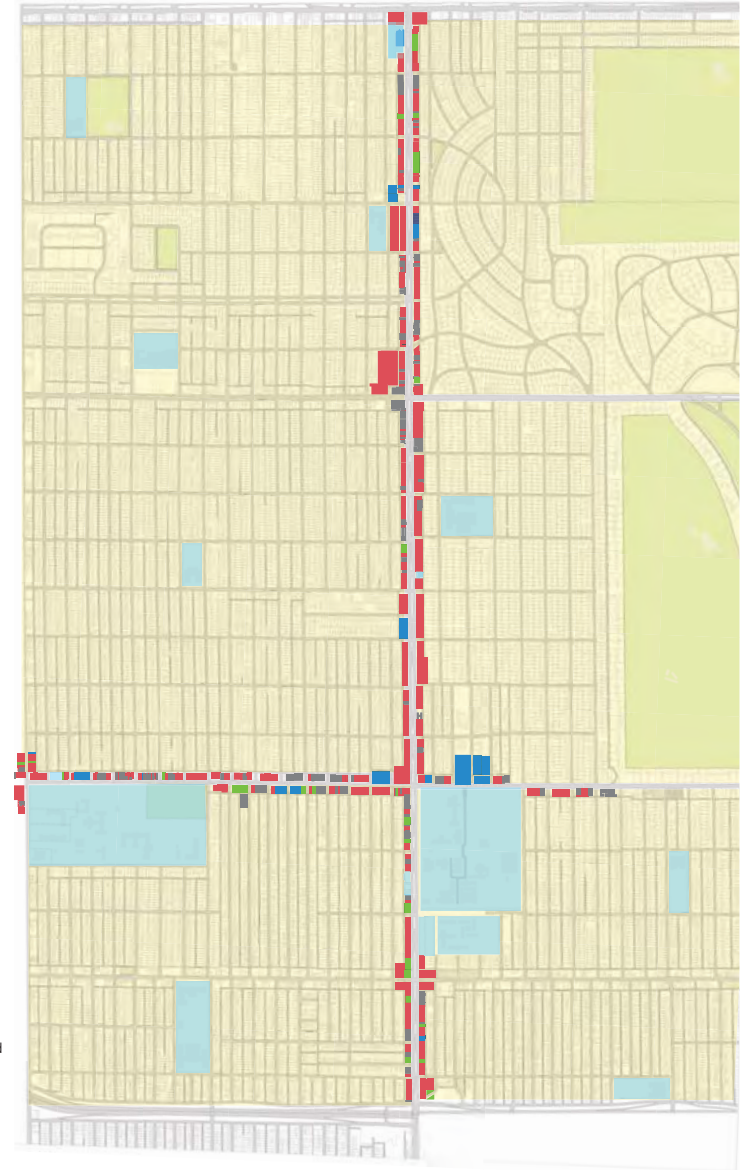
**24%** *of frontage parcels have*  
VACANT BUILDINGS

**7%** *of frontage parcels are*  
VACANT PARCELS

**65%** *of frontage parcels are*  
ACTIVE  
COMMERCIAL

### LAND USE

- Commercial Parcel, Occupied
- Religious Parcel, Occupied
- Residential Parcel
- Institutional Parcel, Occupied
- Parcel with Vacant Building
- Vacant Parcel, No Structure
- Parkland



**FIG 03** Land Use



## > SURFACE PARKING ANALYSIS

Surface parking comprises 33% of the properties along Livernois and McNichols. In almost all cases, surface parking lots are adjacent to the street edge, contributing to an auto-centric character that is unwelcoming for pedestrians. There is also a wealth of street parking along both Livernois and McNichols, which is largely underutilized except in areas of high activities such as the Avenue of Fashion.

**33%** *of frontage parcels have*  
SURFACE PARKING

*over*  
**1,200**  
ON-STREET  
PARKING SPACES

### APPROXIMATE PARKING LOT SPACE COUNTS

- Business parking lot  
Over 2200 spaces
- Semi-public parking lot  
(UDM and Marygrove)  
Over 1300 spaces




**FIG 04** Existing Parking

## > CIRCULATION

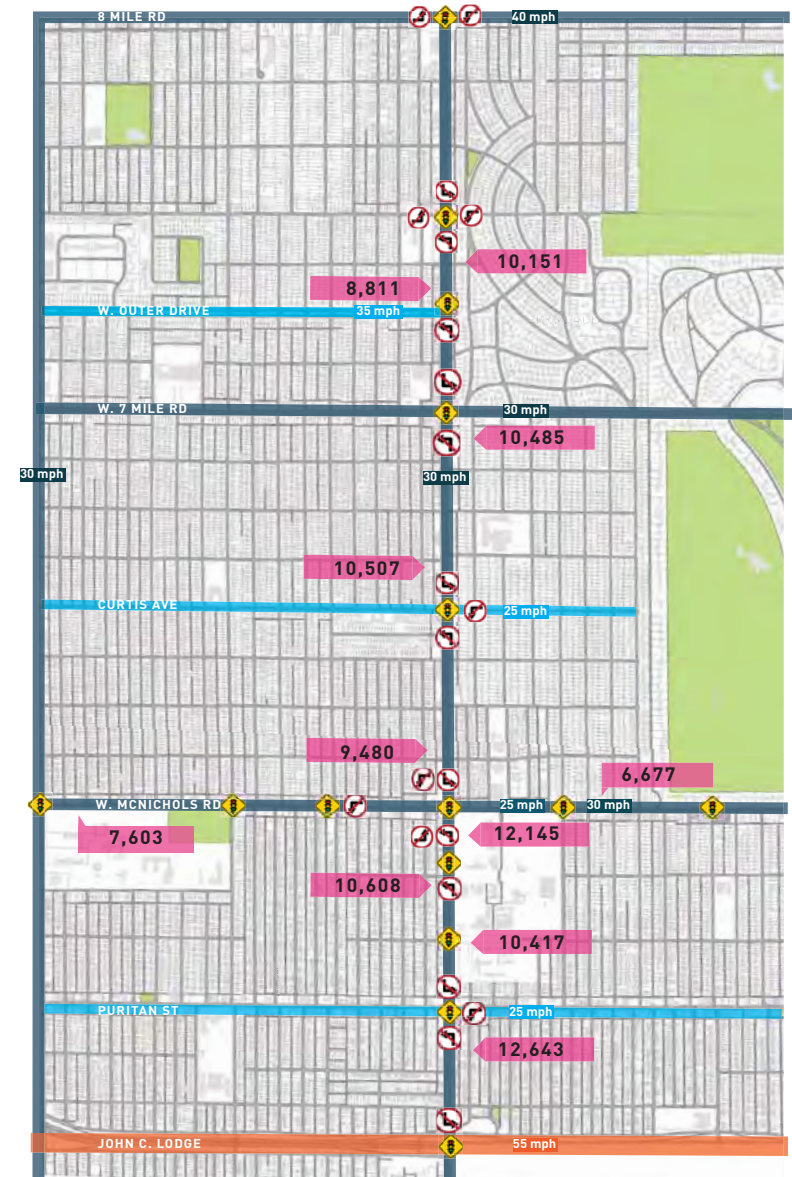
Livernois Avenue is the primary thoroughfare in the area, with approximately 11,000 cars daily. This volume does not warrant the current street design that encourages fast thoroughfare traffic. The Lodge Freeway has an exit onto Livernois which primarily feeds northbound traffic. Secondary routes include Puritan Avenue, Curtis Street and Outer Drive. “J turns” enable left turns along Livernois.

*on average*  
**10,580**  
 VEHICLES TRAVEL  
 LIVERNOIS DAILY  
*(in each direction)*

### CIRCULATION

- Freeway
- Primary Streets
- Secondary Streets
- Tertiary Streets
-  Street Light
-  No Left Turn
- 10,580 Average Daily Traffic\*

\* One direction. Source: SEMCOG, 2012



**FIG 05** Circulation

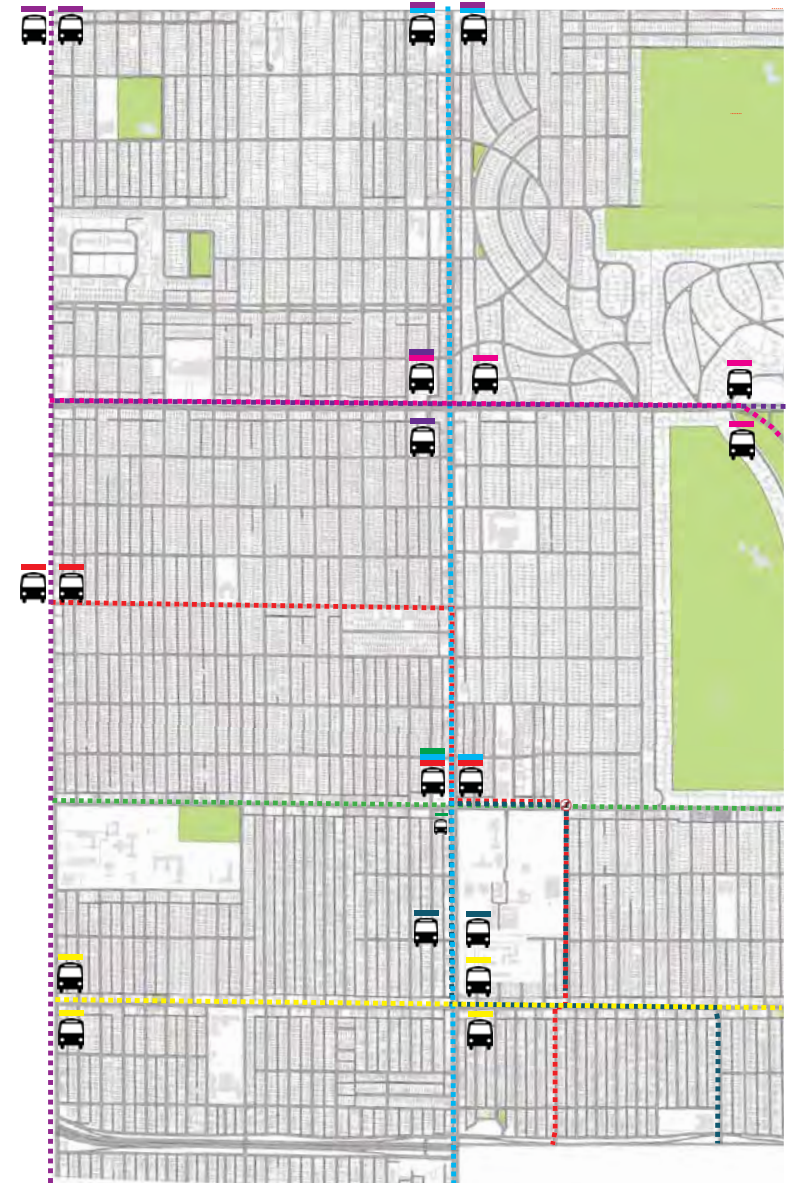
## > BUS ROUTES

All of the primary and secondary streets in the area are serviced by bus routes, but travel times to destinations outside the neighborhood are long. Livernois and Wyoming have north/south lines and Eight Mile, Seven Mile, McNichols and Puritan all have east/west through routes. There are nine routes that traverse this area in total.

**50+**  
MINUTES TO  
DOWNTOWN BY  
BUS  
*(from 7 Mile and Livernois)*

### BUS ROUTES

- ..... Route 16, Dexter
- ..... Route 17, Eight Mile
- ..... Route 23, Hamilton
- ..... Route 29, Linwood
- ..... Route 30, Livernois
- ..... Route 32, McNichols
- ..... Route 39, Puritan
- ..... Route 45, Seven Mile
- ..... Route 54, Wyoming



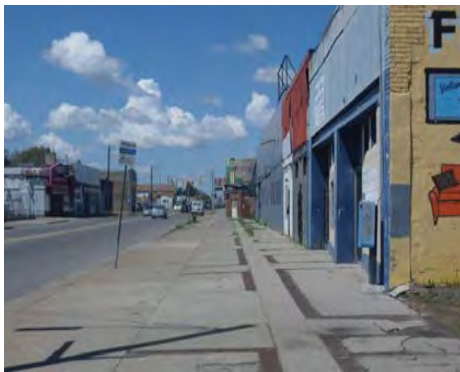
**FIG 06** Bus Routes



# EXISTING STREETSCAPE CHARACTER

## > STREETSCAPE ACTIVITY ZONES

There are distinct zones of activity along Livernois and McNichols that warrant consideration. Zones include areas intended for high pedestrian traffic, more auto-centric businesses, and university edges. Commercial zones adjacent to the universities are prime for mixed-use opportunities. Key intersections along Livernois that warrant design consideration are at McNichols and Seven Mile, as well as the gateways at Lodge Freeway and 8 Mile.



**D** No street trees or pedestrian amenities on McNichols corridor with denser commercial buildings.



**E** Park-like edge along Marygrove College on McNichols Road.



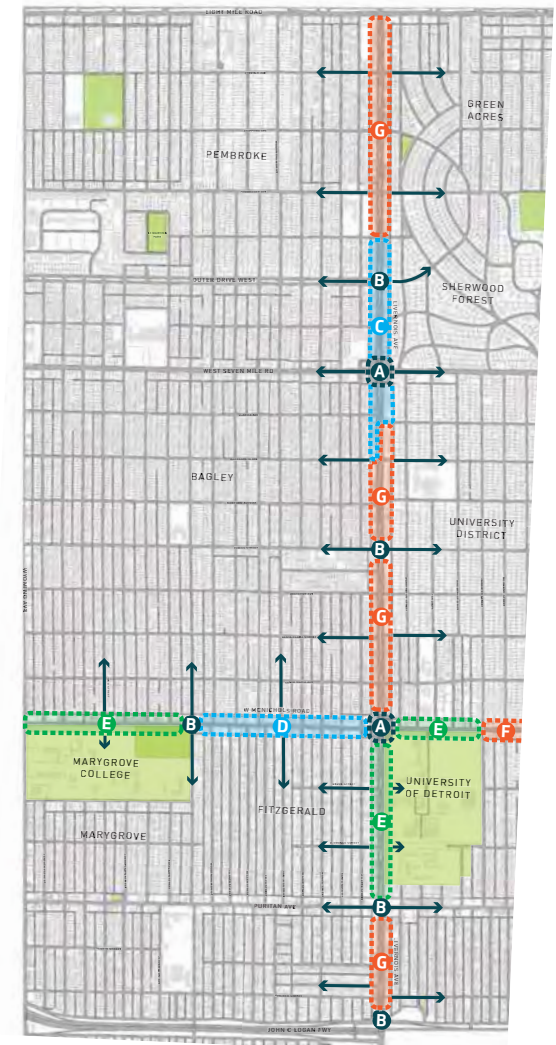
**D** Alleys behind commercial buildings on McNichols could be better utilized for parking if they were cleared, had lighting and other improvements.



**E** Overgrown, vacant blighted properties on north side of McNichols across from Marygrove campus.

### STREETSCAPE ZONES

- A** Major Key Intersections
- B** Minor Key Intersections
- C** Avenue of Fashion
- D** McNichols Neighborhood Retail
- E** University/Mixed-Use Edge
- F** Residential Mixed-Use Edge
- G** Low-Density Commercial
- ↔ Neighborhood Connections



**FIG 07** streetscape zones

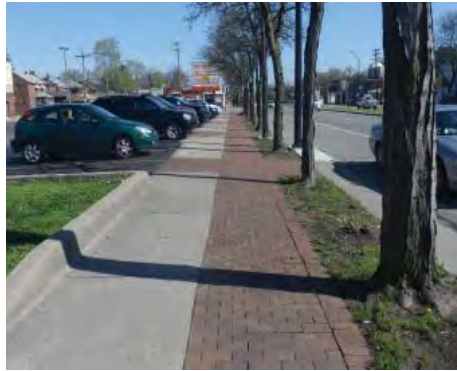
## > STREETSCAPE CONDITIONS

The character of Livernois and McNichols varies along the length of each in the focus area. In the Avenue of Fashion area, street trees, decorative paving and occasional outdoor seating contribute to a sense of place and pedestrian-friendly streetscape. Nevertheless, unmaintained trees, paving and plantings are a challenge. Much of Livernois is auto-centric, with few trees. A fence fronts Livernois along the edge of Detroit Mercy. There are no plantings or pedestrian amenities, a high level of

### GENERAL ZONE CONDITIONS



**G** Fairly healthy and mature canopy of honey locusts on the Avenue of Fashion.



**G** Mike's Market parking lot adjacent to special paving and street trees.



**G** Strip mall with fencing and parking adjacent to sidewalk, on Livernois south of Clarita.



**E** Sidewalk edge with adjacent parking lot, fencing and few trees along Detroit Mercy campus.

### ZONE DETAILS



**G** Cafe seating and long planter beds on Avenue of Fashion leave little room for pedestrians.



**G** Failed paving and bare planting beds on Livernois south of 7 Mile.



**G** Trees in poor condition, planted in cinder block planters and asphalt paved right-of-way.

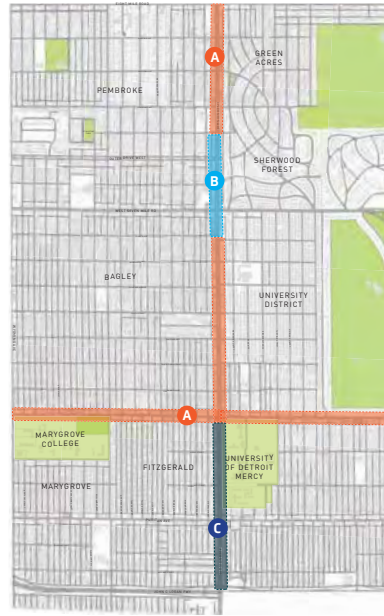


**E** Vacant lot on Livernois across from Detroit Mercy campus, healthy street trees.

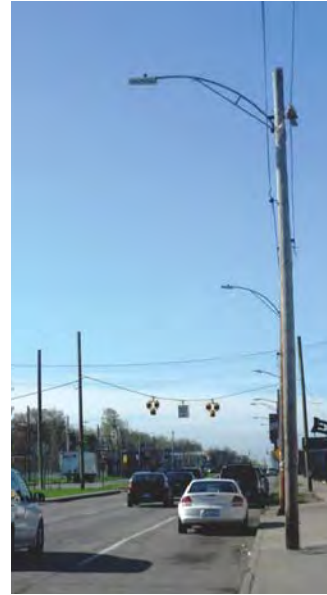


## > STREET LIGHTING

Lighting is not consistent across the corridors, with varying styles, spacing, and placement of light fixtures. Pedestrian lighting is only found in the Avenue of Fashion zone. The other zones only have 25-40' high road lighting fixtures that are not aimed at the sidewalks, which does not contribute to safe nighttime walking conditions. Lighting can also create an ambiance that encourages evening commercial activity and night life.



*Lighting map*



**A** *Wooden utility pole with street lights, no pedestrian lighting*



**B** *Black metal street light with pedestrian attachment*



**C** *New LED street light, no pedestrian lighting*

## > PLANTINGS

The Avenue of Fashion has the most intact tree canopy along the corridor, with fairly mature honey locusts. Planters in the Avenue of Fashion zone contribute to a sense of place but are in disrepair, require more maintenance, and block pedestrian access from adjacent parking spaces. There are few plantings further south on Livernois on the sidewalk edge. McNichols Road has no plantings except along the Marygrove campus.



*Graceful double canopy of trees on Livernois at Outer Drive.*



*Inconsistent plantings and edging of long in-ground beds on Ave. of Fashion.*



*Some trees are in poor condition due to small planting pits and severe pruning.*



*Poorly placed trees and/or planting beds cause damage to the tree.*

# PLAN RECOMMENDATIONS

7

## > OVERALL DESIGN OPPORTUNITIES

With a series of historic and well-defined neighborhoods, two key higher education institutions, and a historic retail corridor, this area holds the potential for economic growth and a high quality of life for the residents in the community. Wide street cross-sections, a collection of retail businesses along the corridor, and an engaged community are some of the key assets of the area.

The strip of high-density retail located at the Avenue of Fashion is an example of successful commercial development along Livernois Avenue that can act as a building block for further retail development. The overall wide street profile of the road offers room to add bike lanes and expanded sidewalks in order to improve the pedestrian experience and bicycle access within the corridors. The long history of the neighborhood as a cultural hub and active retail area is a foundation for the revitalization of the corridor.

Existing alleyways behind Livernois and W. McNichols streets will make it possible to reorient future development to the front of commercial property while keeping the rear of commercial buildings accessible for parking and service. Intersections such as Livernois at Puritan Avenue, W. Seven Mile Road, and Outer Drive West hold the possibility of acting as gateways into the area. This can be accomplished through the use of signage, street treatments to slow traffic, and a focus to increase pedestrian movement, which will communicate the strengthened identity of the newly revitalized area.



Existing businesses along the Avenue of Fashion on Livernois Avenue



Park-like edge along Marygrove College on W McNichols Road

### OPPORTUNITIES

- 1 Area with thriving high-density retail use
- 2 Intersection with a concentration of activity
- 3 Opportunity for parking on streets and streetscape elements in surrounding area
- 4 Potential to re-design Livernois Ave street section to include bike lanes and more pedestrian spaces
- 5 Potential to redevelop existing strip mall
- 6 Curtis Street could serve as a major connector between Bagley and the University District
- 7 Area on McNichols Road adjacent to Marygrove College can be redeveloped for residential use
- 8 Park-like edge on North side of Marygrove College provides visual interest
- 9 Greenway will connect Marygrove College and University of Detroit Mercy through Fitzgerald neighborhood
- 10 Opportunity for new student housing for the University of Detroit Mercy
- 11 University-owned property on west side of Livernois Ave may allow for gateway into Fitzgerald
- 12 Intersection can serve as front door gateway to both universities
- 13 Alley ways behind commercial development on McNichols Road can provide parking and stormwater management
- 14 Potential University of Detroit Mercy street edge redevelopment site
- 15 Possible connections between the University of Detroit Mercy and Marygrove College with University of Detroit Jesuit High School

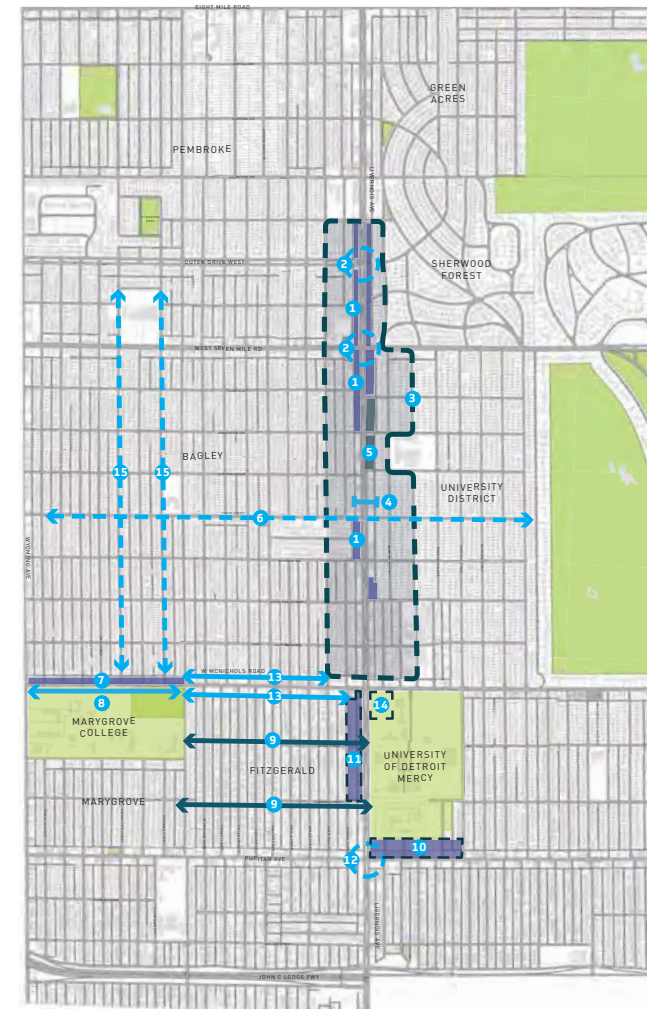


FIG 08 overall design opportunities



## > OVERALL DESIGN CONSTRAINTS

Some of the opportunities in the area, such as wide streets, are in their current state a constraint for growth and development within the neighborhood. Although these design constraints may create various challenges, there is always potential for constraints to be turned into design opportunities once they are identified and a design solution can be formed.

Although currently, much of Livernois Avenue is lined by expanses of parking lots and vacant land, this condition affords much available real estate and land ready to be developed into shops, restaurants, and other businesses. Vacant commercial spaces occurring along W McNichols Road are also opportune platforms for revitalization. On W McNichols, speed limit indications and traffic lane striping are missing which result in increased speeding and dangerous circumstances for drivers and pedestrians. These conditions can be addressed through standard streetscape improvements which will provide safety and improve the pedestrian experience along the road. Campus fencing existing along the edges of both the University of Detroit Mercy and Marygrove College tends to create a barrier between the university and the surrounding community. Local retail shops would benefit from additional commerce received from university students, while the community would benefit from recreational opportunities afforded by the campuses. These design constraints and a few others are diagrammed in the figure to the right.



*Wide streets make crossing Livernois Avenue difficult*



*Vacant retail buildings and deteriorating sidewalks along W McNichols Road*

### CONSTRAINTS

- 1 Large expanse of vacant lots and parking lots along Livernois Ave
- 2 Short lots along Livernois Ave limit development potential
- 3 Many vacant or isolated businesses along McNichols Road
- 4 The speed limit of 25mph and number of lanes on McNichols Road are not clearly marked because of lack of signage and road striping creating excess speeding
- 5 Fences around Marygrove College and University of Detroit Mercy create barriers between the universities and surrounding neighborhoods
- 6 Signage and retail along this portion of Livernois Ave lack streetfront character



**FIG 09** overall design constraints



## > ANALYSIS: UNIVERSITY AND NEIGHBORHOOD CONNECTIONS

The series of neighborhoods and two universities are tied together by a number of major and minor streets. The character, movement, and function of each of these streets is defined by its physical attributes as well as the number of people who use it as a convenient neighborhood connection. Through the assessment of the current use of the roadways in the area, it has been determined which are major driving and bike connections, which are minor connections, and which provide alternate services such as a recreational route through the neighborhoods.

Within this area, Livernois Avenue serves as the major North-South driving and bike connection, while W McNichols Road serves as the major East-West connection. Less prominent major connections include Greenlawn Avenue south of McNichols and Cherrylawn Avenue, W. Seven Mile Road, and Outer Drive West to the north of McNichols. Many other roads including Curtis Street, San Juan Drive, and Canterbury Road cut through neighborhoods and are primarily used by locals of the community. A greenway route offering further bike and pedestrian connection is currently in the process of being implemented in the Fitzgerald neighborhood to connect the University of Detroit Mercy and Marygrove College.

Although both driving and bicycle movement is present on these connections, bicycle use is currently secondary to vehicular use. These major connections would benefit from improved bike infrastructure.

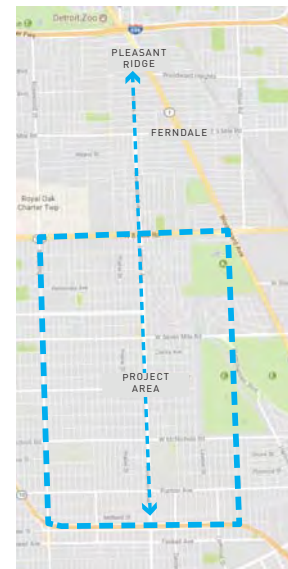


*Vehicles race along W McNichols Road, a biker forced to ride on the sidewalk*



*Livernois Avenue is a main thoroughfare which connects the neighborhoods with greater Detroit*

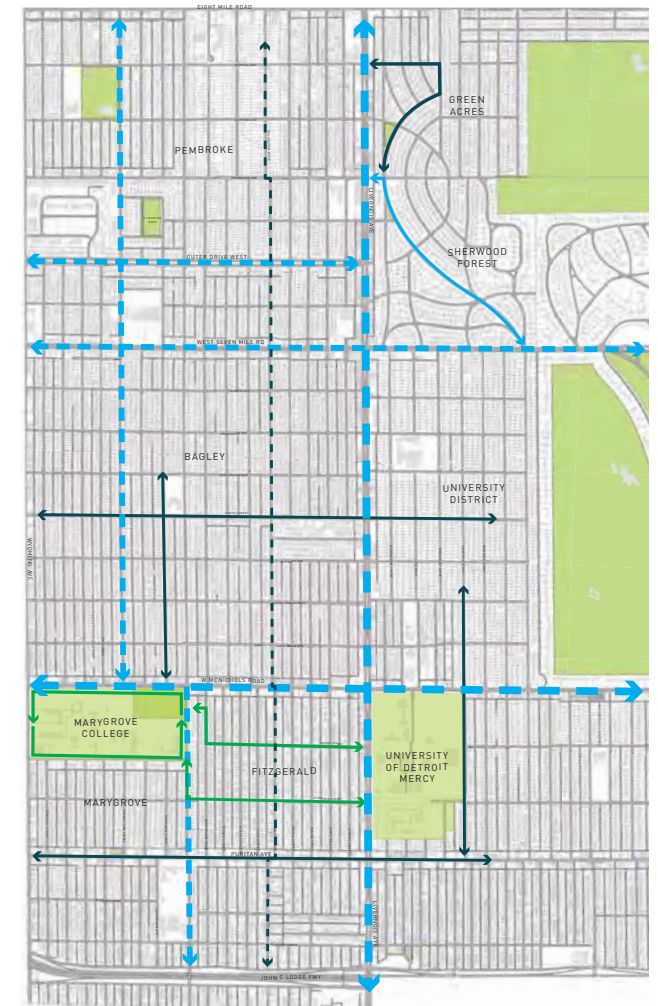
KEY MAP



*Both easy potential bicycle connections, Ferndale is only 1 mile to the North and Pleasant Ridge is less than 2 miles to the North of the project area.*

KEY

- ↔ Existing Major Driving Routes/Bike Connections
- ↔ Existing Minor Driving Routes/Bike Connections
- Recommended Greenway Bike and Pedestrian Routes



**FIG 10** university and neighborhood driving routes/bike connections

## > ANALYSIS: PUBLIC REALM ZONES

Along the major neighborhood connections of Livernois Avenue and W McNichols Road, there are a variety of public realm zones which are associated with the current land use and potential future use. The zones outlined along these two roads are also tied to the streetscape and its role in the community.

Five public realm zones have been determined, along with major and minor key intersections. Most of Livernois can be considered Low-Density Commercial due to how spread out the businesses are in this area. Near the major intersection of W. Seven Mile Road, Livernois offers a compact series of businesses which creates a zone already known as the Avenue of Fashion. From the major intersection of W. McNichols Road south, a University/Mixed-Use Edge occurs at the University of Detroit Mercy. Along McNichols, the University/Mixed-Use Edge occurs along Marygrove College and at the University of Detroit Mercy. From Livernois Avenue to Marygrove College, this area is categorized as McNichols Neighborhood Retail due to the high density of commercial buildings. From the University of Detroit Mercy to the East, the area consists of a Residential Mixed-Use Edge.

Each of these public realm zones will help to define the necessary improvements to be proposed by the design proposals.



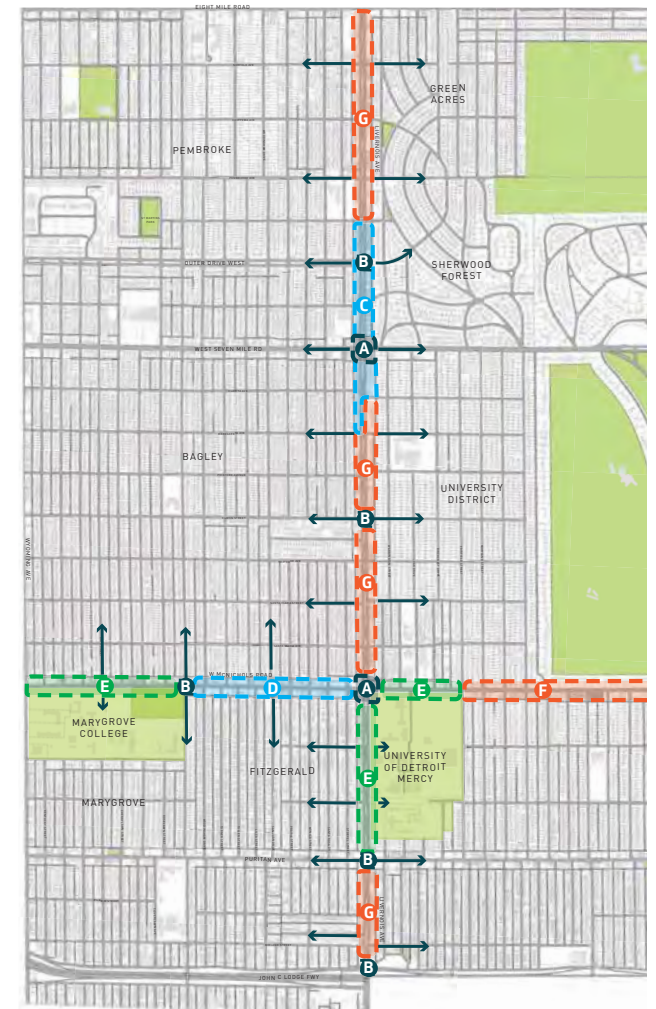
*W McNichols Road offers new businesses a wealth of potential commercial space*



*The Avenue of Fashion is exemplified by a surplus of neighborhood shops, healthy street trees, and the walkability of the streetscape*

### KEY

- A** Major Key Intersections
- B** Minor Key Intersections
- C** Avenue of Fashion
- D** McNichols Neighborhood Retail
- E** University/Mixed-Use Edge
- F** Residential Mixed-Use Edge
- G** Low-Density Commercial
- ↔** Neighborhood Connections



**FIG 11** public realm zones



## > LIVERNOIS AVENUE PROPOSED IMPROVEMENTS

Major intersection improvements are targeted to occur at the intersections of Livernois and W McNichols Road, W Seven Mile Road, and Eight Mile Road. These improvements will include updates to the layout of traffic signals, re-striped crosswalks, turning lanes, curb bumpouts, bus stop improvements, bike lane crossings and turning boxes, as well as, gateway signage and neighborhood branding.

The removal of Michigan left turns are proposed along the whole stretch of Livernois within the project site, from Lodge Freeway to Eight Mile Road. Where a Michigan left turn currently exists, the median will be filled in, and instead, conventional left turns will be allowed. Conventional left turn lanes will be added at every street intersection. Mid-block crossings will be added for a series of blocks in two places: along the Avenue of Fashion and along the University of Detroit Mercy. This design intervention is designed to increase the ability for pedestrians to move from one side of the street to the other. These additional crossings are proposed to occur in these two areas along Livernois where there is the most initial need for greater pedestrian movement.

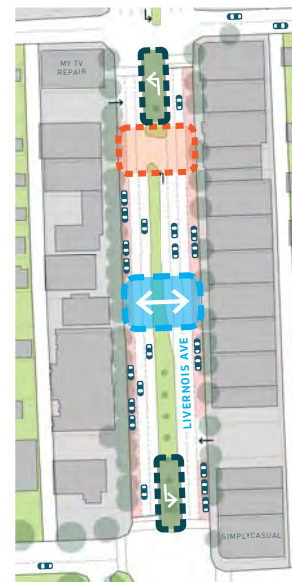


Many are forced to cross between traffic due to a lack of designated crossings



Michigan left turns make navigating Livernois more challenging for drivers

DETAIL PLAN



KEY

-  Intersection Improvements
-  Remove Michigan Left Turns
-  Add Mid-Block Crossings
-  Add Left Turn Lanes



**FIG 12** livernois avenue proposed improvements

## > STUDY AREAS WITHIN THE COMMUNITY

As a result of the overall analysis of the area, including overall design opportunities and constraints, university and neighborhood connections and public realm zones, three study areas have been selected. These study areas have been studied in further detail, through the creation of plan and section drawings, with the purpose of developing informed design proposals for the improvement of the public realm spaces of the Livernois and McNichols corridors.

Within each study area there are common existing conditions including adjacent land use and existing streetscape features and function. Within each chosen area there are also common potential roles and opportunities for the improvement of the streetscape.

Study Area A spans along W. McNichols Road from Livernois Avenue to Greenlawn Avenue and encompasses the McNichols Neighborhood Retail area. Study Area B continues along W. McNichols Road from Greenlawn Avenue to Wyoming Avenue creating the McNichols along Marygrove College area. Study Area C outlines the whole Livernois Avenue section of the project bounds.



*Both the McNichols and Livernois corridors have areas with unique existing conditions and potential design opportunities.*

KEY



**FIG 13** study areas within the community



# STUDY AREA

> MCNICHOLS NEIGHBORHOOD RETAIL

A

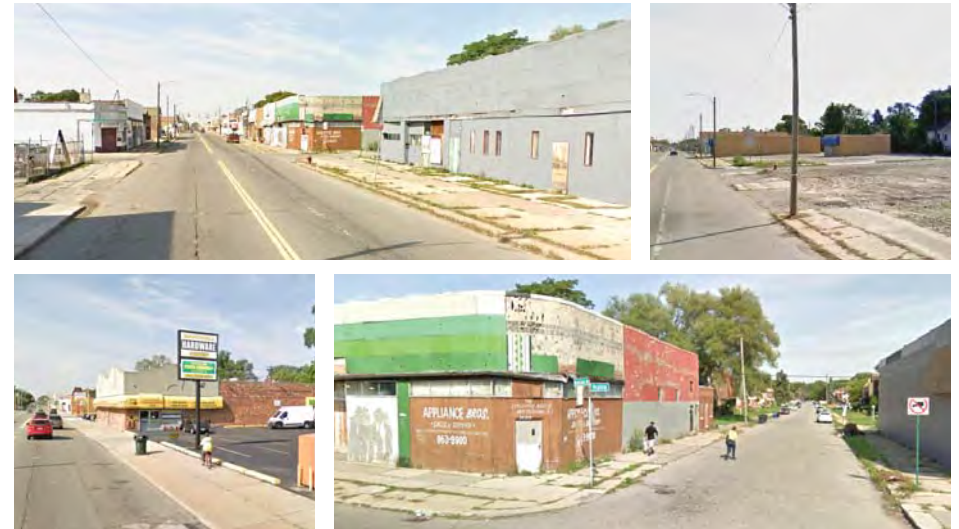


# SNAP SHOT: STUDY AREA A

## > MCNICHOLS NEIGHBORHOOD RETAIL

Investment for the McNichols Neighborhood Retail area is targeted to occur along W McNichols Road spanning for half a mile from the University of Detroit Mercy campus at Livernois Avenue to Marygrove College at Greenlawn Avenue.

Although the University of Detroit Mercy and Marygrove College campuses are relatively close to one another, there is not currently a prominent or highly desirable public space connection between the two. Once a business lined street, W McNichols Road affords a wealth of potential retail and commercial use spaces. Today, many of these retail spaces are vacant and the condition of the streetscape is in neglect. This section of McNichols holds the potential to become a popular retail hub similar to the Avenue of Fashion while facilitating connectivity between the two neighboring universities to the East and West.



Existing images along the McNichols Neighborhood Retail area



Context map



Existing street view of W McNichols Road at Lilac Street looking West



## > EXISTING CORRIDOR LAYOUT

This section of W McNichols Road is currently lacking the aesthetic character and infrastructure that make up a healthy streetscape. Although there are a number of businesses spread along the road, there are many sections of boarded up buildings where local shops and stores have gone out of business. Glimpses of revitalization and private investment can be seen through the establishment of a few successful new businesses. People from the community can be seen walking or riding a bike along the sidewalk, but the street is not currently a lively shopping or dining destination.

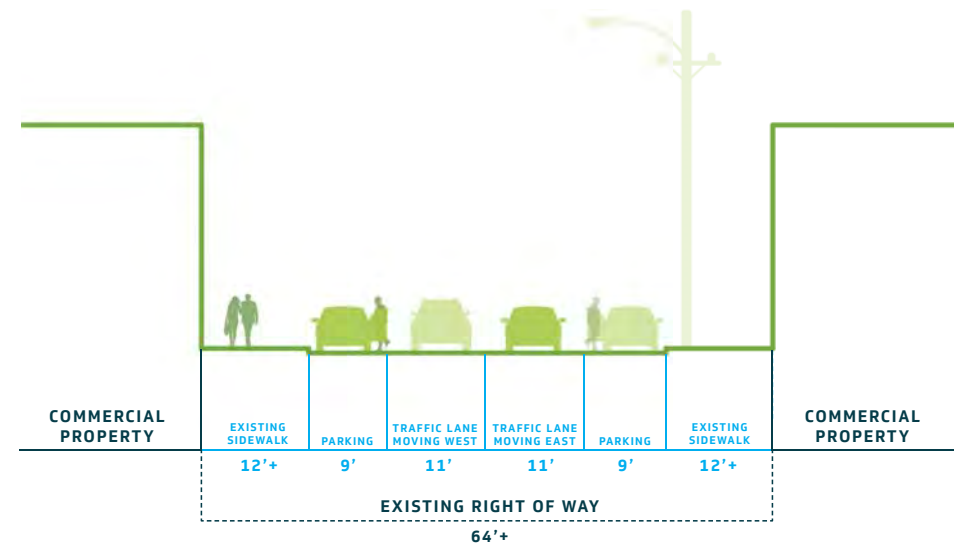
Overall, a strong foundation for greater use of the public realm space and bicycle activity is absent because of the poor condition of the sidewalks and lack of bike lanes. There are cracks in the pavement, make-shift curb-cuts, and an overall lack of maintenance of the sidewalks. Without street trees,

there is no buffer between the sidewalk and the cars that speed along the road. These conditions create an unpleasant experience for those on foot and hinder the road from having inviting outdoor spaces along the road.

A lack of spatial organization of the road increases risks for those who use it. Drivers make the two lane road into a four lane road because the traffic lanes are not clearly marked. This is dangerous for both drivers and pedestrians due to the creation of blind spots and tight spaces. On-street parking is not clearly indicated which can make entering and exiting a parked vehicle more dangerous. The San Juan Drive intersection is the only instance of marked crosswalks currently along the corridor. Unless people are able to easily navigate the street on foot, pedestrians will be less attracted to strolling along the street's retail and public realm spaces.



Existing plan



Existing section

## > EFFICIENT BIKE ROUTE CONNECTION

As part of the design process, various improvements were considered for the enhancement of the public realm space. One of the most important factors of the design is to ensure the viability of the McNichols Neighborhood Retail area to become an attractive corridor between the University of Detroit Mercy, Marygrove College, and the surrounding neighborhoods. Both consideration for improved sidewalk spaces to provide more pleasant routes by foot and for the efficiency and safety of traffic movement within the roadway are key for a healthier streetscape. The addition of a bicycle route within the corridor with bike lanes moving east and west will make the half-mile stretch between the two universities an easy and enjoyable ride. People coming from neighborhoods surrounding the McNichols Neighborhood Retail area will be able to hop on their bikes and ride safely to restaurants, shops, or a community park with ease.



Proposed plan

## > PROCESS ALTERNATIVE: TWO-WAY CYCLE TRACK

During the development of the design, the new streetscape was envisioned to have a two-way cycle track on the South side of the road which would connect to a bicycle and pedestrian shared path proposed to run along the same side of the road at the Marygrove College edge. It was determined that the two-way cycle track would require additional bicycle crossing infrastructure including street crossing signalization which would not be plausible due to cost implications, so the two-way cycle track is not recommended.

As a result, a typical separated bike lane layout will be proposed within this option, with specific design strategies employed for the transition between the McNichols Neighborhood Retail separated bike lanes and the McNichols at Marygrove College two-way shared path.



Proposed section



## > DESIGN PROPOSAL: BIKE AND PEDESTRIAN ORIENTED CORRIDOR

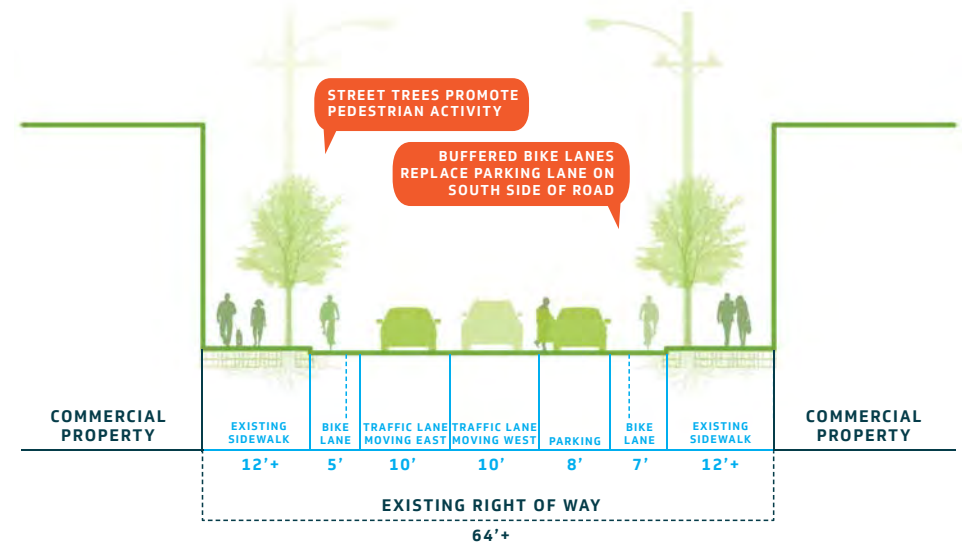
The proposed design will create an updated public space corridor connecting the University of Detroit Mercy, Marygrove College, and the surrounding neighborhoods. The key design changes are the reduction of the width of the traffic lanes, the removal of one lane of on-street parking from the South side of the street, and the addition of various amenities including bike lanes and street trees. A more in depth parking study of the McNichols Neighborhood Retail area is included on the following page.

Two traffic lanes will be clearly striped with widths of 10 feet in order to reduce speeding. The street will include one 8 foot wide parking lane on the North side of the street. Bike lanes will run along both sides of the road moving cyclists safely alongside vehicular traffic to the east and west.



Proposed plan

The sidewalks and street curbs will be replaced creating an even walking surface for pedestrian traffic. Along every block, a row of street trees will create a distinction between the available sidewalk public realm space and movement of traffic along the street. The lush trees will increase the aesthetic character and provide shade to elevate the pedestrian experience along McNichols Road. Crosswalks will be implemented at the point where each side street to the North meets McNichols. To provide greater pedestrian safety, curb bumpouts are used at crosswalks on the North side of the road at the end of the parking lane of each block. Curb bumpouts at the side street corners may also be added to create protected parking lanes along the side streets.



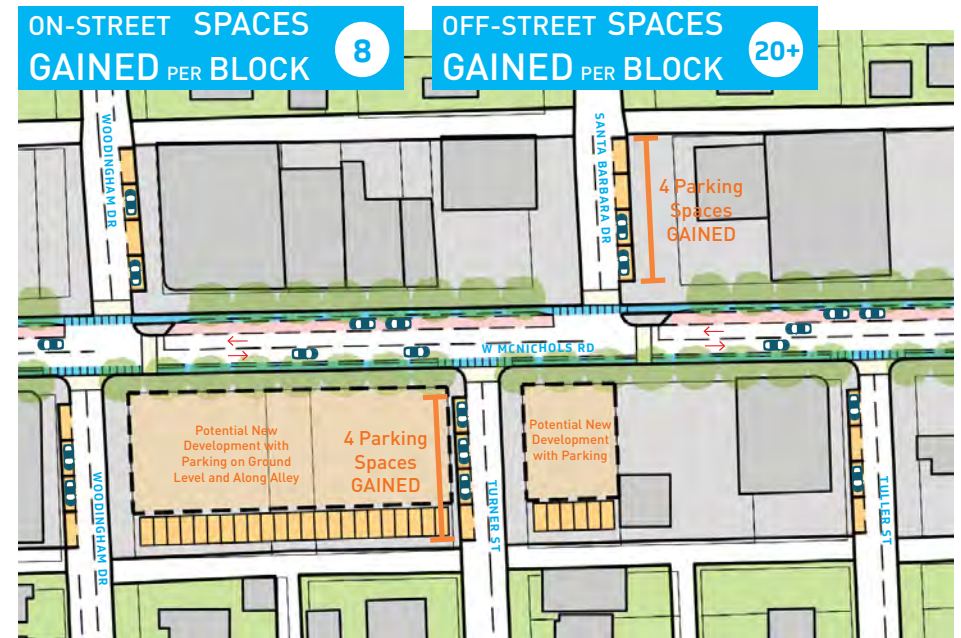
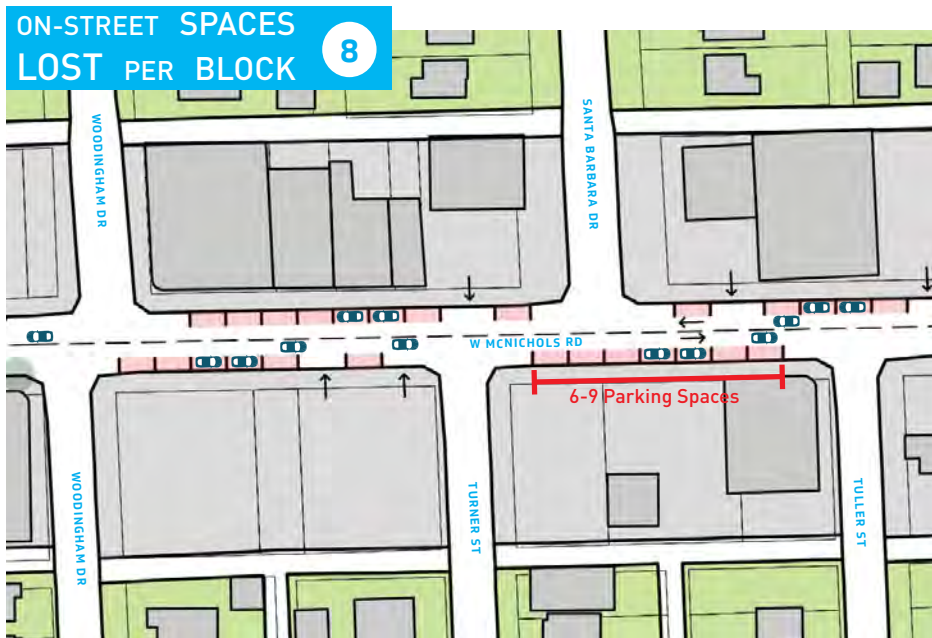
Proposed section

## > PROVIDING PARKING SOLUTIONS

In order to account for an increased need for parking as the McNichols Neighborhood Retail corridor is developed, a parking study was generated to envision potential parking solutions while incorporating the proposed design for the streetscape. With the addition of bike lanes along this section of McNichols, the South on-street parking lane will be removed. Blocks along the South side of the road currently offer between 6 to 9 parking spaces. With the removal of the South parking lane, there will be a reduction of parking by an average of 8 on-street spaces per block. Most side streets to the North and South of McNichols afford space for 4 on-street parking spaces to be added between W McNichols Road and the residential neighborhood. As shown in the diagram on the bottom right, the curbs at the street corners may be extended in order to create a protected parking lane along each side street. Counting the 4 spaces on both the North and South side streets, 8 on-street parking spaces may be added per block to replace the 8 on-street

spaces lost per block along W McNichols Road. This strategy for the addition of side street, on-street parking will ensure that parking stays within the commercial areas and is not needed within residential areas.

Continual growth and investment triggered by the improved McNichols Neighborhood Retail streetscape may produce additional parking needs. As empty lots along the corridor are developed, off-street parking should be added as ground level parking within new developments. In order to create a dense retail corridor, surface parking should be minimized while proposed buildings should be positioned along the front property line to maximize the connection between pedestrians and local businesses. The existing alley running behind the commercial properties of McNichols will allow service access and support the potential for parking spaces to be added in the rear of existing or future buildings.





## > TRANSFORMING THE PUBLIC REALM

Improving the pedestrian experience by providing clear sidewalk space for use by outdoor cafés or retail shops, and rebuilding the character of the street through new trees, lights, and paving will encourage additional retail development along the corridor. Neighboring residents, university students, and visitors alike will feel welcome to stop and experience this section of McNichols. Either for a stroll along the retail strip or a stop in for a snack at a café or restaurant the area will become an enticing new rendezvous point.

Parking spaces may be upgraded into parklet sitting spaces. Café tables and chairs may be added to the sidewalk space between newly planted shade trees. Both bikers and pedestrians will be offered a safe and more pleasant experience along the newly designed McNichols Neighborhood Retail corridor.



The existing street view shows the current conditions of the street in neglect.



The proposed streetscape is proposed to offer an enhanced public realm complete with bike lanes, street trees, and spaces for sidewalk cafe tables.

*The series of underutilized spaces currently existing along W McNichols Road is proposed to be transformed into a continuous stretch of inviting public space.*

# STUDY AREA

> MCNICHOLS AT MARYGROVE COLLEGE



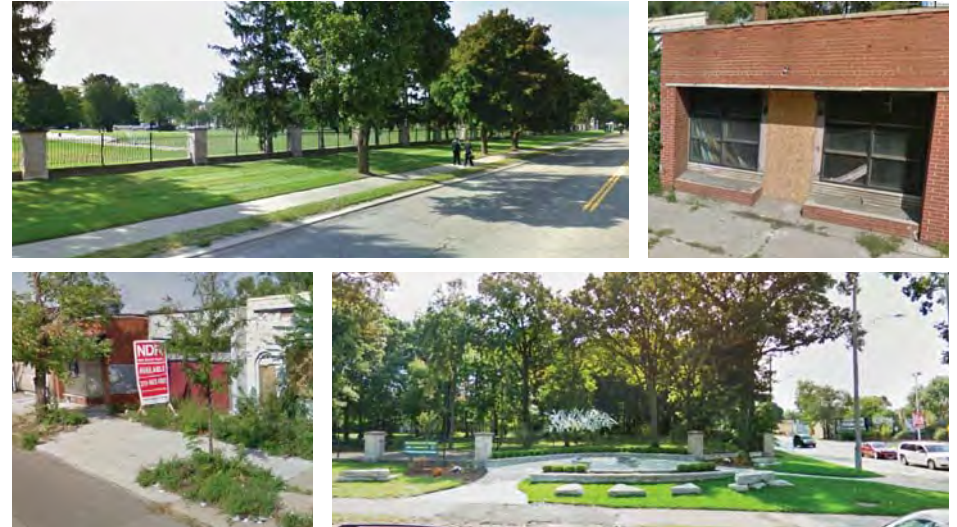


# SNAP SHOT: STUDY AREA B

## > MCNICHOLS AT MARYGROVE COLLEGE

Investment for the McNichols at Marygrove College area is proposed to span just under half a mile from the edge of the Marygrove College campus at Greenlawn Avenue west to the campus edge at Wyoming Avenue. The design will create a consistent typical condition along Marygrove's campus to produce a delineation of function and aesthetic character for this neighborhood corridor.

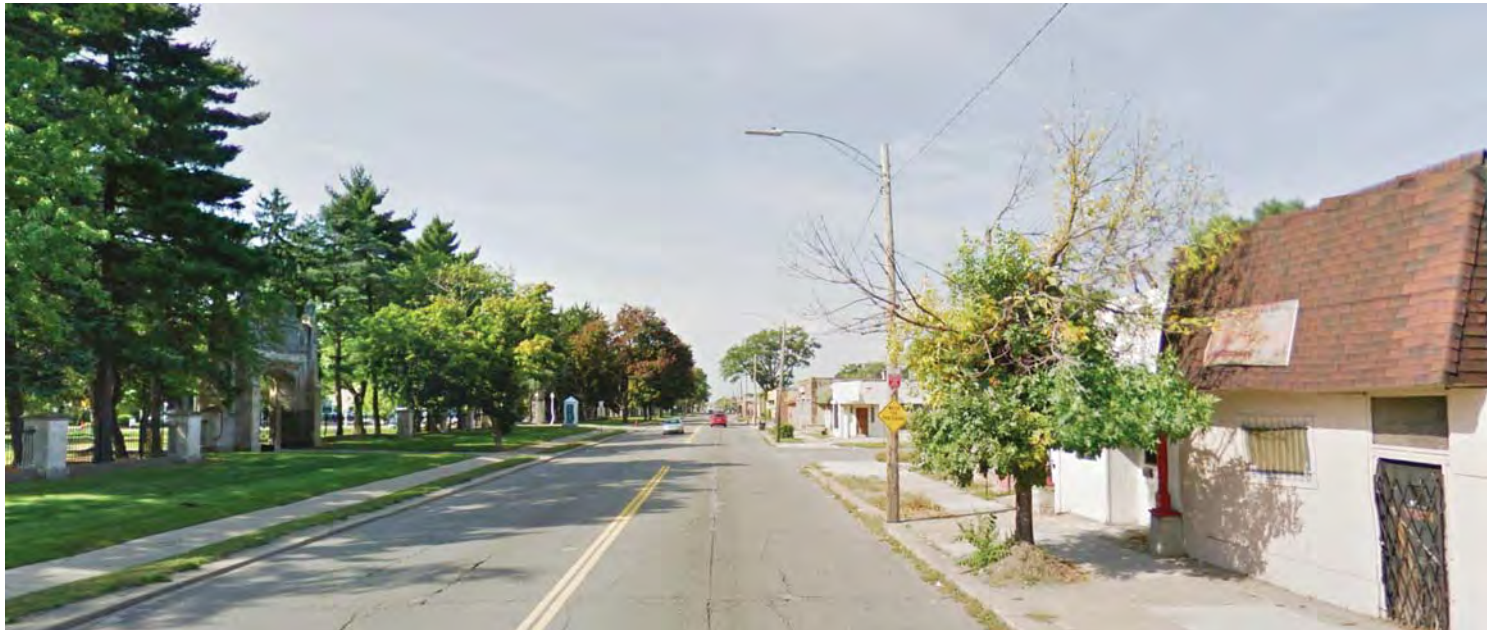
Where currently there exists a divide between each side of the road and its uses, the improvements will seek to bridge the two sides, creating a welcoming park-like aesthetic as the foundation for a stabilized retail strip on one side and a recreational shared path corridor on the other. This redefinition of the streetscape, will boost the economic viability of the area while also strengthening the health of the community around the corridor.



Existing images along McNichols at Marygrove College area



Context map



Existing street view of W. McNichols Road at Marygrove College looking West

## > EXISTING CORRIDOR LAYOUT

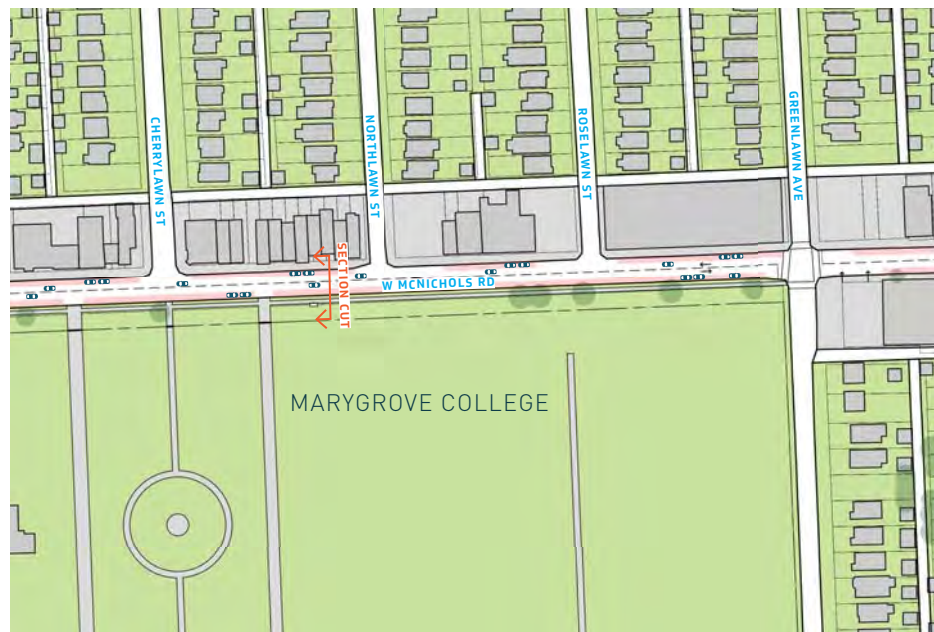
This stretch of W. McNichols Road offers the beautiful, green landscape of Marygrove College on one side, while on the other side unused shop fronts are falling into disrepair. There is a lack of connection between the two sides of the roadway in addition to a surplus of unused space.

On the Marygrove side of McNichols, the sidewalks and lawn are well maintained. Mature trees run consistently along the campus fence which is offset from the road, although along the road the line of mature trees is not as consistent. This area between the existing sidewalk and the Marygrove College fence has a welcoming park-like aesthetic, but most of the space is not currently taken advantage of.

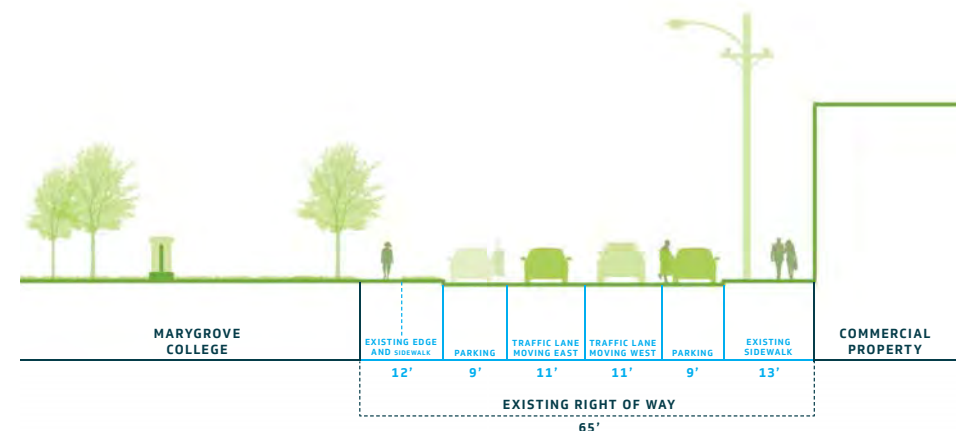
To the North side of McNichols, the condition of both the streetscape and many of the retail fronts has been neglected. Many of the commercial

buildings are boarded up and vacant. The sidewalks are in poor condition, allowing successional plant species to overtake the pedestrian space. The few street trees which are existing are unhealthy and stunted due to a lack of porous soil for their roots to grow into.

Within the roadway, there is a lack of roadway spatial organization which makes the road more dangerous for vehicular traffic and pedestrians alike. Since the two traffic lanes are not clearly marked, drivers often make the road into a four lane road, creating blind spots for pedestrians and unsafe driving and parking conditions along the road. There are no instances of crosswalks along the whole stretch which further plays into the disconnect between the North and South sides of the road.



Existing plan



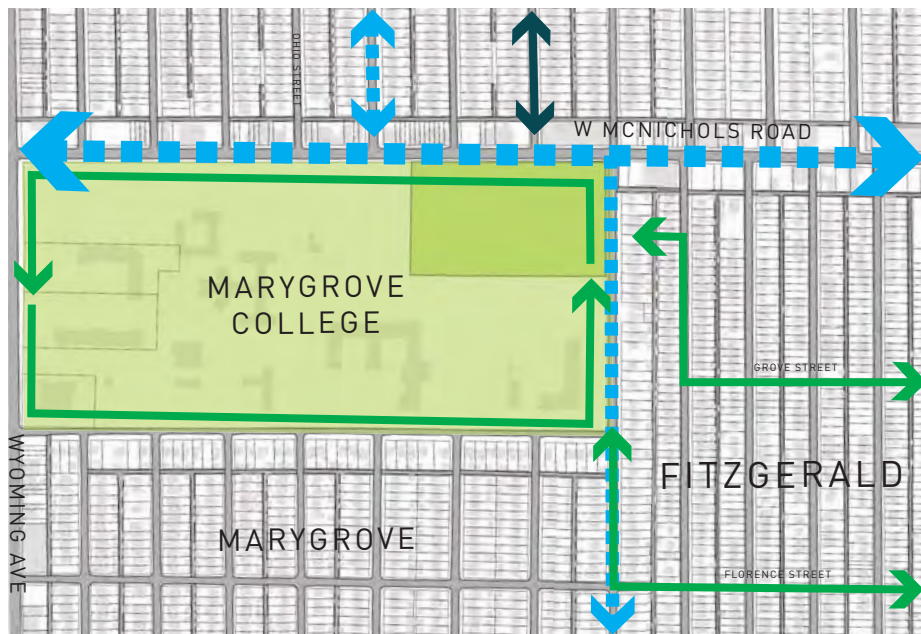
Existing section



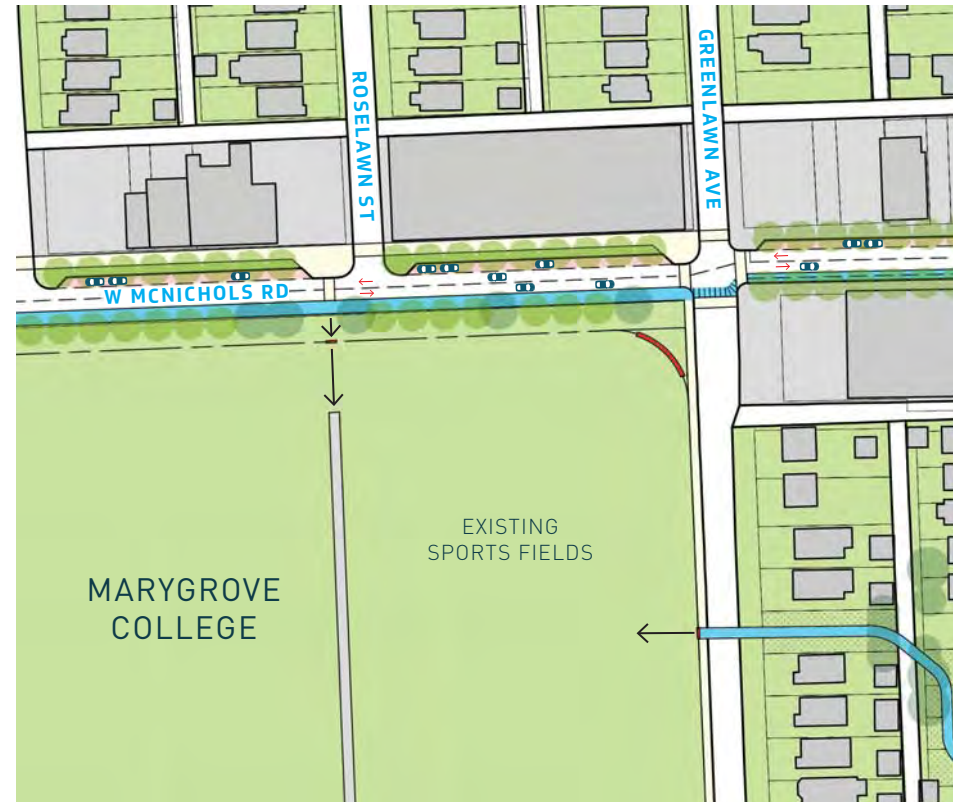
## > GREENWAY LOOP CONNECTIONS AND SIGNAGE

The initial planning process, which has led to the selection of areas of investment and to the overall proposed circulation of the Livernois Avenue, W. McNichols Road, and surrounding neighborhood connections, began with specific goals to provide to the community. These goals include connecting major community anchors, providing alternate modes of transportation, and increasing access to recreational and fitness opportunities.

As shown in the circulation diagram below, the enhancement of the Marygrove College campus by creating a greenway loop along the campus perimeter would supply an extension to the Fitzgerald neighborhood greenway path while also connecting with the McNichols at Marygrove proposed shared path. The greenway loop would provide opportunities for recreation and fitness to the university students and faculty, along with



*Proposed circulation diagram*



*Proposed plan*

local business owners and residents of the community. The loop would also further promote the usefulness of the connection between Marygrove College and the University of Detroit Mercy.

The addition of prominent signage at the Northeast corner of the Marygrove College campus may provide further benefit to the identity of the campus. This addition would compliment the engaging art installation and landscape space at the Northwest corner of the campus by anchoring both sides of the campus to the community.

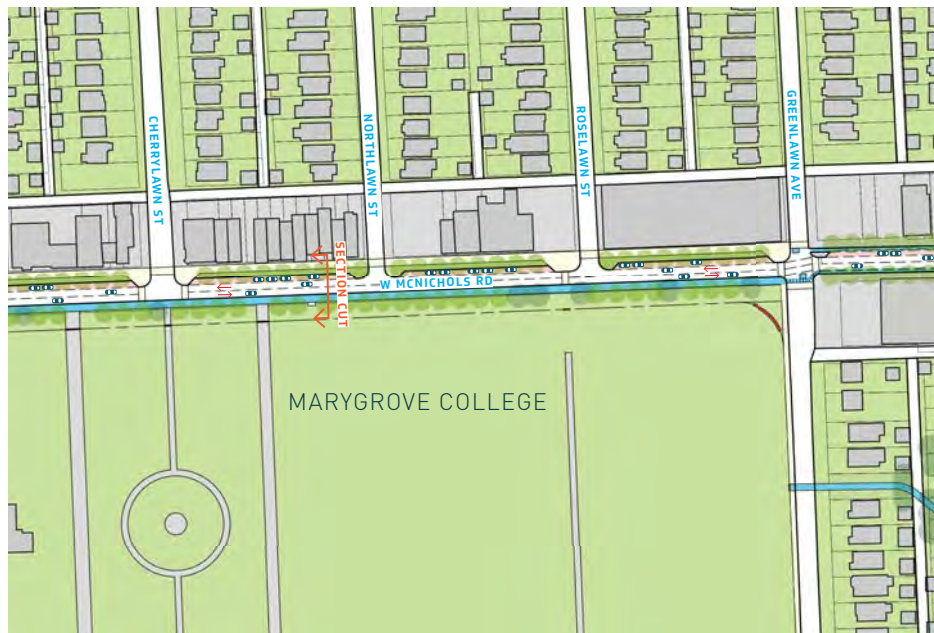


## > DESIGN PROPOSAL: PARK-LIKE THOROUGHFARE

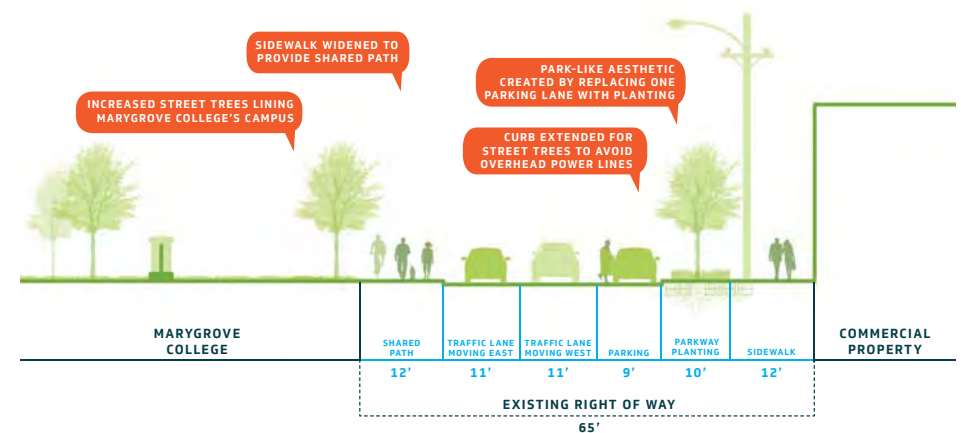
The design aims to create an inviting pedestrian and recreational park-like thoroughfare on W. McNichols Road at Marygrove College. A more desirable streetscape character will be achieved through unifying the North and South sides of the road to create connection between users and function.

Along the South side, a 12' wide shared path will extend along Marygrove College's northern edge, continuing the bicycle corridor, while also taking advantage of the beautiful landscape offered by the space. Street trees will be planted to fill in both sides of McNichols with lush, leafy, green color and texture. Crosswalks and curb bumpouts will help pedestrians cross back and forth from Marygrove College and the proposed recreational corridor to the retail shops and businesses. The clear designation of two travel lanes, as well as a parking lane on the North side of McNichols will produce a safer streetscape. The curb on the North side will be extended to create a series of

generous and lush planting beds. A soil cell system will be used within the planting beds to produce two primary benefits. The structural system will work to protect the tree roots from compaction allowing the trees to grow to a mature, healthy size. The system will also be used as a piece of green infrastructure to capture stormwater runoff from the surrounding sidewalk and roadway then to store, purify, and infiltrate the water into the subsoil. The use of this system will enable the street trees to grow tall and create a healthy canopy over the road. Purposeful changes to the layout of the street right of way will go a long way in improving the function and potential of the space.



Proposed plan

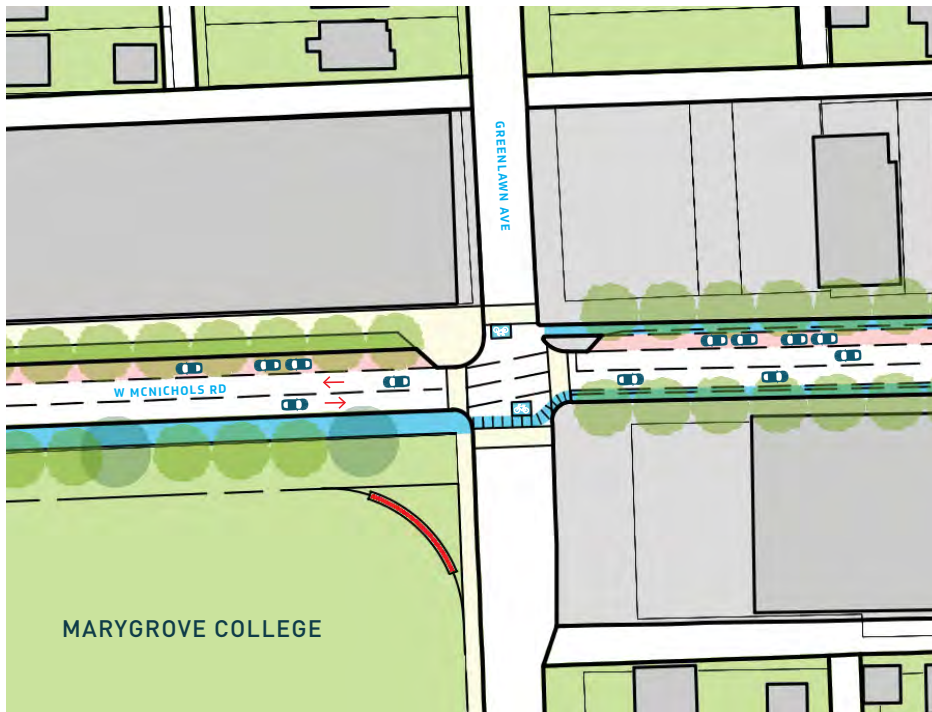


Proposed section

## > BIKE PATH TO SHARED PATH TRANSITION

A unique intersection strategy will be needed where W. McNichols Road crosses Greenlawn Avenue in order to connect the two differing bike lane strategies for the McNichols Neighborhood Retail area and the Livernois at Marygrove College area. The McNichols Neighborhood Retail design proposes separated bike lanes on each side of the road, while the McNichols at Marygrove College design will take advantage of the park-like campus grounds by implementing a shared bicycle and pedestrian path along the South side of W. McNichols Road.

The selected strategy to provide for cyclist crossing at this intersection is the implementation of two-stage turn queue boxes on either side of McNichols.



**FIG 14**

Two stage turn queue boxes are proposed for the transition between the McNichols Neighborhood Retail separated bike lanes and McNichols at Marygrove College shared path.

These queue boxes are indicated zones that cyclists are able to turn into to wait for a safe opportunity to cross to the other side of the intersection. Two-stage turn queue boxes may be used at both signalized and unsignalized intersections. More information as well as precedent images and diagrams about the two-stage turning queue box strategy can be found at the National Association of Transportation Officials (NACTO) website.



Two stage turn queue boxes are used to provide cyclists with a protected zone while waiting for an opportunity to make a turn or cross through the intersection.



## > TRANSFORMING THE PUBLIC REALM

The character of McNichols at Marygrove is designed to be a holistic, inviting space for many activities and users. Street trees will be planted to fill in both sides of the road with lush, leafy, green color and texture. The trees will create shade for shoppers in the summer and a beautiful display of fall color in autumn. On the Marygrove side, a wide shared path would wind under the tree canopy, providing a path for students and community residents to walk, jog or bike.

Designated crosswalks with extended bumpouts would act to slow traffic and enable pedestrians to transition from one side of the road to the other. In the same way, community residents will be able to easily transition between activity within this corridor. The goal is to facilitate varied activities, from recreation and fitness to stopping by a favorite restaurant or shop, to all be located along the McNichols at Marygrove College stretch.



*The existing street view shows the current divide between each side of McNichols at Margrove College.*



*The streetscape can be transformed into a lively park-like corridor with an active shared path to one side and an attractive platform for a prominent retail strip on the other.*

*The goal is to facilitate varied activities to all be located along the McNichols at Marygrove College stretch.*



# STUDY AREA

> LIVERNOIS AVENUE

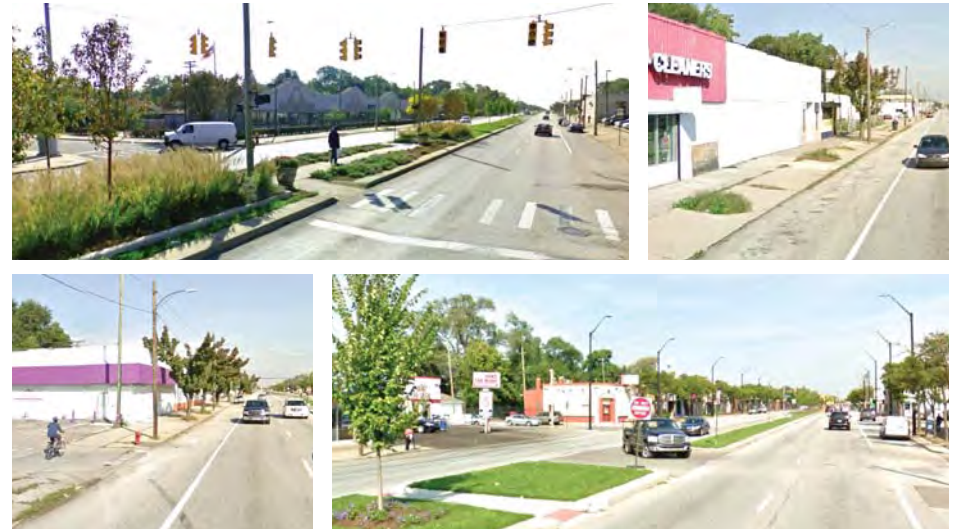


# SNAP SHOT: STUDY AREA C

## > LIVERNOIS AVENUE

The proposed area of investment along Livernois Avenue stretches from Lodge Freeway 2.8 miles north to W. Eight Mile Road. Through analyzing the physical characteristics of Livernois within the study area, three main conditions have been determined to describe the existing use of the road. These three conditions are Low-Density Livernois, Livernois at the University of Detroit Mercy, and Livernois at the Avenue of Fashion.

Livernois Avenue functions primarily as a low-density commercial thoroughfare exemplified by the presence of big-box stores, strip centers, and parking lots along much of the road. This condition is interrupted in two places: where the University of Detroit Mercy adjoins the avenue and at a heavily concentrated area of retail fronts, restaurants, and businesses, known as the Avenue of Fashion. Improvements to the streetscape as a whole will benefit all three conditions along Livernois Avenue.



Existing images along Livernois Avenue



Context map



Existing street view of Livernois Avenue at the end of the Avenue of Fashion looking South



## > LOW-DENSITY LIVERNOIS

Low-Density Livernois refers to low-density commercial use areas which exist roughly from Eight Mile Road south to St. Martins Avenue, from Margareta Avenue to W. McNichols Road, and from Puritan Avenue to Lodge Freeway. Along these sections of Livernois, large warehouses, fast-food chains, retail strip centers, and expanses of parking lots create a scale which is intimidating for the human experience and instead cater directly to vehicular use. Through the implementation of safe and efficient bike lanes on Livernois Avenue, businesses along these commercial areas will have the opportunity to become better connected, and more easily accessed by the community. Much of the real estate which is currently underutilized or used primarily for vehicular use and parking will become opportune land for redevelopment. The economic and social outlook of Livernois Avenue as a whole holds potential for growth as an avenue connected by improved bicycle and pedestrian infrastructure.



Context map



Existing images along the McNichols at Marygrove College area



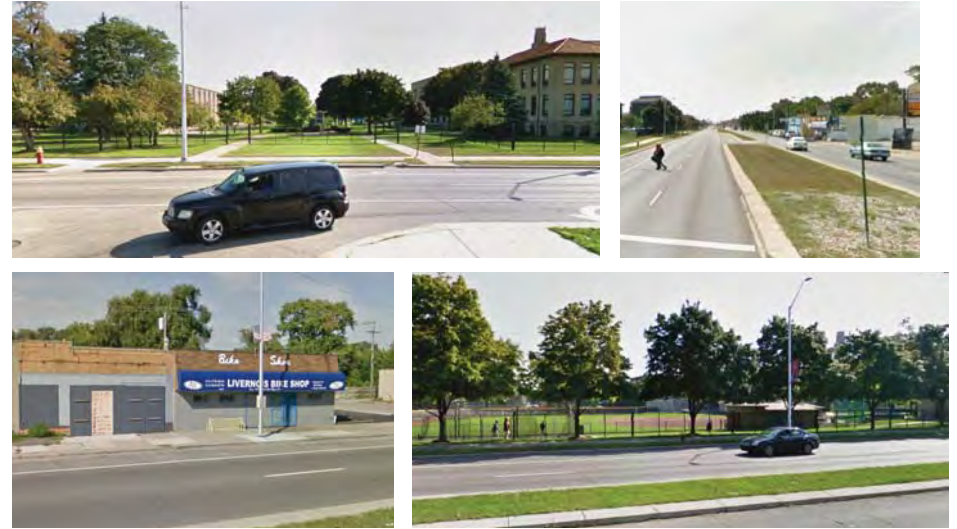
Existing street view of Livernois Avenue at the University of Detroit Mercy looking North



## > LIVERNOIS AT THE UNIVERSITY OF DETROIT MERCY

Livernois at the University of Detroit Mercy exists from W. McNichols Road south almost reaching Puritan Avenue. A beautiful green campus lies to the East, while a mix of commercial buildings and vacant land reside to the West.

University campuses are typically areas with the greatest need for pedestrian and bicycle connection to surrounding restaurants, retail, and housing. Although both sides of the road should be part of the daily lives of the surrounding community, they are partitioned off by the busy, hard to navigate condition of Livernois Avenue, and by other obstructions including poor sidewalk conditions and the absence of accessible paths connecting into the heart of the university's campus. Through the reinterpretation of function along Livernois Avenue, an improved streetscape holds the potential to revive all aspects of the road by connecting users to the resources that Livernois at the University of Detroit Mercy has to offer.



*Existing images along Livernois Avenue at the University of Detroit Mercy*



*Context map*



*Existing street view of Livernois Avenue at the University of Detroit Mercy looking North*



## > LIVERNOIS AT THE AVENUE OF FASHION

Livernois at the Avenue of Fashion runs from St. Martin Avenue south to Margareta Avenue for 5 blocks and includes the intersection of W. Seven Mile Road. Composed of a high density of retail stores, restaurants, and businesses, this area is a clear juxtaposition to most other parts of Livernois Avenue. Here, the character of the streetscape has been developed in greater detail which has played a part in strengthening the physical and social characteristics of the space. A mid-block crossing implemented on one of the blocks enhances movement and connectivity.

Although some attention has been paid to improving the pedestrian experience, both the community and local business owners would benefit greatly by pushing these concepts forward even farther to create a thriving public realm zone along Livernois at the Avenue of Fashion.



*Existing images along the McNichols at Marygrove College area*



*Context map*



*Existing street view of Livernois Avenue at the Avenue of Fashion looking North*

# LOW-DENSITY LIVERNOIS AND LIVERNOIS AT THE UNIVERSITY OF DETROIT MERCY

## > EXISTING CORRIDOR LAYOUT

Low-Density Livernois is the most prominent condition that occurs along Livernois Avenue. In these areas, commercial use buildings are spread along the roadway accompanied by large expanses of parking lots. One of the two interruptions to the low-density commercial condition is where the University of Detroit Mercy meets Livernois Avenue. Here, the university campus creates a lush green edge with sports fields and university buildings along the roadway, but the campus is partitioned from the community by a campus fence as well as the hard-to-navigate condition of Livernois Avenue.

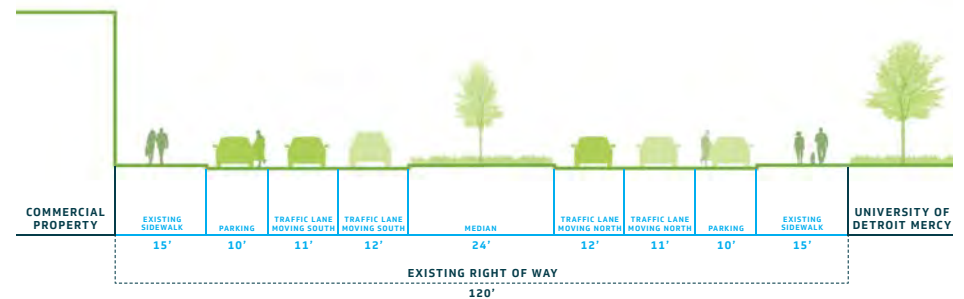
Present at both conditions of Livernois, four lanes of traffic plus a parking lane on each side create a six lane wide street profile which makes crossing

the street an intimidating and risky task. The existing concrete sidewalks are in poor condition in many areas. Street trees are very sparse along the street and are stunted in growth by small tree pits. Vehicular circulation is dominant, minimizing the presence of pedestrian and bicycle circulation.

There exists a lack of pedestrian connection between both sides of the street. Livernois exhibits long block lengths averaging 600' for most blocks and 800' along the University of Detroit Mercy's campus. Crosswalks exist only at intersections. The long distance between crossings as well as a lack of marked crosswalks discourage movement and reduce commerce along Low-Density Livernois and Livernois at the University of Detroit Mercy.



Existing plan



Existing section



## > EXISTING CORRIDOR LAYOUT

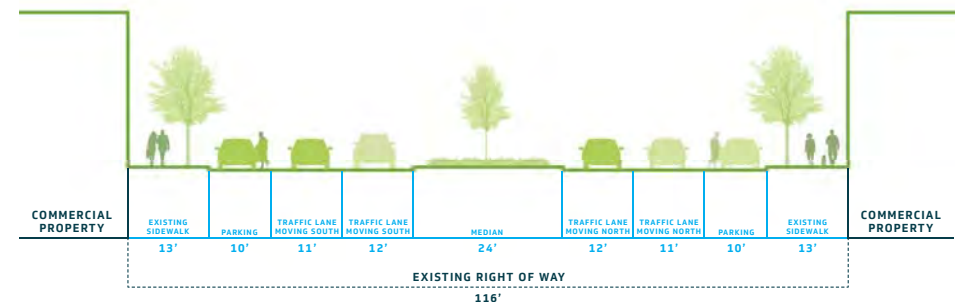
The second area that interrupts the Low-Density Livernois condition is Livernois at the Avenue of Fashion. This area is characterized by small-scale retail fronts, restaurants and businesses consistently lining five blocks of the avenue. The storefront windows and entrances abut the sidewalk creating greater opportunities for engagement between the businesses and pedestrians. The sidewalks, although somewhat outdated, are differentiated from the rest of Livernois Avenue through the use of pavers in addition to concrete paving. Large tree planters with breaks only large enough for pathways to parked vehicles divide the 15' wide sidewalks. Consequently, the sidewalks may only be used for pedestrian circulation and are not wide enough for local businesses to create outdoor café seating in the public realm

space. Some tree planters are well-maintained by the adjacent business, while some are neglected resulting in undesirable plots of overgrown vegetation. This produces an aesthetic that is inconsistent along the avenue.

Consistent with the rest of Livernois Avenue, the road at the Avenue of Fashion exists as a four lane road with one lane of on-street parking adjacent to the curb on each side. Street crossings exist primarily at intersections, but is also possible at a mid-block crossing on one block. Although pedestrian activity is promoted because of adjacent storefronts and street tree cover, the wide streets and lack of crossing opportunities along the road limit pedestrian flow from one side of the street to the other.



Existing plan



Existing section

## > 3 ORIGINAL PROCESS ALTERNATIVES

As design considerations to address the challenges of the road and achieve the goals for the improved function of the new streetscape began to unfold, three distinct design options were formed.

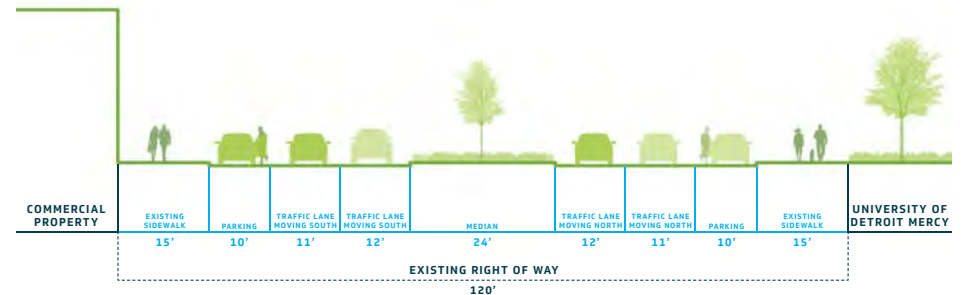
Each of these design options were explored through plan and section drawings, comparing the existing conditions to the proposed. All three options introduced protected bike lanes, retained parking, proposed bumpouts at intersections, and added street trees within new sidewalks. The variations of the options included decisions for reducing the median, reducing vehicular traffic to one lane in each direction, and the extension of the sidewalks. At least one or a combination of these streetscape variations were necessary to accommodate the width of the added bike lanes.

All three options were also presented before the community and stakeholder meetings. Feedback from those meetings helped to inform the design process. Using the comments and concerns of the community members and stakeholders, a number of design studies were produced in order to test and further illustrate the function of the designs. These studies included options to designate a devoted bus rapid transit lane, options experimenting with the layout of the parking lane along the sidewalk versus along the median, street intersection treatments, bike lane crossings, right hand turn lanes, bus stop locations, and emergency vehicle assess studies.

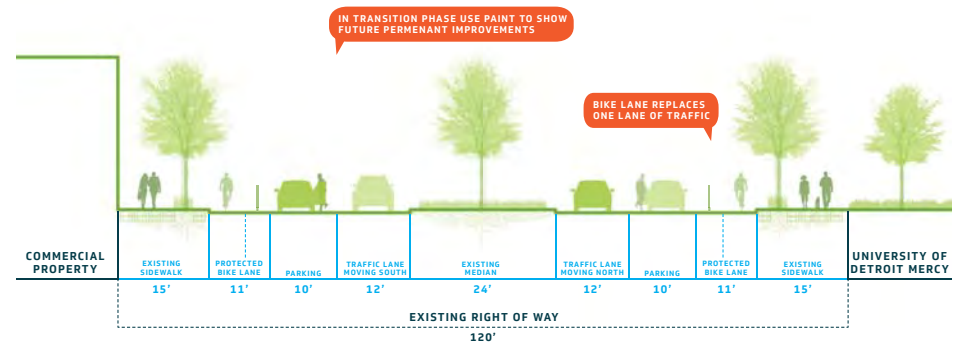
Overall cost estimates were calculated for the implementation costs of the three original alternatives. The cost estimate results along with important feedback from the community and stakeholder meetings helped to narrow the three design alternatives down to two: Options A and B. The results from the design studies and community feedback has also helped to shape the final two design option proposals for Livernois Avenue. This section includes a greater look at all three original design alternatives along with the studies that have shaped the final design proposals for Options A and B.

## > PROCESS ALT #1: WOULD MINIMAL ALTERATIONS WORK?

The first alternative proposed minimal design intervention on Livernois Avenue with the purpose of maintaining certain elements of the streetscape, including the wide median, while providing the most basic desired improvements. This strategy was intended to be the lowest cost option. The proposed improvements could be implemented solely through the re-striping of the roadway and the addition of delineator poles. This alternative proposed to decrease the vehicular traffic lanes to one lane in each direction. The on-street parking lanes would then shift out away from the sidewalk to open up space on the street for an 11 foot protected bike lane on each side of the street. The bike lane would have a 7 foot wide travel area with a 4



Existing section



Proposed section

The first alternative design sections illustrate the most minimalistic proposed improvements for Livernois Avenue.

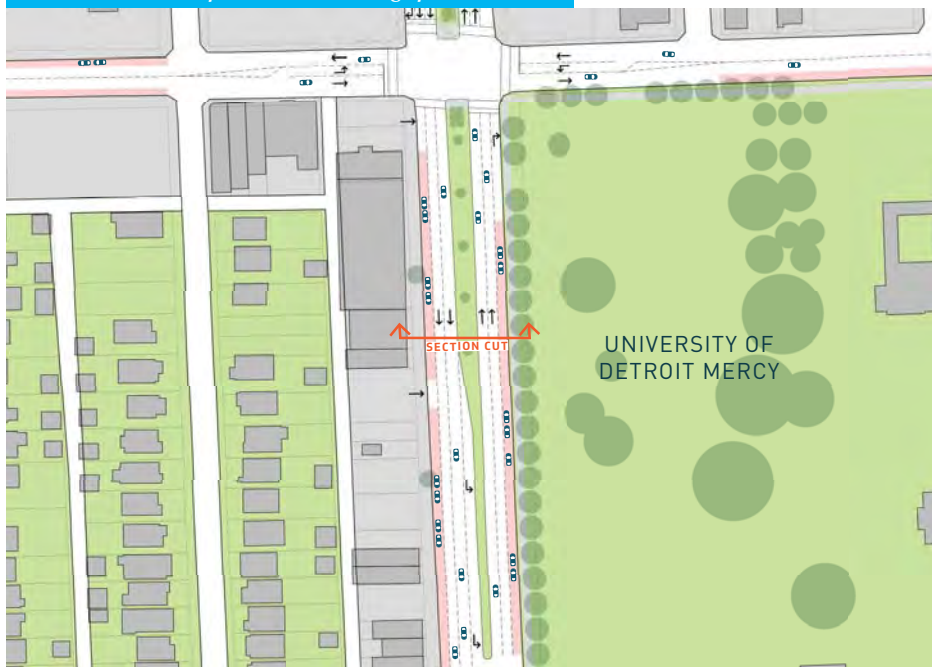
foot wide striped buffer area. The buffer area would be necessary to ensure that people exiting parked cars would have plenty of room to open car doors without injuring passing cyclists. The sidewalks would be repaved, but would remain 15 feet wide with added street trees spreading along each side of the street. The 24 foot wide median would remain the same width, although, in order to remove Michigan left turns and their existing long turning lanes, along with adding new turning lanes at street intersections, much of the new median would need new curbs to be added. Within the new median, street trees would fill the length of each block.

Once preliminary cost estimates were calculated for all three alternatives, the first alternative which was designed to be the least expensive option was found to be of a similar cost to the second alternative. Compared to the second alternative, which would include the opportunity for a future bus

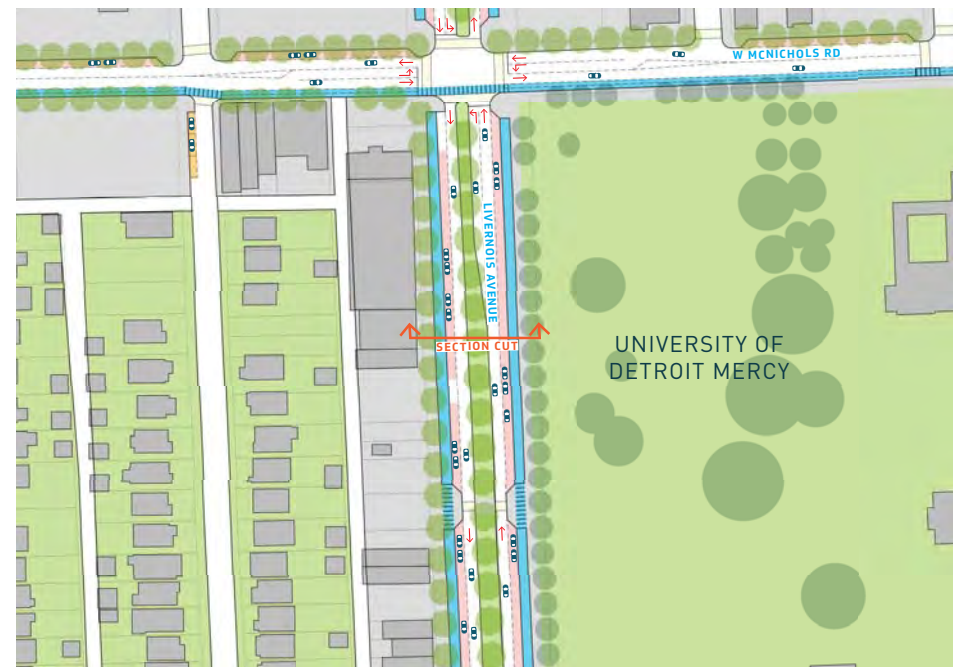
rapid transit lane, the improvements included in the first alternative would have a less significant impact for the same price. Within the same time frame, feedback from community and stakeholder meetings provided additional challenges for the feasibility of the first alternative. Both the community members and fire department had concerns that emergency vehicle access would be blocked because cars would have nowhere to pull out of the traffic lane to allow emergency vehicles to pass. With the alternative to maintain the median at the current width, the available width for travel lanes does not meet the 20 feet minimum width for the fire department vehicles to pass.

In the end, the first alternative was found to be a less justifiable use of capital investment when compared to the other two alternatives, and also was found not feasible for the function of Livernois Avenue. Consequently, the first alternative was negated from the proposed options.

#### The most minimal impact alternative design plans



Existing plan

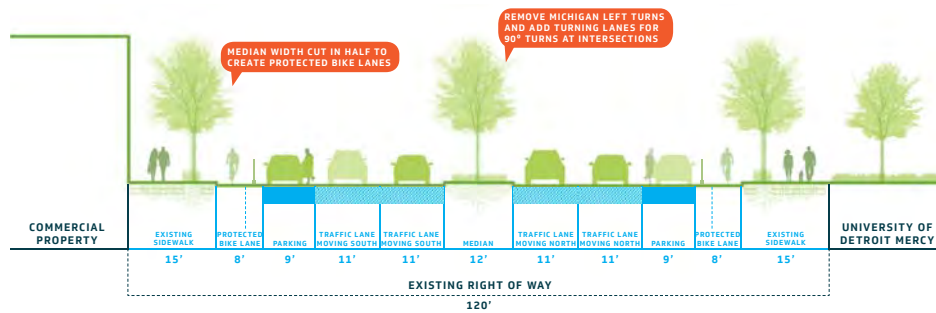


Proposed plan

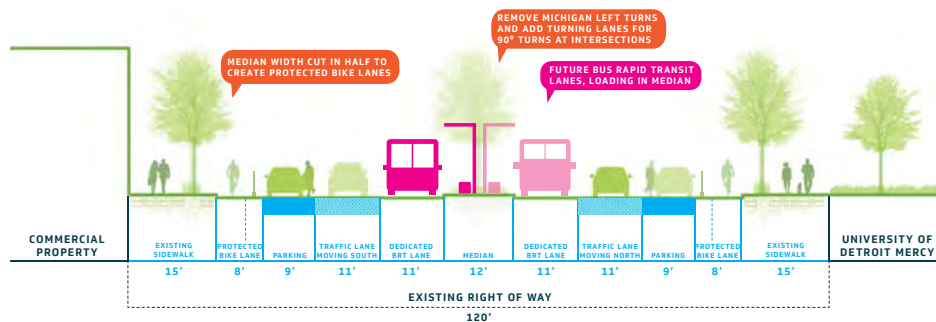


## > PROCESS ALT #2: MULTI-MODAL TRANSIT STREETSCAPE

The second alternative proposed adding similar streetscape improvements as the first, but relied on the reduction of the median width in order to add a protected bike lane and keep two lanes of vehicular traffic. The median width would be reduced by half to 12 feet wide. Two traffic lanes moving in each direction would be narrowed to 11 feet as a traffic calming measure. An 8 foot bike lane, with 5 feet for travel and 3 feet for the buffer area, would be protected by the parking lane positioned between the bike lane and traffic lanes. The sidewalks would remain at 15 feet in width, but would be shaded by a new row of street trees on each side of the street. The second alternative afforded the possibility of creating a future bus rapid transit (BRT) lane out

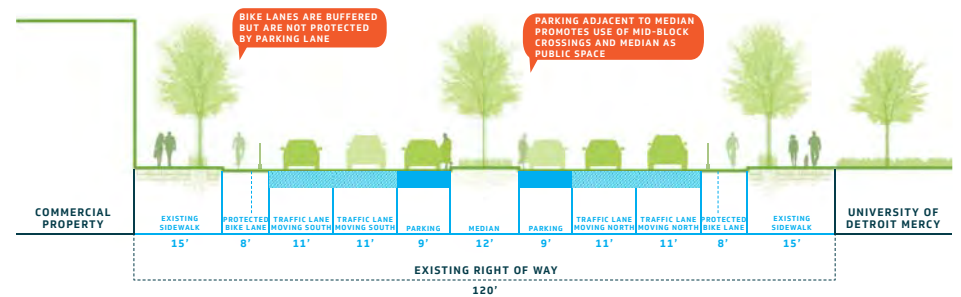


Existing section

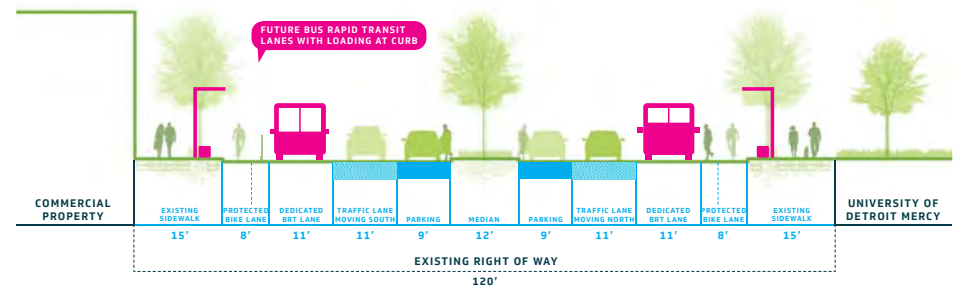


Proposed section

Studied in alternative two, future bus rapid transit lane could replace the second traffic lanes with the potential to locate bus stops in the median.



Existing section



Proposed section

Studied in alternative two, by locating the parking lanes along the median, a future bus rapid transit lane could run adjacent to the bike lane and have bus loading and stops along the sidewalk.

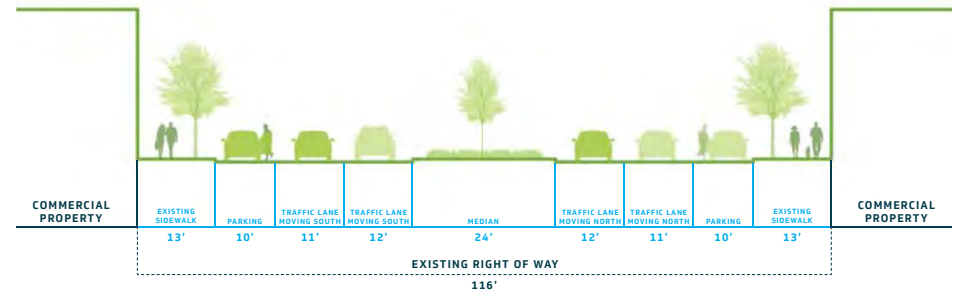
of one of the two proposed traffic lanes moving in each direction. Sectional studies were produced to test the configuration of the lanes on the street. The variations in each section considered different layouts for the parking lanes, traffic lanes, and possible future BRT lane. Other necessary considerations included the placement of bus stops and determining the pros and cons of locating them near the bike lanes or within the median. These considerations are very specific design details which would continually need to be worked out as the final design strategy is developed. The results of these sectional studies, along with community feedback, both, have been translated into the first final design proposal for Livernois Avenue, presented in Option A.

## > PROCESS ALT #3: LUSH LIVERNOIS

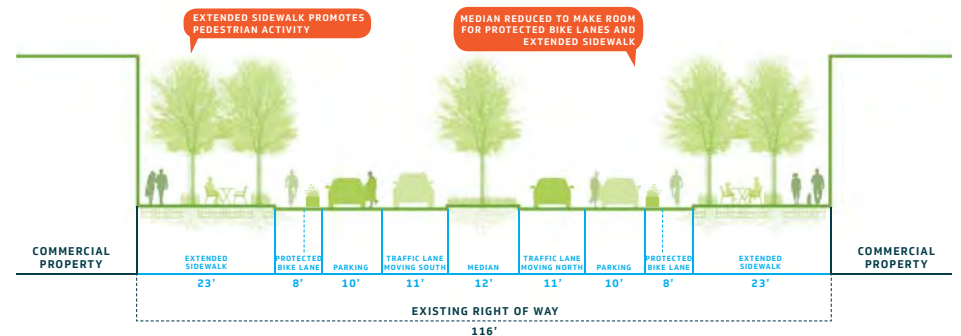
The design explored through the third alternative focused primarily on implementing major improvements to the pedestrian experience along Livernois Avenue, while enhancing the business platform of the road by defining the area as a destination rather than a fast-paced thoroughfare. The revitalized streetscape would offer 23 foot wide sidewalks with an allée of street trees on each side of the road. The median would be reduced to 12 feet with a row of trees planted within. The traffic lanes would be reduced to one lane flowing in each direction ensuring ease for pedestrians when crossing the street. A generous parking lane would border a protected bike lane running along the shady, park-like sidewalk. Planters could be placed between the bike lane and parked vehicles to add color and interest within a buffer area. The leafy-green and shaded sidewalks would have enough space for local businesses to add café tables and chairs, clothing racks, temporary signage, or even sculpture pieces.

The result of generously creating space for pedestrians, local businesses, cyclists, parking lanes, and the growth and establishment of healthy street trees would equal the creation of a lush, park-like avenue, welcoming for locals, visitors, and even passers-by to enjoy a pleasant experience along Livernois Avenue. Even speeding and road rage has been shown to be reduced along streets with an atmosphere proposed by this alternative.

As the design process developed through creating drawings and making presentations at a series of community and stakeholder meetings, one major concern about the third alternative surfaced. Both the community and the fire department voiced the same concern as found with the first alternative due to the reduction to one traffic lane. The assumption was that emergency vehicle access would be obstructed because of an absence of open space for vehicular traffic to pull over out of the single traffic lane. Immediately following the voicing of this concern, a number of studies were generated in plan and section in order to test the feasibility of emergency vehicle access within the third alternative design. Plan diagrams were created to compare



Existing section



Proposed section

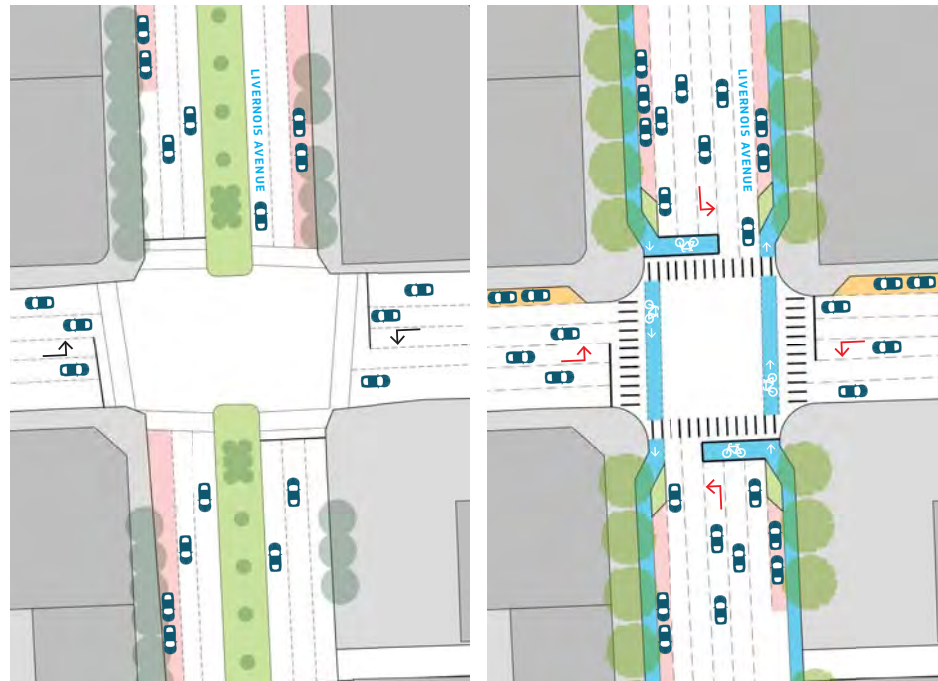
*The third alternative proposed extended sidewalks and two rows of street trees to be planted to create a dense canopy of shade and a leafy, park-like feel along the streetscape.*

the third alternative street layout with existing streets with a similar layout. Additional plan and sectional studies were also created to show variations to the street lane widths and the addition of designated void striped pull-over areas. The studies led back to the original assumption.

After further meetings with the fire department, it was determined that for the safety of the community, the median in the third alternative would need to be completely removed. Since this design has been translated into the second final design proposal, Option B proposes a void, striped median which will provide space for vehicles to move out of the traffic lane and allow emergency vehicles to safely pass without any delays.

## > BIKE LANE AT STREET INTERSECTIONS

In order to propose the addition of safe and efficient bike lane infrastructure within each design alternative, there needed to be additional consideration for bike lane crossings and turning at street intersections. The three design alternatives proposed implementing protected bike lanes which will run along the sidewalk between the curb and parking lane. The alternatives also proposed the addition of curb bumpouts at street intersections which will push the curb out and force the bike lane to cut through or flow around the bumpouts. There are a number of intersection treatments which have been implemented across the U.S. to ensure the safety of cyclists. These strategies are used in combination to serve the needs of specific intersections and street layouts.



The bike lane at street intersections design study shows a combination of three bike lane intersection treatments: bike boxes, refuge islands, and intersection crossing markings.



A similar bike lane intersection strategy implemented in Higgins, MT.

In the design study shown to the left, three bike lane intersection treatments were employed: bike boxes, refuge islands, and intersection crossing markings. By using bike boxes, a designated zone at the head of traffic lanes would be provided for bikers to wait during the red signal phase to make a left turn. Bike boxes can only be used at signalized intersections; at unsignalized intersections, two-stage turn queue boxes should be implemented, as shown in Figure 14. Refuge islands would be placed at the end of parking lanes, to both protect bikers from cars leaving the parking lane, and prevent cars from encroaching into the bike lanes when turning right. The bike lane would then flow between the refuge island and curb bumpout and through the street intersection within an area of intersection crossing markings. These markings could be made up of solid painted lanes, dashed lines, or bike and arrow symbols to indicate the crossing zone to vehicles. One of the benefits and safety features to bringing the bike lane around, instead of cutting through, the curb bumpout is that the bikers must then respond to the same traffic signalization as vehicles in order to cross the intersection. Precedent images and greater explanations of intersection treatments can be found on the NACTO website.

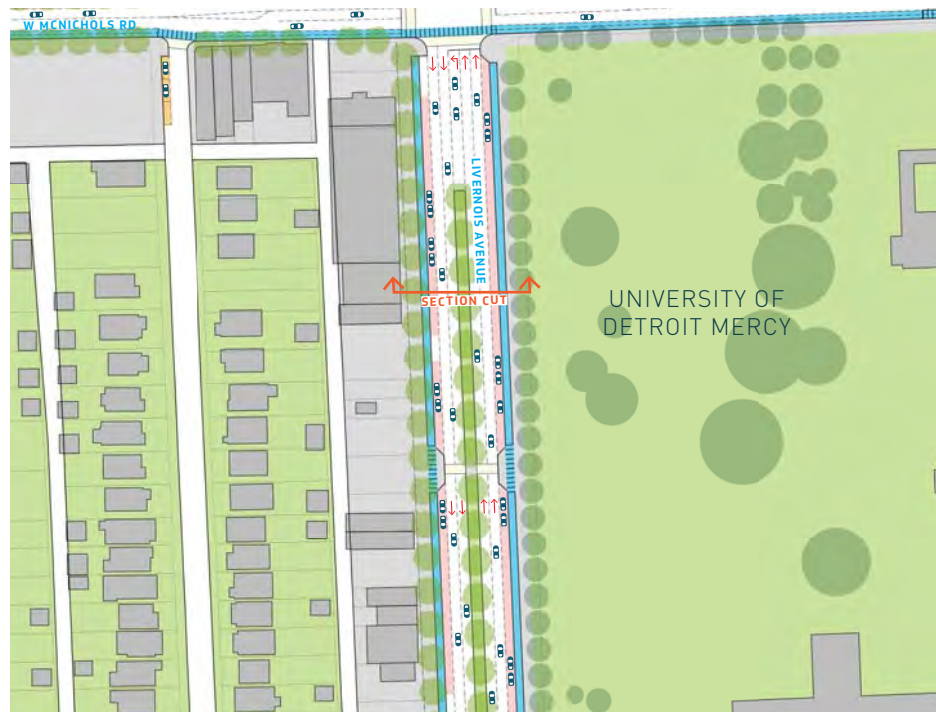


## > DESIGN PROPOSAL: LUSH TRANSIT THOROUGHFARE

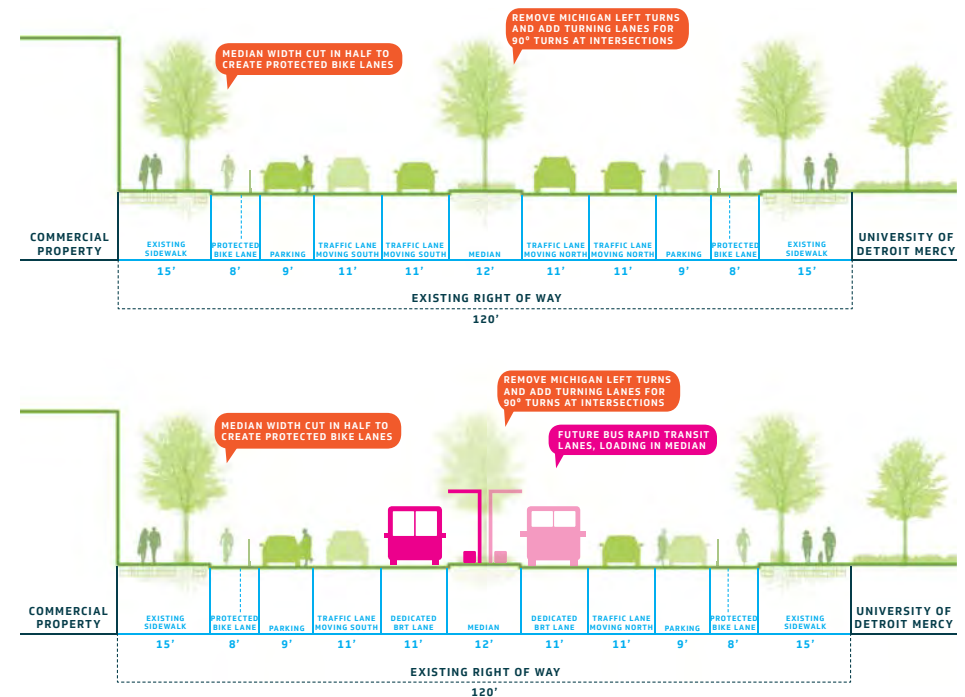
Design Option A proposes alterations to the Livernois Avenue streetscape, which will improve the function of the roadway for vehicular movement, cyclists, and pedestrians, while also offering the opportunity to designate a bus rapid transit (BRT) lane in the future.

Option A will transition Livernois into a multi-modal transit oriented street through the addition of a protected bike lane, the enhancement of pedestrian routes, and the potential for a future BRT lane. The protected bike lane will be 8 feet wide with a 5 foot travel width and 3 foot buffer area, and will be located between the sidewalk curb and on-street parking lane. Two vehicular traffic lanes moving in each direction will remain, but will be

reduced in width to 11 feet in order to slow traffic. To accommodate both the bike lane and traffic lanes, the median will be reduced by half to a width of 12 feet. The new median width will have plenty of space to establish healthy street trees along the length of the block. Continuous street trees will also be planted along the sidewalks. The width of the sidewalks will remain at 15 feet, but will be repaved to create a revitalized route for pedestrians and could incorporate elements of green infrastructure as well. Finally, a future bus rapid transit lane could feasibly be positioned in the traffic lane closest to the median. Bus stops could potentially be located within medians close to mid-block crossings, or on the sidewalk setback from street intersections within right turn lanes.



Proposed plan



Proposed sections

## > TRANSFORMING THE PUBLIC REALM

Through implementing safe and efficient protected bike lane infrastructure, the proposed Livernois Avenue streetscape aims to give residents of the community confidence in the opportunity to commute to work, run errands, or go to the neighborhood park on a bike. Drivers would also feel more comfortable on this stretch of Livernois due to the removal of Michigan left turns and the addition of turning lanes at street intersections.

Newly paved sidewalks, rows of street trees, and a revitalized streetscape are proposed in order to attract use by residents of the near-by neighborhoods, university students, and local business owners, as well as, pioneering entrepreneurs, developers, and visitors. The proposed transformation would set the foundation for increases in commerce, private investment, and value along Livernois Avenue.



The existing street view shows the currently underutilized and hard-to-navigate streetscape conditions.



The proposed streetscape will enhance the experience of pedestrians, cyclists, and drivers along the improved Livernois Avenue corridor.

*The proposed transformation would set the foundation for increases in commerce, private investment, and value along Livernois Avenue.*



## > DESIGN PROPOSAL: LIVELY PEDESTRIAN STREETScape

Design Option B proposes a substantial change to the Livernois Avenue streetscape and will have the most impact for creating a lively pedestrian and retail-oriented streetscape.

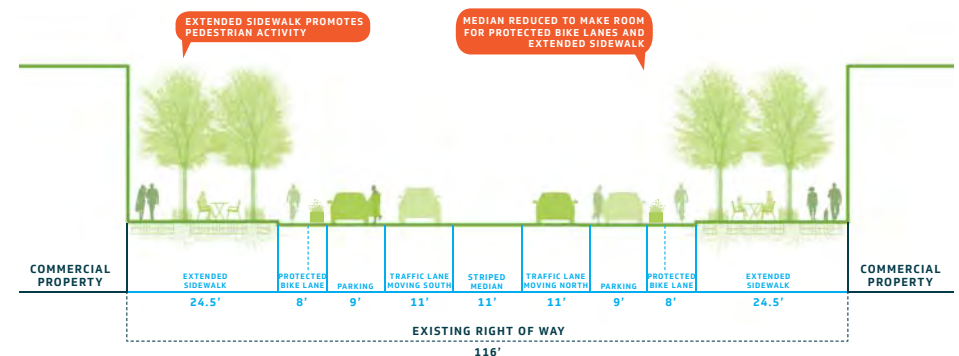
Through the removal of one lane of traffic flow in each direction, vehicular dominance will be reduced along the street. The on-street parking lanes will be shifted out to create space for an 8 foot wide protected bike lane. This positioning will create a physical safety barrier between vehicular traffic and cyclists and will allow street trees to shade the cyclists. Comprised of a 5 foot travel lane, the bike lane will also have a 3 foot wide striped buffer area. Planter boxes may spaced every 20 to 30 feet within these buffer areas to

display color and create visual interest along the streetscape. The width of the vehicular travel lanes will be reduced to 11 feet in order to slow traffic. The median will also be reduced to 11 feet and due to required passage widths necessary for emergency vehicle access, the median will become a diagonally striped clear area.

The most influential changes to the streetscape will occur along the sidewalks which will be extended to 24.5 feet in width. A double row of street trees will be planted along both sides of the road yielding a lush and shady outdoor space in front of retail shops and businesses. Cafe tables and chairs may be added within this space to create outdoor dining along Livernois Avenue.



Proposed plan



Proposed section



## > TRANSFORMING THE PUBLIC REALM

The design proposes to transform the character of Livernois Avenue into an urban hot spot where people come to dine, shop, socialize, or just relax. Generous sidewalk spaces, shaded under street trees, will create a linear park-like setting for commerce and leisure. Cyclists will be able to swiftly move along the bike lane. People will be able to easily and safely move from store to store and across the avenue due to the reduction of traffic lanes and the addition of mid-block crossings, clearly indicated crosswalks, and curb bumpouts.

The streetscape previously dominated by vehicular movement is proposed to become a vibrant public realm space which connects the residents of the community along with people from around Detroit with all that Livernois Avenue has to offer.



The existing street view shows the space available for the addition of improvements to the streetscape.



The proposed streetscape will create a vibrant atmosphere for community residents and visitors alike, while attracting further retail and local business growth along Livernois Avenue.

*The design calls for generous sidewalk spaces, shaded under street trees, to create a linear park-like setting for commerce and leisure.*

## > HOW TO INCORPORATE OPTIONS A & B

Both options A and B have been designed to occur along any part of Livernois Avenue. The design options can each individually occur along the whole section of Livernois within the project area, or both options could be implemented in different segments along the avenue.

There are a number of factors that should be considered in order to determine which options should occur along which sections of Livernois Avenue. Both options A and B propose the additions of key amenities such as bike lanes, street trees, and curb bumpouts which will help to improve pedestrian and bicycle circulation, but the options vary greatly in the overall function and character that will be accomplished within each streetscape option. Additionally, the options vary in cost, which is further detailed in Section 9. The use of each option should relate directly to the needs of that area along Livernois Avenue and the improvements that are proposed within the streetscape option.

KEY



**FIG 15** *study area for options A and B*

# STREETSCAPE DESIGN GUIDELINES





## > THE AIM FOR CREATING GUIDELINES

This section includes high level goals and guidelines for streetscape elements that are included in the plan. This document provides overall intentions for the streetscape elements and key design and installation considerations. Much more detailed design and engineering is necessary prior to installation. This section references streetscape design guidelines from other cities that offer greater detail and are valuable resources. The guidelines included here are specific to the Livernois and McNichols corridors and larger streetscape recommendations, but offer lessons for streetscape design citywide.

The streetscape elements detailed here include street trees, protected bike lanes, bumpouts and crosswalks, and chicanes and other side street strategies. All of these elements contribute to a safe, comfortable and enjoyable pedestrian and bicycling experience which will strengthen both Livernois and McNichols. Seasonal parklets and green alleys are also included as additional strategies that will enhance the commercial corridors and support retail activity. Street furnishings including benches, pedestrian lighting, waste receptacles, cafe tables and bike racks are also considered with corresponding recommended sidewalk plans.

## > STREET TREES

Street trees provide many environmental and social benefits to urban commercial corridors. Primary goals and considerations for street trees on Livernois and McNichols focus on place-making and user experience.

A HEALTHY TREE  
CANOPY CAN  
INCREASE RETAIL  
SALES BY

12%  
(Wolf, K. 2009)

## > GOALS

### VISUAL INTEREST + SENSE OF PLACE

- Street trees provide a sense of scale, visual interest and beauty to help make the area a destination.
- Tree canopies arching over the street and sidewalk will create a unique sense of place. A continuous line of street trees with consistent spacing helps to establish a visual rhythm for the street.
- Different species of trees can visually distinguish stretches of the corridor into unique zones and provide cues for wayfinding.

### PEDESTRIAN EXPERIENCE

- Shaded seating areas and sidewalks are more comfortable for people to interact, unwind, linger and support local businesses.

### PROPERTY VALUE

- Street trees increase property values and attract new businesses. People are willing to pay more for goods and services on streets that have trees.<sup>1</sup>

### ENVIRONMENTAL BENEFITS

- Trees improve air quality for residents and visitors and can also help manage stormwater runoff.

## > GUIDELINES

### INTEGRATION WITH OTHER STREETScape ELEMENTS

- The location and spacing of new street trees should be coordinated with the location of other street amenities, such as lighting, seating, special paving, and utilities as well as business entries. Trees should not block street lights.

<sup>1</sup> This study showed that consumers were willing to pay 9 percent more in small cities and 12 percent more in large cities for equivalent goods and services in business districts having a healthy tree canopy. Wolf, K. L. University of Washington. (August 2009) Trees Mean Business: City Trees and the Retail Streetscape. [http://www.naturewithin.info/CityBiz/MainStreetNews\\_Aug09\\_Trees.pdf](http://www.naturewithin.info/CityBiz/MainStreetNews_Aug09_Trees.pdf)

## SIZING

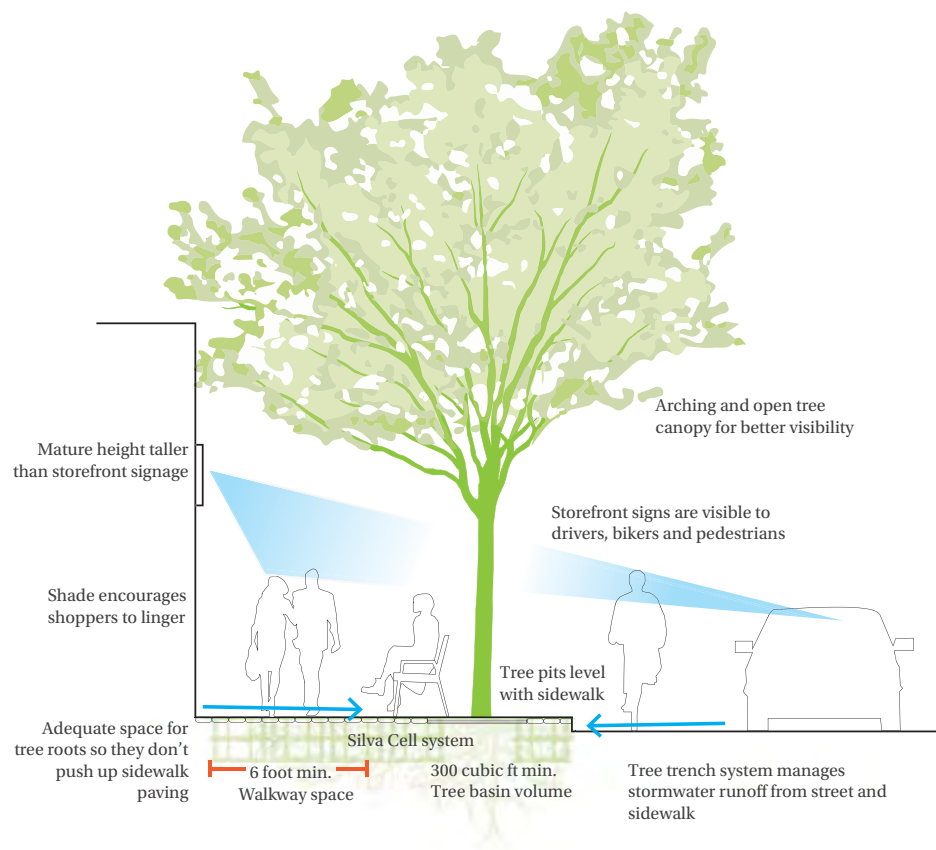
- Tree species should be carefully selected, with preference for medium to large trees with a mature height greater than 30 feet, that are also tolerant of tough urban conditions. Smaller trees may be planted if necessary due to overhead lines or restricted space.
- Street trees should be planted along the entire stretch of the Livernois and McNichols corridors, regularly spaced at 25-40' intervals, depending on tree spread and other street conditions.

## PRUNING

- The first 1-2 years after planting, pruning should be limited to removal of sucker growth at the base of trees and removal of dead branches for optimum canopy and root development.<sup>1</sup>
- Young trees should be pruned the second or third year after planting to improve their branching structure. Refer to the ISA tree pruning guidelines for more detailed instructions.<sup>1</sup>
- The best time to prune deciduous trees is November - February.<sup>1</sup>
- No more than 1/4 of the functioning leaf and stem area should be removed within one calendar year from any street tree.<sup>1</sup>

## TREE BASIN DESIGN

- In-ground basins for the trees should be designed with appropriate sizing and base material so they are large enough for tree roots to spread out without causing the sidewalk pavers to buckle, with at least 300 cubic feet of per tree basin.<sup>2</sup> The planting basins can be connected underground, creating a tree trench that maximizes root growth.
- Permeable pavers should be installed on top of the tree basins/trenches to allow stormwater infiltration.
- Below grade, a Silva Cell system is recommended to provide structural support and create ideal growing conditions for healthy street trees.
- Trees basins should be properly irrigated have a means of aeration. The system should be engineered to manage stormwater runoff from the street and sidewalk where possible.



## > VARIATIONS

- The widened sidewalk in Option B allows for a double row of trees.

<sup>1</sup> Seattle Dept. of Transportation Street Tree Manual: <https://www.seattle.gov/transportation/docs/Street%20Tree%20Manual%20WEB.pdf>

<sup>1</sup> International Society of Arboriculture Manual for Pruning Young Trees: [www.treesaregood.com/treecare/resources/Pruning\\_YoungTrees.pdf](http://www.treesaregood.com/treecare/resources/Pruning_YoungTrees.pdf)

<sup>3</sup> City of Minneapolis Urban Forest Policy: [http://www.ci.minneapolis.mn.us/www/groups/public/@cped/documents/webcontent/convert\\_282934.pdf](http://www.ci.minneapolis.mn.us/www/groups/public/@cped/documents/webcontent/convert_282934.pdf)

## > GOALS

### MAKE BICYCLING ACCESSIBLE, SAFE AND FUN

- Protected bike lanes make bicycling safer and more enjoyable for all ages and abilities.
- Buffers help reduce conflicts with motor vehicles.
- Biking improves resident and community health. It reduces incidents of chronic disease and stress for improved long-term health.<sup>1</sup>

### PROVIDE ACCESS TO JOBS + SERVICES

A city-wide bicycling network:

- Provides transportation choices for people to get to job centers, job training, education and neighborhood services.
- Improves the ability of people to travel safely via bicycle and creates a more flexible commute.
- Offers direct, convenient routes to many destinations along routes where bikers feel comfortable.

### BUILD ECONOMIC GROWTH

- Protected bike lanes promote development and increase property values.<sup>2</sup>
- Retail customers on bike tend to make more frequent trips and spend more money than customers traveling by car.<sup>3</sup>

## > GUIDELINES

### SIZING

- AASHTO standards require a minimum bike lane width of five feet from the curb face.
- A bike lane buffer of three feet allows enough space for bikers to ride outside of the “door zone,” avoiding conflicts with opening doors from parked cars in the adjacent parking lane and with people exiting them.

### PAINT

- The buffer should be marked with two solid white boundary lines with interior diagonal cross hatching to indicate where cars are discouraged from crossing. The buffer boundary lines may be dashed at intersections and driveways for clarity.

## > VARIATIONS

### TEMPORARY DELINEATOR POSTS - *LIVERNOIS OPTIONS A + B, MCNICHOLS*

- Flexible delineator posts should be used temporarily during an initial education period to provide an additional barrier.
- Posts should be removed before winter to allow for snowplow access in the bike lane.
- Recommended spacing is every 20 feet, in coordination with adjacent on street parking spacing.

### RAISED PLANTERS - *LIVERNOIS OPTION B*

- Planters provide an aesthetic barrier to the bike lane by adding greenery and color to the street.
- Sturdy, semi-permanent styles, such as concrete planters should be used.
- Plants should be selected carefully. A single species will decrease maintenance and provide visual continuity.
- Planters present a higher cost and their installation should be prioritized in areas with high pedestrian activity. They should only be considered if a strong maintenance program is in place.

### BOLLARDS - *MCNICHOLS (SOUTH SIDE OF STREET)*

- Bollards provide a visible, vertical barrier in the bike lane buffer and are a more permanent option.
- Because the south side of McNichols is not protected from a row of parked cars, bollards should be installed 10-20 feet apart, especially at the beginning of blocks where cars may try to turn into the bike lane.

### RAISED CURB - *MCNICHOLS (AT KEY INTERSECTIONS)*

- Pre-cast or cast in place raised curbs can provide protection for bike lanes that are not buffered from driving lanes by a lane of parked cars.
- Raised curbs can be strategically placed to deter drivers from accidentally turning into the bike lane at intersections
- Raised curbs may require specially-sized maintenance equipment for street sweeping and snow removal in the bike lanes.

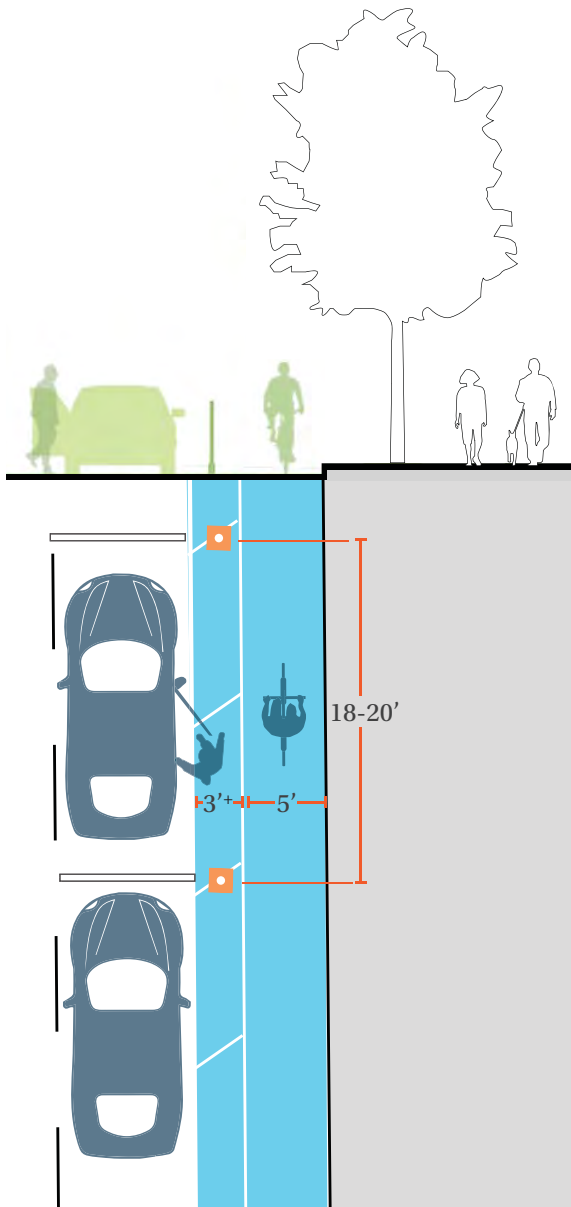
<sup>1</sup> Beil, K. “Physical Activity and the Intertwine: A Public Health Method of Reducing Obesity and Healthcare Costs,” 2011.

<sup>2</sup> Anderson, M., Hall, M.L., Protected Bike Lanes Mean Business: How 21st Century Transportation Networks Help New Urban Economies Boom. People for Bikes. Jan 2014.

<sup>3</sup> Clifton, K.J., Morrissey, S., Ritter, C. “Business Cycles: Catering to the Bicycling Market,” TR News 280, 2012: 26-32.



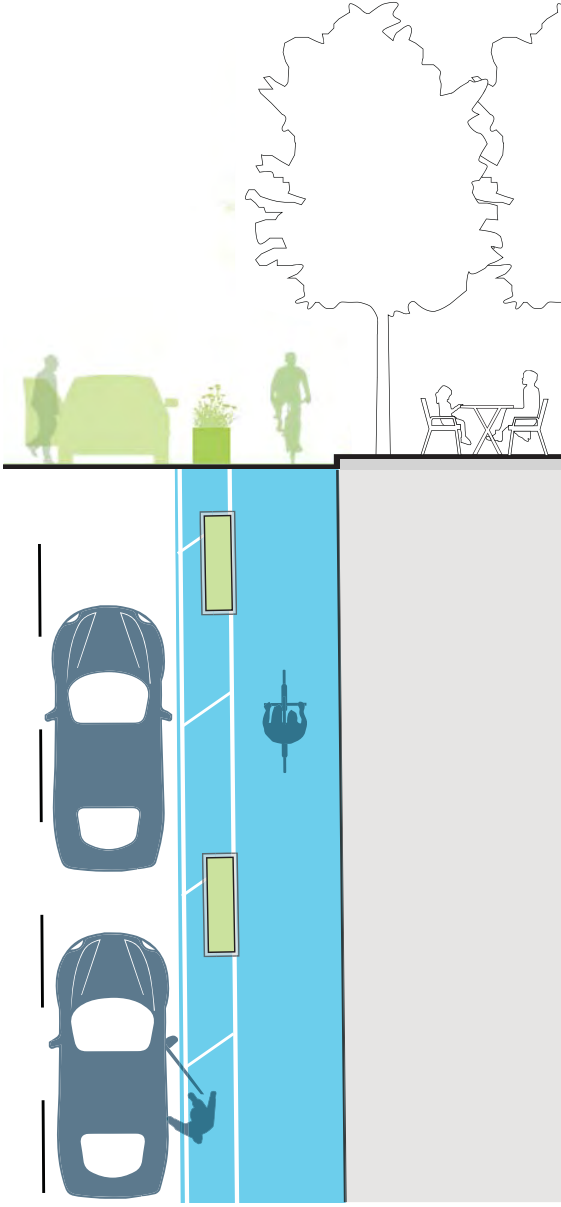
## PAINT WITH BOLLARDS OR DELINEATORS



Spacing of posts and  
planters should not  
interfere with parked  
cars

A 3' minimum buffer  
zone avoids car door  
conflicts and protects the  
biker

## PAINT WITH PLANTERS



A curb bumpout is an extension of the sidewalk into the parking lane, shortening the crossing distance at intersections. Bumpouts also provide more space for plantings and street furnishings, such as benches, trash cans and bike racks, and can support stormwater management with bioswales.

## > GOALS

### PEDESTRIAN SAFETY + COMFORT

- Bumpouts increase safety of pedestrian crossings by making pedestrians more visible to drivers, shortening the crossing distance, physically and visually narrowing the street, and slowing cars down.
- Curb bumpouts are combined with visible crosswalks.

### PARKING LANE BUFFER

- Curb bumpouts will be placed on each corner where there is an on-street parallel parking lane.
- This protects the parking lane and ensures that vehicles will not drive in the parking lane.

## > VARIATIONS

- In areas with high pedestrian traffic, mid-block bumpouts should be installed and incorporate street furnishings and plantings.
- Bus bumpouts must have loading zones clear of any plantings or streetscape elements. It is recommended that the bike lane is elevated at bus bumpouts to allow ADA access without multiple curb ramps. Bus bumpouts may incorporate bus shelters, lighting and other street furnishing outside of the loading zones.
- Bumpouts can incorporate bioswales to help manage stormwater.

## > GUIDELINES

### LOCATIONS

- Located at every intersection on Livernois and McNichols where there is an on-street parking lane.
- Additional bumpouts should be placed mid-block in denser retail areas on Livernois.

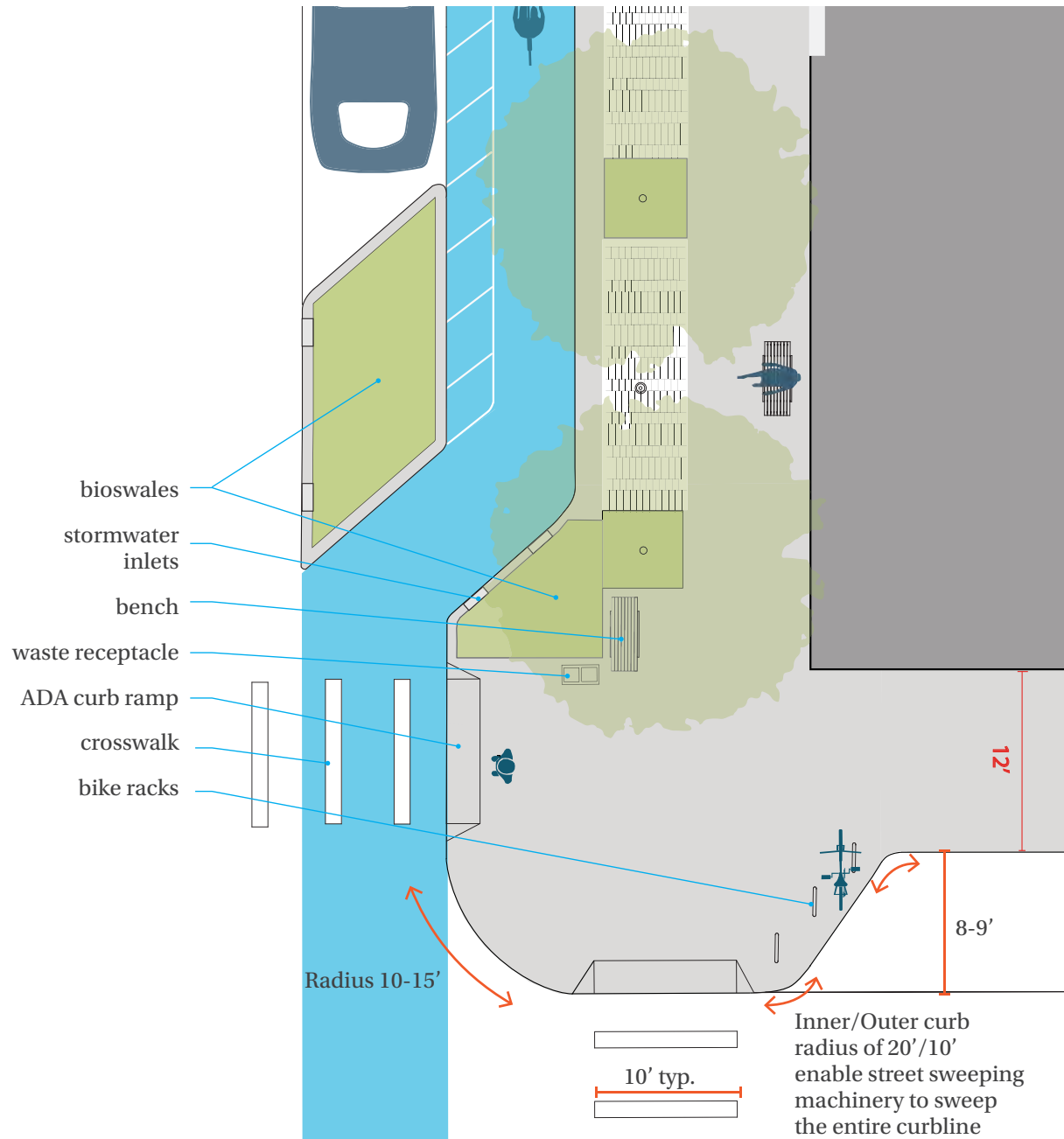
### SIZING

- Bumpouts should be designed to maximize pedestrian space, while accommodating turning requirements of vehicles and allowing for ease of maintenance by street sweepers.
- Crosswalks and bumpouts should meet all ADA guidelines.

### MAINTENANCE REQUIREMENTS

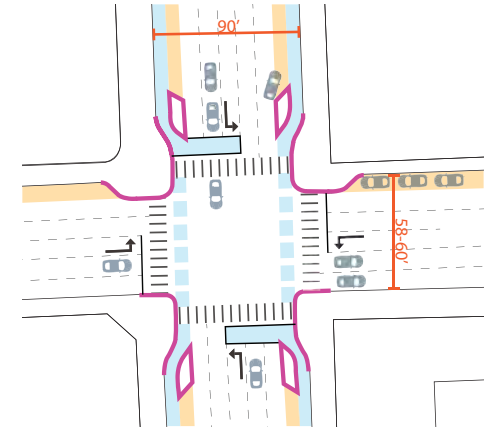
- Any stormwater infrastructure incorporated into the bumpout will require maintenance to ensure that it does not become blocked with debris.
- A maintenance strategy should be agreed upon between the City Department of Public Works, Detroit Department of Transportation, Detroit Water and Sewerage Department, and any other partners prior to construction.

## BUMPOUT STANDARD ELEMENTS

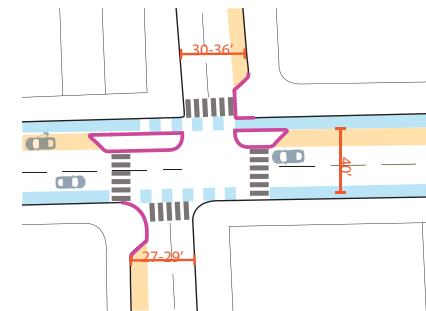


## VARIATIONS

### LIVERNOIS, GENERAL



### MCNICHOLS RETAIL STREET





During the pop-up streetscape installation, many residents perceived an increase in cut-through traffic and speeding on neighborhood side streets. Side street traffic calming has been a concern for several years, and will be an important component as the City moves forward with streetscape changes on Livernois. Chicanes were preferred by the community and approved by fire officials as a feasible option to manage traffic.

## > GOALS

### SLOW DOWN + DETER CUT-THROUGH TRAFFIC

- Chicanes force drivers to slow down on side streets, and also may make it less likely that drivers will use the neighborhood street as a shortcut in the future.

### HIGHLIGHT RESIDENTIAL ZONE

- The use of side street traffic calming strategies differentiates residential streets. Neighborhood signage can also be incorporated, building community identity.

## > GUIDELINES

### LOCATION

- The streets east and west of Livernois, especially Stoepel and Warrington, were noted to be most affected by cut through traffic. Additional resident feedback and traffic data will be an important next step in determining where to employ traffic calming strategies.
- Chicanes are recommended for streets with driveways and alternatives to on-street parking, as they will reduce the number of parking spaces.

### PLANTINGS

- Chicanes should be planted with low maintenance trees and perennial plants, with heights and forms that will not obscure drivers' views.
- Chicanes should be designed to capture and infiltrate stormwater runoff from the street.

### SIZING

- Chicanes should allow two-way traffic to maneuver at a reduced speed. Chicanes should extend far enough into the street to ensure single vehicles cannot simply drive straight through and avoid the curves.
- Chicanes should allow safe access to resident driveways

### SAFETY

- A chicane design may require additional striping, signs and reflective paint on the curb to ensure drivers are aware of the slight bend in the road.

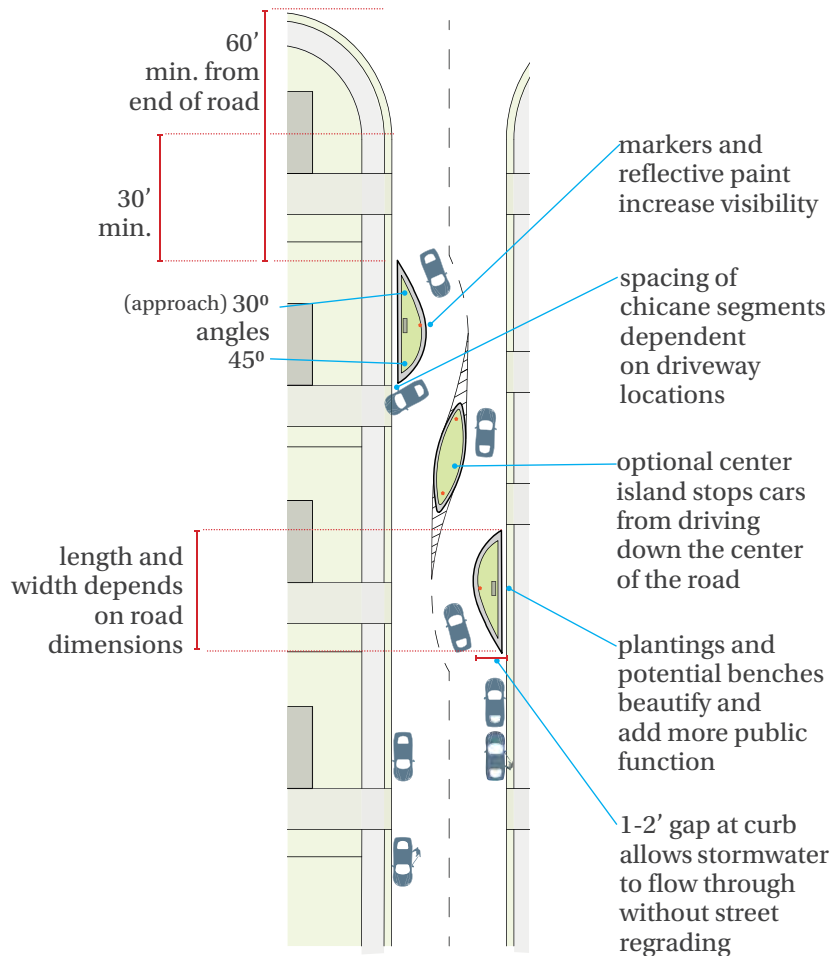
### SHORT TERM STRATEGIES

- Cheaper interim strategies may be employed, such as large flower planters to serve as bump outs or chicanes before funds are gained for more permanent options. These can be used to gather data during a trial period.

### MAINTENANCE REQUIREMENTS

- The space between the chicane and the curb may become clogged with leaves or debris and should be cleaned regularly.
- Any plantings will need to be irrigated, weeded and trimmed regularly.
- The curb radii should allow street sweepers and snow plows to easily service the street with minor adjustments.

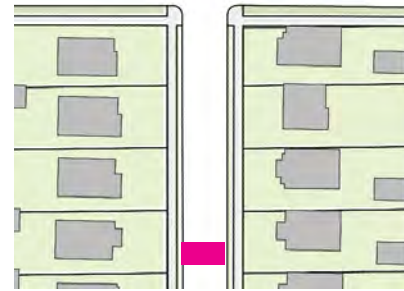
## CHICANES



## OTHER STRATEGIES

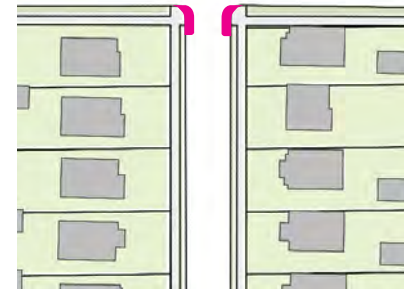
Other traffic calming options that could be considered include:

### SPEED TABLES



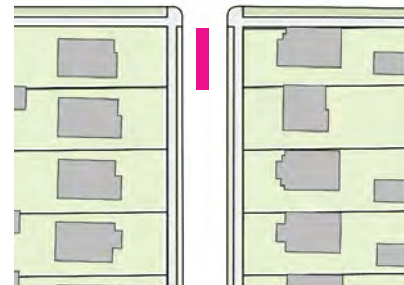
- Reduces speeds to 15-20 mph at the speed table.
- It could span all parts of a four-way intersection, or be placed on a single straight section of road.
- Can be combined with pedestrian crosswalk at intersection or midblock.
- Should include a maintained channel to allow water to drain to existing catchbasins, in some cases may require costly street regrading and relocating catchbasins.

### CURB BUMPOUTS



- Visually and physically narrows the entrance into the street.
- May be planted to beautify the streetscape.
- Slows turning speeds and may deter traffic from entering.
- Doesn't inhibit speeding in between intersections.

### GATEWAY ISLANDS



- Visually and physically narrows the entrance into the street.
- Gateways should be planted to beautify the streetscape.
- Ideal location for visible community signage.

# SEASONAL PARKLETS

Parklets can be installed seasonally to create more public space than sidewalks can provide. They create a more vibrant, active street life by encouraging people to patronize local food and drink establishments and linger outside.

## DESIGN

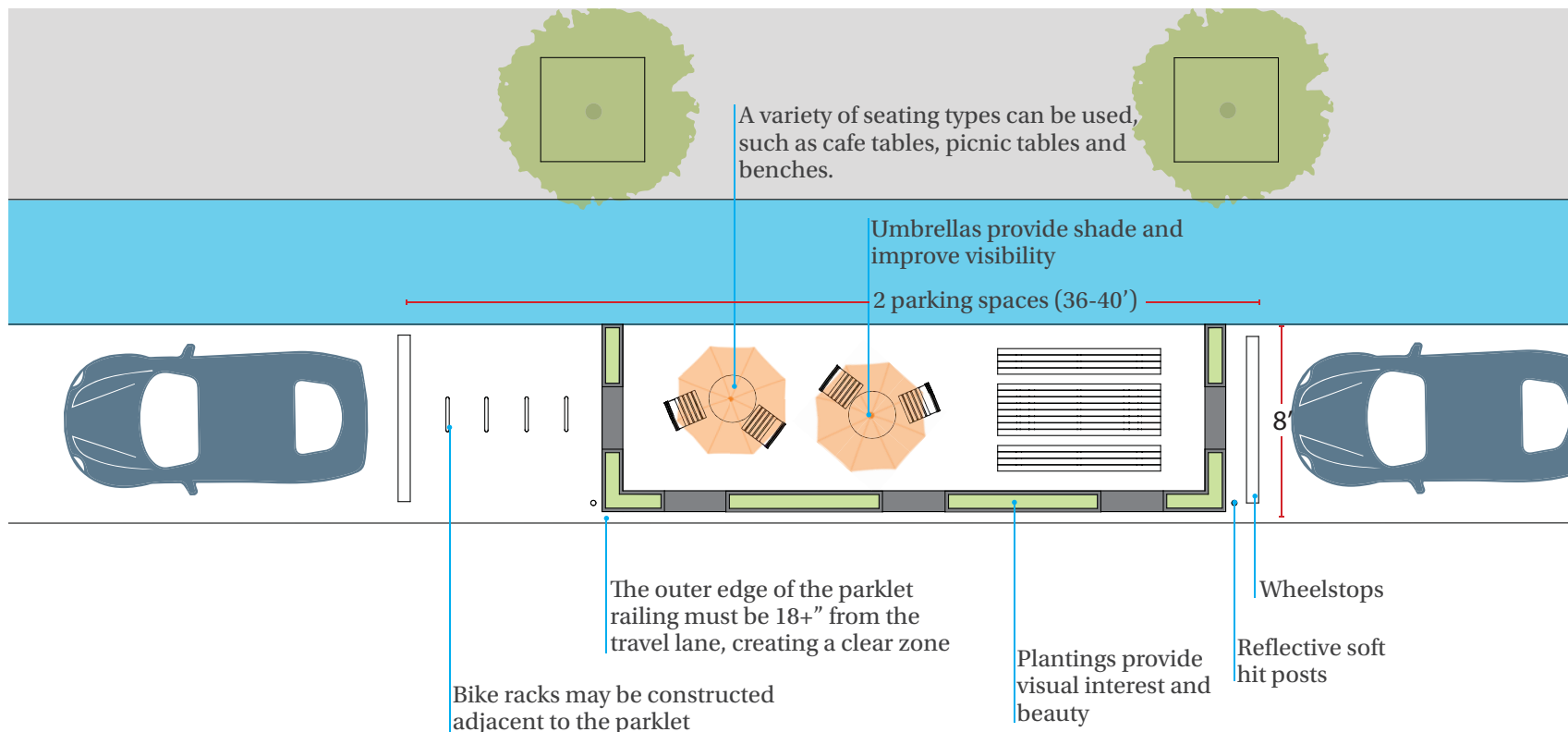
- Parklets should have some vertical elements (e.g. planters, umbrellas) to be visible to passing vehicles.
- The sidewalk-facing side of the parklet should be open to pedestrians.
- The parklet should utilize high quality materials and be able to be easily assembled and disassembled.
- Guard rails must be less than 3' high and able to withstand at least 200 lbs horizontal force.
- Parklet may be at grade with street level or built on an elevated platform.

## LOCATION

- Parklets should be located on streets with posted speed limits of 25 MPH or less so they are only appropriate on McNichols. Mid-block locations are ideal.
- They should be located in front of businesses or institutions that generate foot traffic and can take care of the parklet.

## SIZING

- Parklets should be less the width of than the parking lane.
- Parklets are typically the length of one or two parking spaces, and can include room for bike parking.





## MULTI-FUNCTIONAL ALLEYS

- Most of the businesses on Livernois and McNichols have entrances from the alley, but are rarely used because of the poor condition of the alleys. The alleys current primary use is to service waste disposal.
- Some existing buildings on McNichols have space for employee and limited customer parking that would more effectively utilized if alleys were improved.
- On Livernois, alleys should be used as access points to reach surface parking lots in between buildings, allowing for curb cuts to be narrowed or eliminated from sidewalks.
- Permits may be issued for businesses to use the alleys for temporary events.

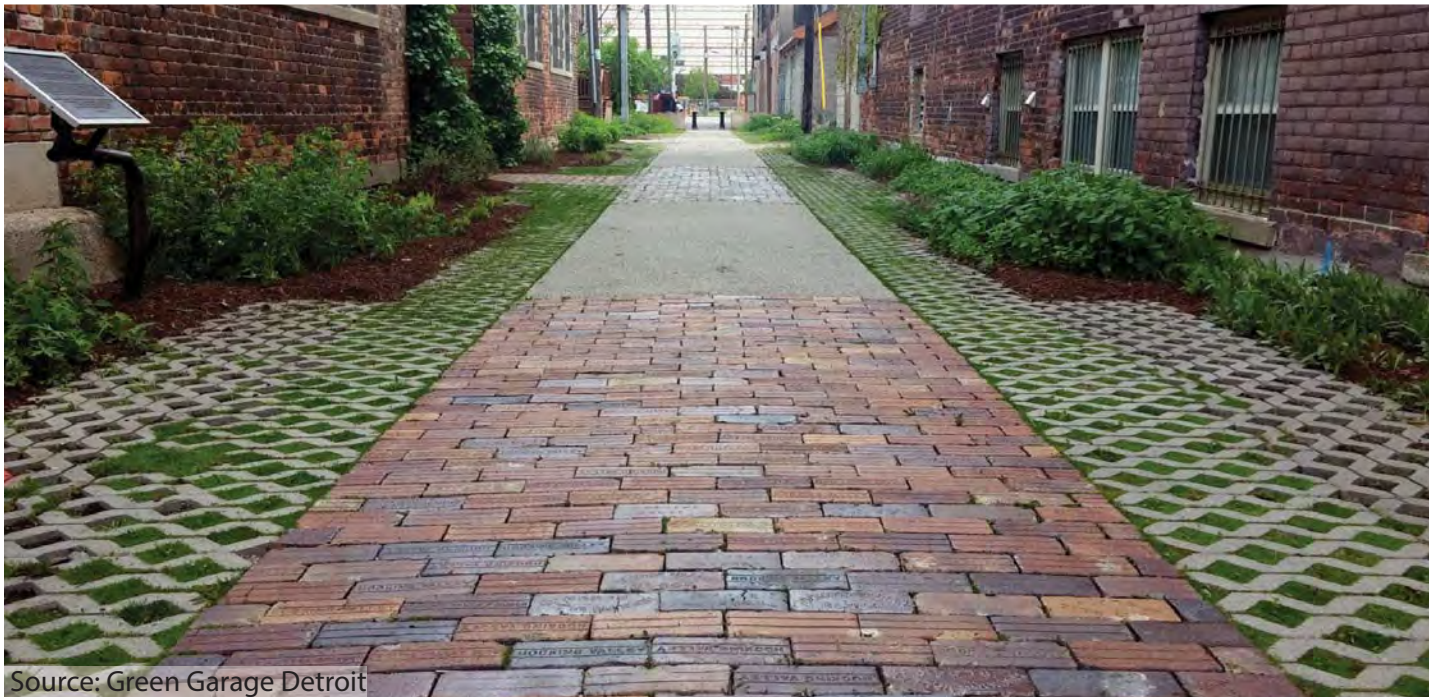
## STORMWATER MANAGEMENT

- Alleys should be repaved with permeable asphalt and/or permeable paving systems that allow for stormwater runoff from adjacent buildings to infiltrate into the ground.

- If the alley is wide enough, planting areas may be constructed next to buildings with perennial flowers, grasses, small shrubs and vines.

## SAFETY + AESTHETICS

- Adjacent fence lines should be clear of any overgrown trees or shrubs that reduce visibility.
- Dumpsters in the alley should follow sanitation codes. New paving materials and plantings will contribute to a more pleasant alley to walk or drive through.
- Alleys should be well lit, either through lighting installed on building exteriors, or additional City light posts.
- Murals may be painted on buildings to create a vibrant, creative atmosphere.



*Detroit's first green alley was installed in 2010 between Cass and 2nd in Midtown. It incorporates stormwater retentions, native plants, historic paving, and lighting. It is a beautiful pedestrian space and connecting piece of the Midtown Loop. The design does not incorporate vehicular access.*

Source: Green Garage Detroit

Street furnishings help build an active street life by creating a more comfortable place to spend time. Attention to the design, materials and placement of streetscape elements enhances the public realm and adds to a sense of place. All street furnishings should be in the same or similar style to provide a cohesive visual environment. All elements should be made of durable, high quality materials that resist corrosion and graffiti. All street furnishings should be located at least two feet from the curb edge to allow room for snow storage and street utilities and allow for at least six feet of clear walkway width.

## BIKE RACKS

- Individual bike racks should be well-distributed along a block, providing convenient access to building entrances. Bus stops, grocery stores and other major destinations should have clusters of multiple racks in one area closeby.
- Racks should be in public view with high visibility and good lighting.
- Bicycles should not block pedestrian thoroughways by jutting out into a sidewalk.
- Bike racks should support the bicycle by the frame, not the wheels, and accept both U-locks and cables.

## BENCHES

- Public seating activates the streetscape by providing a comfortable environment where people can rest, read, people-watch and interact with others.
- Benches should be placed either against buildings and facing toward the sidewalk, closer to curb edge and facing an active storefront, or if sidewalk width allows, perpendicular to the street.

## CAFE TABLES

- Outdoor cafe seating creates a place for people to socialize and linger, building a more lively street environment.
- Cafe tables should be located either at the building edge, or closer to the curb, maintaining a clear walkway of at least six feet. Cafe seating areas are not to be roped or fenced off.

## PEDESTRIAN LIGHTING

- Pedestrian lighting should be added to street light poles where feasible unless spacing between streetlight poles does not support adequate pedestrian lighting, in which case pedestrian lighting may need to be located between existing poles.
- Placement of light poles should provide adequate light distribution and have a consistent rhythm. Light poles should be coordinated with other streetscape elements, especially street trees.

## WASTE RECEPTACLES

- The presence of waste receptacles in areas of high pedestrian activity discourages littering, resulting in a healthier more aesthetically pleasant environment.
- Waste receptacles should be consistently placed near street corners (but out of a clear zone), and as close to bus stops as possible.
- Waste receptacles should have a dual-waste stream options for both recycling and trash, or two separate containers should be located side by side.
- Waste receptacles should be monitored and emptied regularly. Ideally they should be able to open from the side to allow easy access for removal of garbage bags.





*These are examples of durable and functional street furnishings made with high quality materials and simple modern designs. All elements should match or have complementary design styles and materials. There are many other examples to choose from.*



Source: Forms + Surfaces



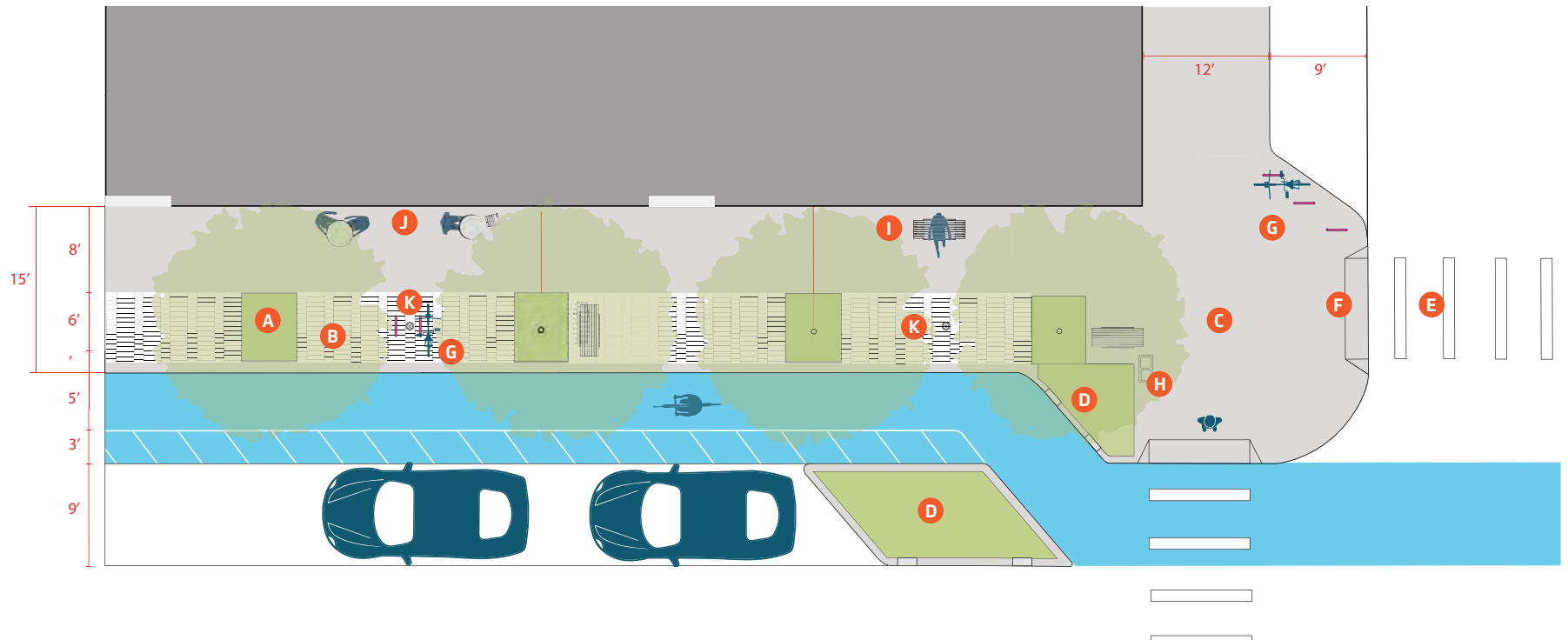
Source: Forms + Surfaces



Source: Forms + Surfaces



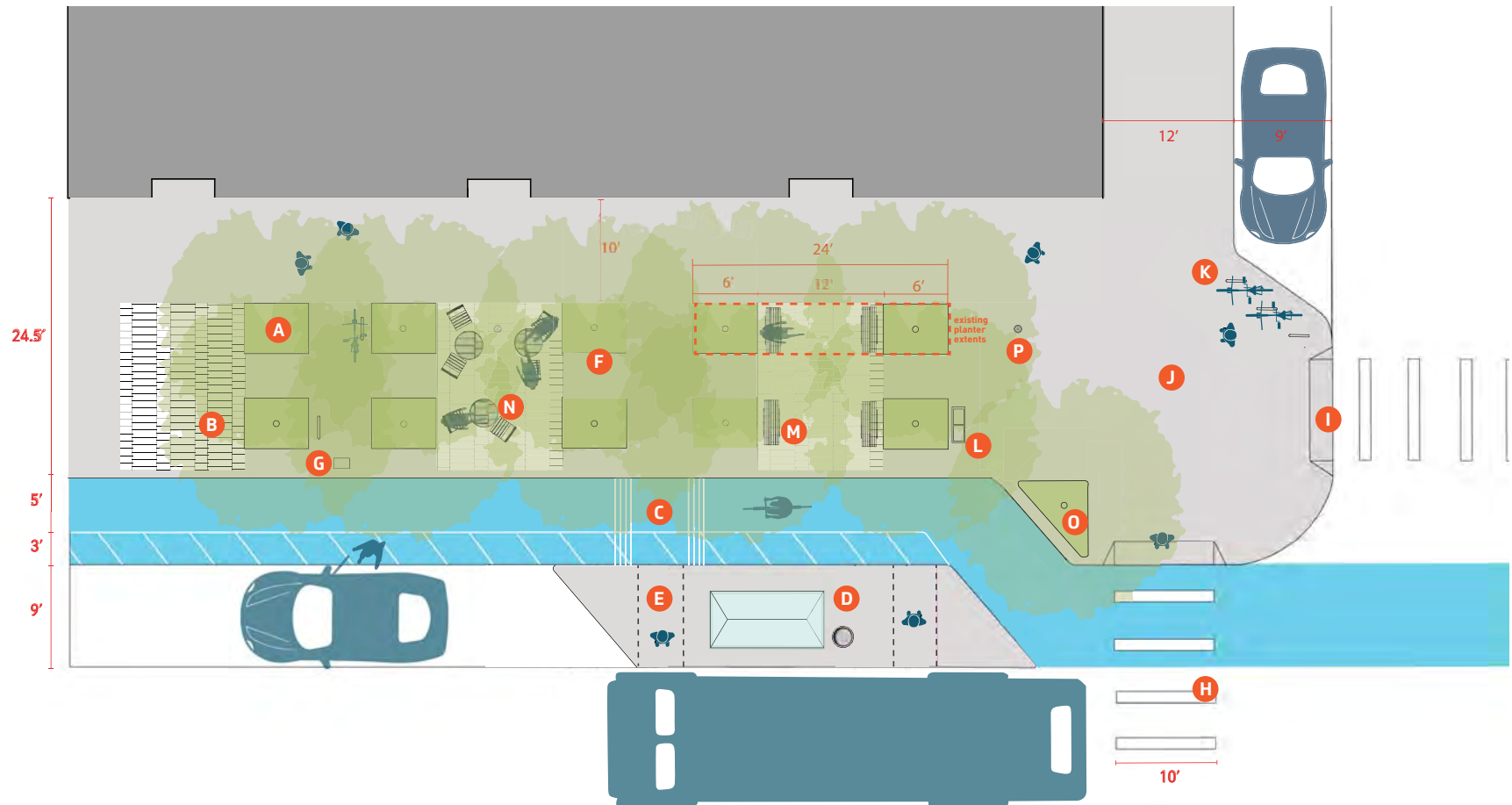
## > TYPICAL STREETSCAPE ELEMENTS LAYOUT



### KEY

- A** Street trees
- B** Permeable paving and tree trench with Silva cell system below grade
- C** Curb bumpout
- D** Bioswale planter
- E** Enhanced crosswalk
- F** ADA curb ramp
- G** Bike racks
- H** Dual-stream waste receptacle
- I** Benches
- J** Cafe tables
- K** Pedestrian lighting

## > TYPICAL STREETSCAPE ELEMENTS LAYOUT



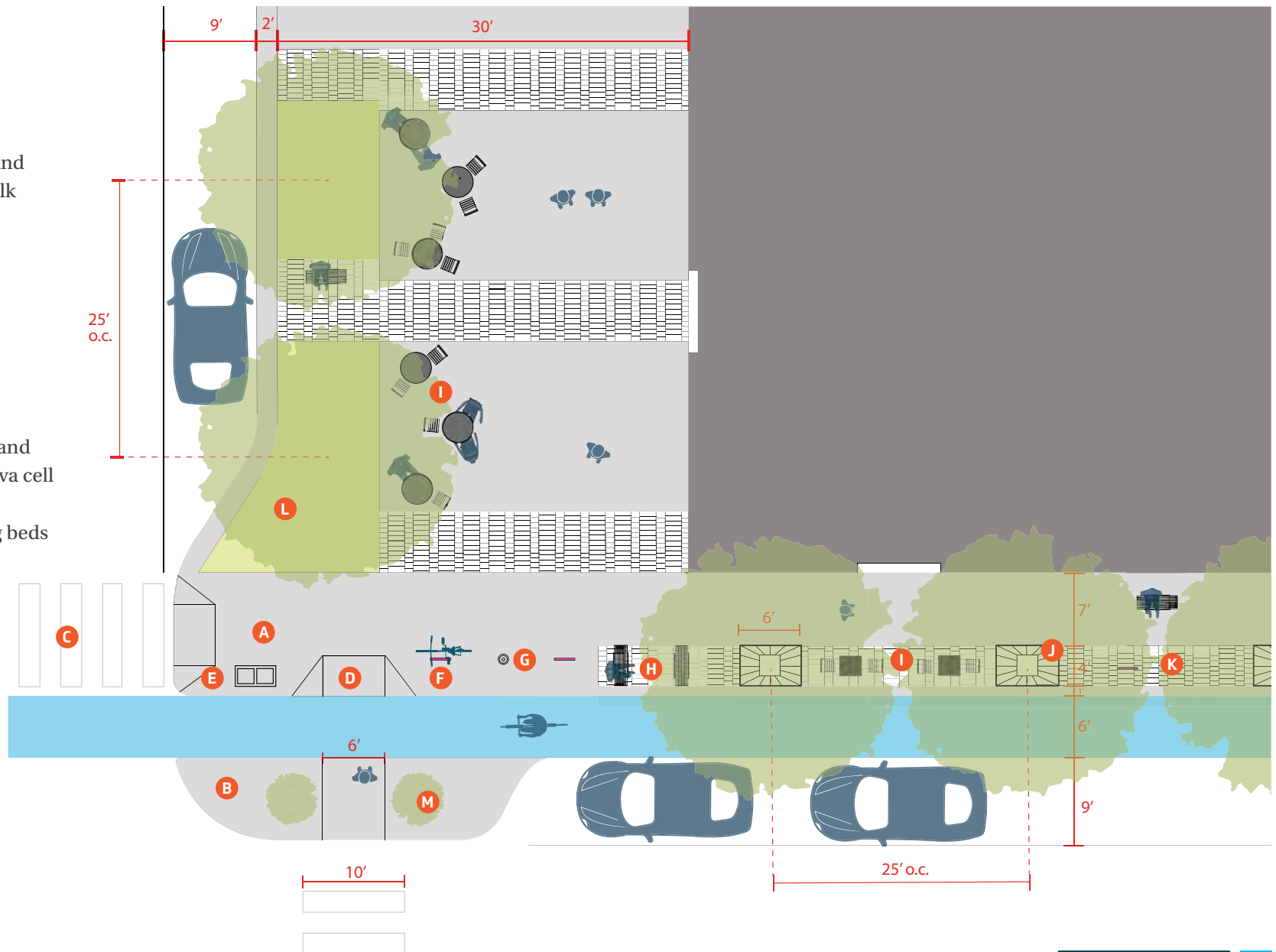
### KEY

- |  |                                       |                              |
|--|---------------------------------------|------------------------------|
| <b>A</b> Street trees  | <b>G</b> Relocated parking meter      | <b>M</b> Benches             |
| <b>B</b> Permeable paving and tree trench with Silva cell system below grade | <b>H</b> Enhanced crosswalk           | <b>N</b> Cafe tables         |
| <b>C</b> Elevated bike lane  | <b>I</b> ADA curb ramp                | <b>O</b> Bioswale planter    |
| <b>D</b> Bus bumpout with shelter  | <b>J</b> Curb bumpout                 | <b>P</b> Pedestrian lighting |
| <b>E</b> Bus loading clear zone  | <b>K</b> Bike racks                   |                              |
| <b>F</b> Existing fire hydrant   | <b>L</b> Dual-stream waste receptacle |                              |

## > TYPICAL STREETSCAPE ELEMENTS LAYOUT

### KEY

- A** Curb bumpout
- B** Curb bumpout island
- C** Enhanced crosswalk
- D** ADA curb ramp
- E** Dual-stream waste receptacle
- F** Bike racks
- G** Pedestrian lighting
- H** Benches
- I** Cafe tables
- J** Street tree grates
- K** Permeable paving and tree trench with Silva cell system
- L** In-ground planting beds
- M** Seasonal planters

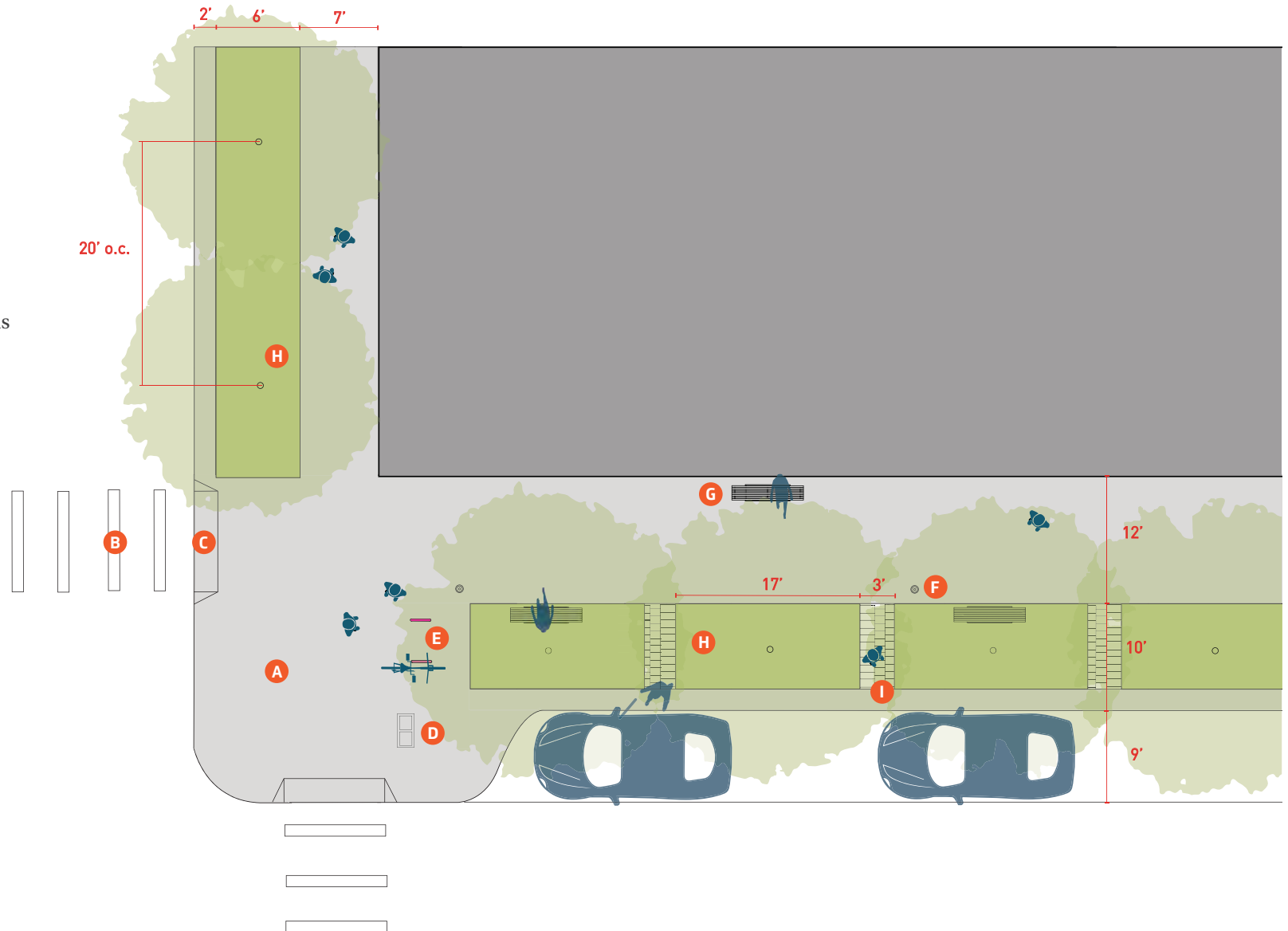




## > TYPICAL STREETSCAPE ELEMENTS LAYOUT

### KEY

- A** Curb bumpout
- B** Enhanced crosswalk
- C** ADA curb ramp
- D** Dual-stream waste receptacle
- E** Bike racks
- F** Pedestrian lighting
- G** Benches
- H** In-ground planting beds
- I** Parking spot pathways



# OVERALL COST ESTIMATES



## > DESIGN STRATEGY COSTS

Cost estimates were calculated using estimates for materials and services sourced in the Detroit area. These estimates are the approximate costs for the installation of each design strategy. Listed within both the Livernois Avenue and McNichols Road design strategies is the base strategy category. Within this category are standard updates for the entire streetscape which are not dependent upon the implementation of a design option or site-specific

strategy. The base strategy estimates are given as lump sum quantities. The cost given is expected to cover the cost of implementing these strategies to the entire project site. On the other hand, each design option and site-specific strategy is calculated as an approximate cost per linear foot. The overall cost for the implementation of these design strategies is dependent upon the determined bounds of the project site.

### LIVERNOIS AVENUE DESIGN STRATEGIES

0	Base Strategy	Estimated Cost
	-Remove Michigan Left Turns	<b>\$1.4 Million</b>
	-Improve 3 Intersections	<b>\$450,000</b>
	-Resurface Road	<b>\$1.2 Million</b>
1	Livernois Option A Design	<b>\$430/LF</b>
	- Sidewalks Remain at 12'	
	- Median Reduced to 12'	
	- Added Turning Lanes	
	- 4 11' Traffic Lanes	
	- 2 9' Parking Lanes	
	- 5' Separated Bike Lanes	
	- 3' Bike Buffer Lanes	
2	Livernois Option B Design	<b>\$900/LF</b>
	- Sidewalks Extended to 23'	
	- Median Reduced to 12'	
	- Added Turning Lanes	
	- 2 11' Traffic Lanes	
	- 2 10' Parking Lanes	
	- 5' Separated Bike Lanes	
	- 3' Bike Buffer Lanes	

### MCNICHOLS ROAD DESIGN STRATEGIES

0	Base Strategy	Estimated Cost
	-Improve 1 Major Intersection	<b>\$150,000</b>
	-Improve 2 Minor Intersections	<b>\$100,000</b>
	-Resurface Road	<b>\$35,000</b>
1	McNichols Neighborhood Retail	<b>\$1,012/LF</b>
	- 4' Separated Bike Lanes	
	- 2' Buffers	
	- 2 Traffic Lanes	
	- 1 Parking Lane	
	- Parking Added on Side Streets	
2	Marygrove College Park-like Aesthetic	<b>\$1,315/LF</b>
	- 12' Shared Pedestrian and Bike Path	
	- 9' Parkway Planting Strip	
	- 2 Traffic Lanes	
	- 1 Parking Lane	



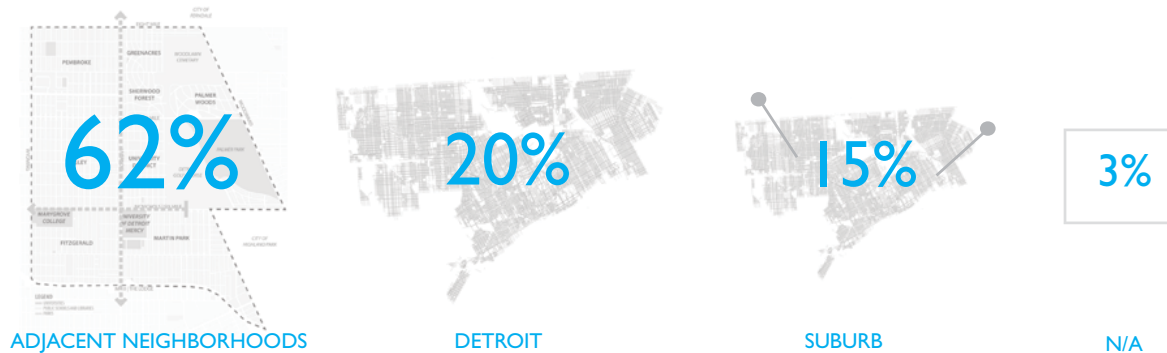
# APPENDIX

10

# LIVERNOIS STREETSCAPE SURVEY 1

This survey was distributed on Saturday, Aug 6, 2016 during the Jazz on the Ave festival to get feedback on the pop-up streetscape installation which was on Livernois Ave, from 7 Mile to St. Martins (3 blocks). The demonstration included adding bike lanes, reducing the number of vehicular travel lanes to one lane in each direction, adding crosswalks, bumpouts, bike rack, seating and planters. The following data was gathered from 57 participants during the Livernois Better Block / Jazz on The Ave Festival August 6th, 2016 from 12-4pm.

1. Most respondents lived in adjacent neighborhoods including, Green Acres, Fitzgerald, Bagley, Palmer Park, Sherwood Forest, University District...ect.

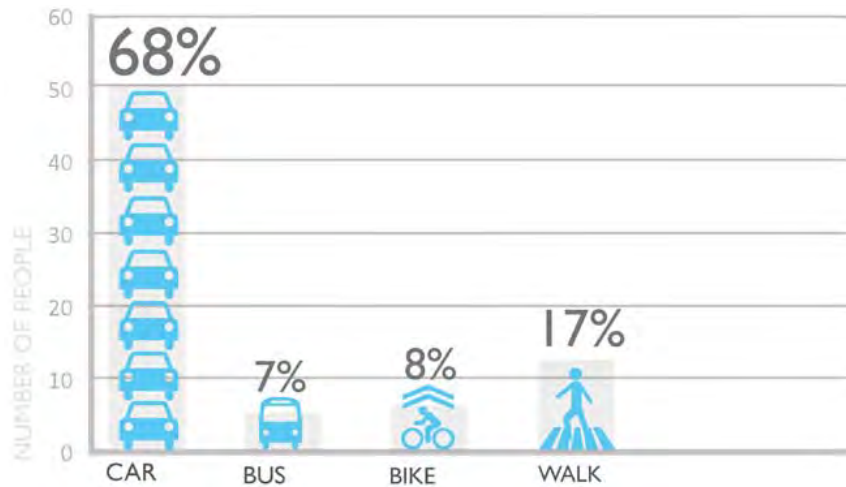


2. Most respondents visit Livernois businesses weekly or monthly.

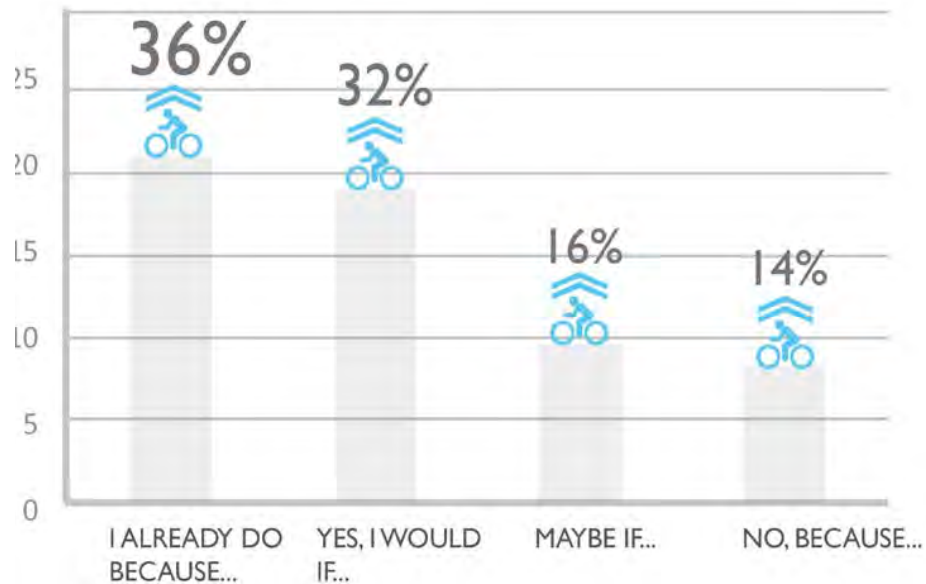


# LIVERNOIS STREETSCAPE SURVEY 1

3. Most respondents typically get to Livernois by **car**.



4. Most respondents **already walk / bike** to Livernois. Many said they would if it were more bike friendly and safer.



- **I ALREADY DO BECAUSE...**

- Parking is too expensive
  - I don't have a car
  - I live near by

- **YES, I WOULD IF...**

- Bike Friendly
  - Less Traffic / Safer
  - Lived Closer

- **MAYBE, IF...**

- I Owned a Bike
  - Bike Lanes / Bike Friendly
  - Wider Street

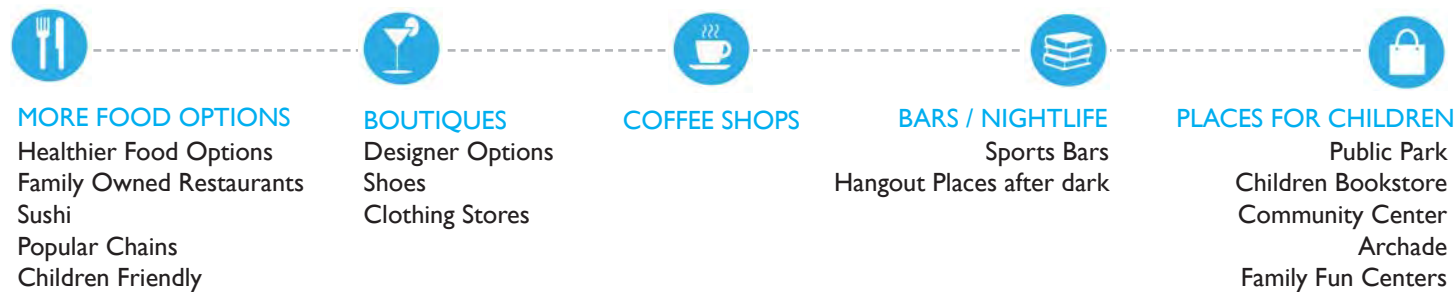
- **NO, BECAUSE...**

- Too Far to Walk
  - I don't bike, but I'd walk
  - If I lived closer

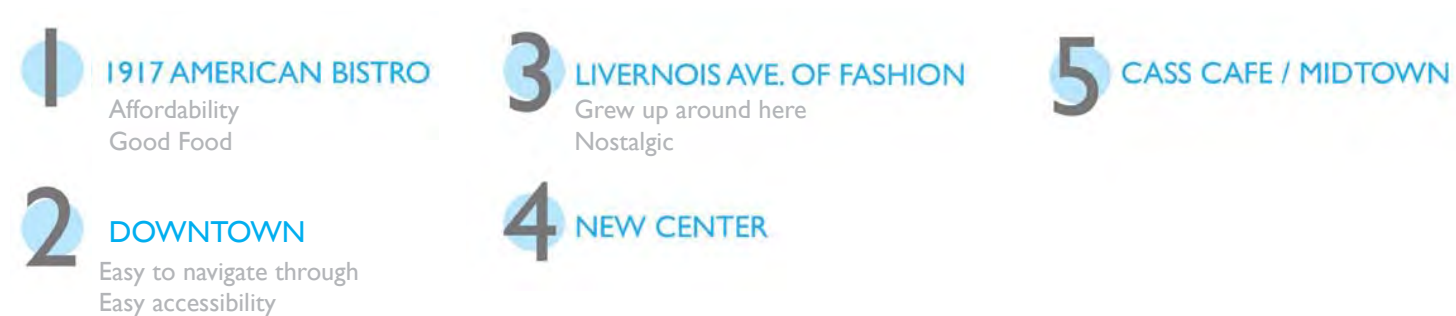


5. Respondents stated the following types of businesses are missing from Livernois: Food Options, Boutiques, Coffee Shops, Bars/Nightlife, and Places for Children.

Majority of the respondents mentioned they would like to see local family owned restaurants along with healthier food options like Panera Bread / Whole Foods and having an after work/school place to hangout like coffee shops.



6. Many respondents stated that their favorite place in Metro Detroit to shop/dine was 1917 American Bistro + Livernois' Ave of Fashion.

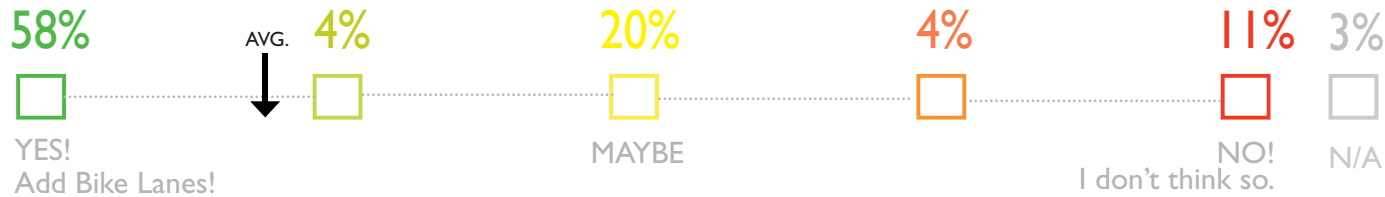


## > BETTER BLOCK INSTALLATION FEEDBACK



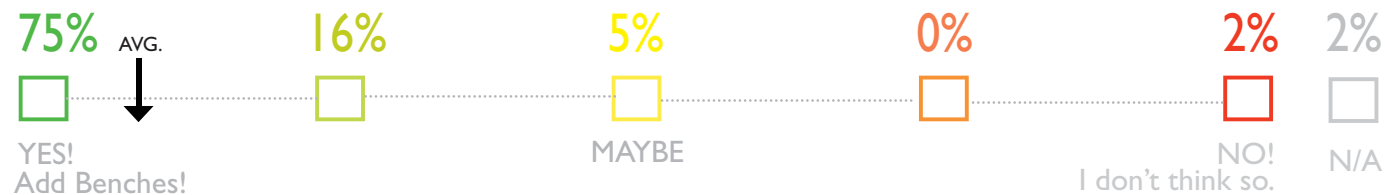
### BIKE LANES

62% of respondents said that bike lanes are a **good idea!**



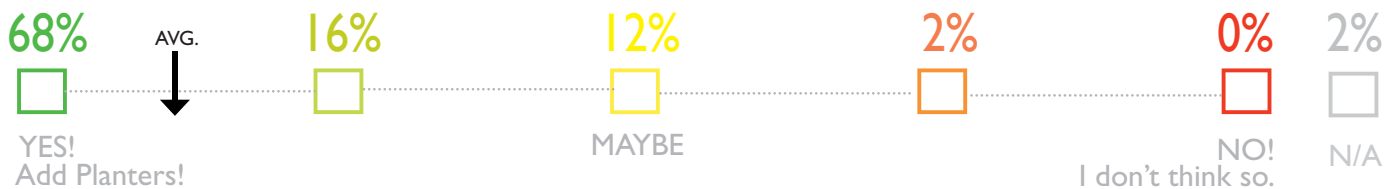
### BENCHES

91% of respondents said that benches are a **good idea** to improve sidewalk seating



### PLANTERS

84% of respondents said that improved flower planters are a **good idea!**

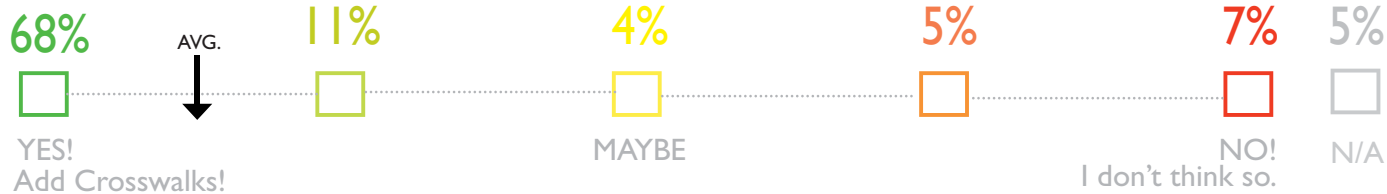


## > BETTER BLOCK INSTALLATION FEEDBACK



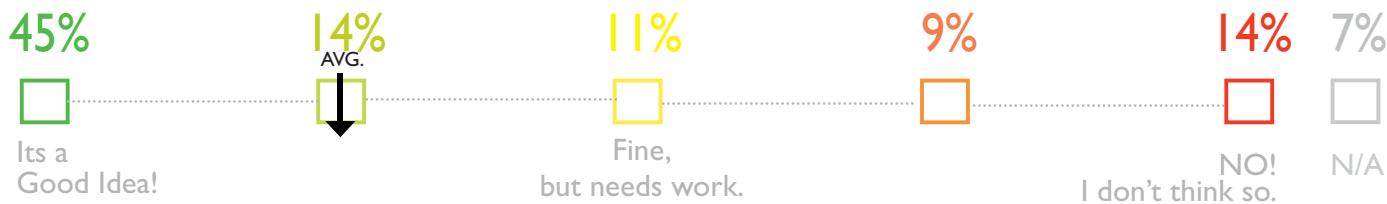
### CROSSWALKS

79% of respondents said that improved crosswalks are a **good idea!**



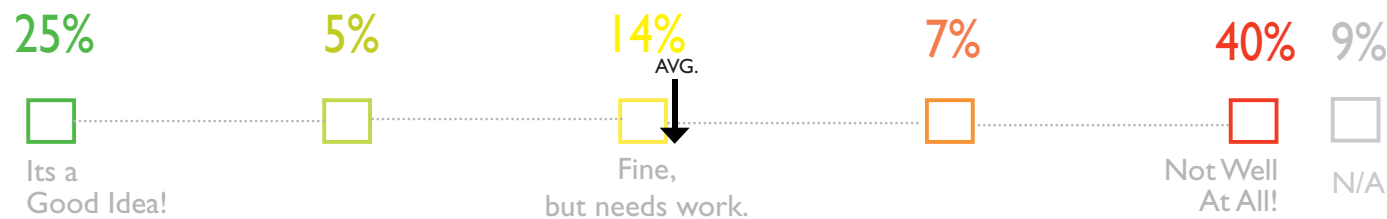
### CURB BUMPOUTS

59% of respondents said that curb bumpouts are a **good idea!**



### ONE LANE OF TRAFFIC (FOR CARS)

40% of respondents said that taking Livernois down to one lane of traffic each way would **not work at all!**





## > BETTER BLOCK INSTALLATION FEEDBACK

### WRITTEN FEEDBACK:

- “Free parking”
- “Take out the median”
- “Turn arounds are too long”
- “Time the traffic lights better - traffic is a bit slow”
- “Livernois (South of 8 Mile) needs activation + better linkage to more developed areas of Livernois”
- “Demo Strip Malls”
- “Streetside Infill”
- “I would like to see more days like today”
- “Parking in back”
- “Art would be nice”
- “bike lanes = reduced road capacity leading to terrible traffic. Don’t do it. Biking season is primarily short.”
- “Neighborhood website”
- “Too congested with one lane”
- “One lane = terrible traffic problems”
- “Bike racks wanted”
- “Afraid of those who double park”
- “The median cramps the flow of traffic already”

# LIVERNOIS STREETSCAPE SURVEY 2

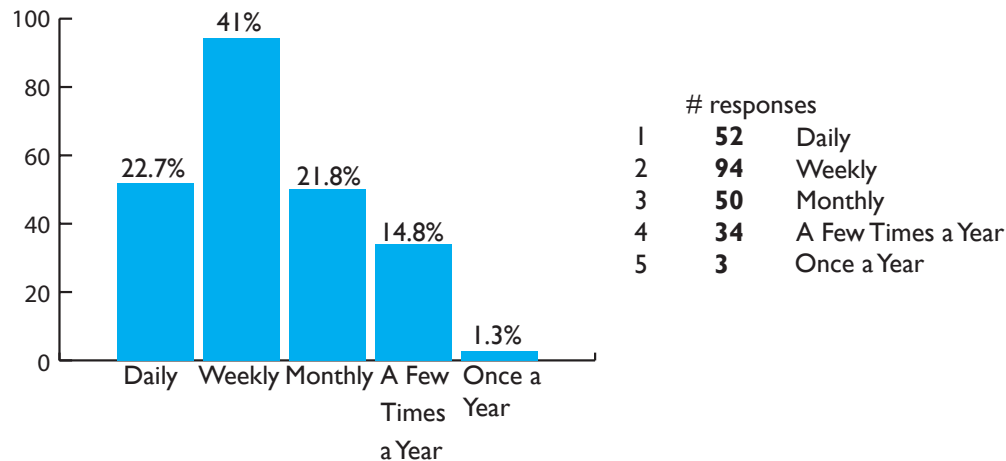
The following data was compiled and summarized from 242 responses to the Livernois Streetscape Survey, which was open from Oct. 14th- Nov 2. The survey was distributed both digitally and via hard copies to Livernois businesses.

1. Most respondents lived in adjacent areas including Sherwood Forest (14), University District (12), Bagley (4) and Palmer Woods (1).



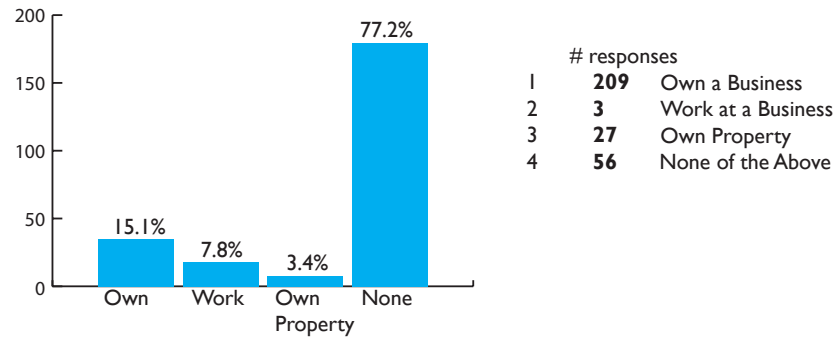
\*171 of respondents did not provide a neighborhood

2. Most respondents visit Livernois businesses weekly or daily.



# LIVERNOIS STREETSCAPE SURVEY 2

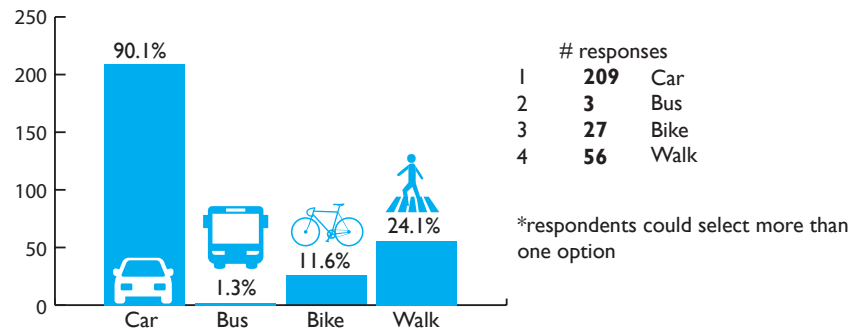
3. Most respondents do not own property or a business located on Livernois.



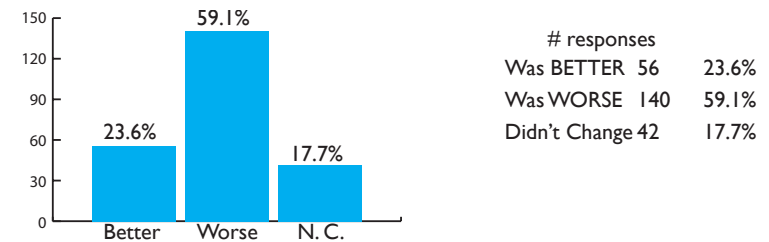
5. (81.7%) of respondents didn't use the bike lanes on Livernois during the Streetscape demonstration.



4. MODE SHARE: (90.1%) of respondents typically get to Livernois by car, while only (3%) get there by Bus.



6. DEMONSTRATION EXPERIENCE: Most respondents thought that the streetscape demonstration changed their experience of being on Livernois for the worse.





## 7. Respondents described their experience of being on Livernois was changed in these ways

### Negatively

slower traffic, difficulty parking, congestion

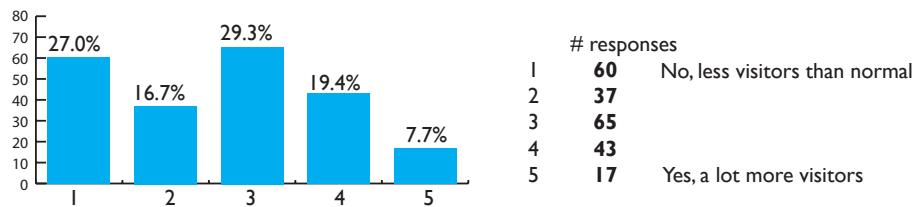
- General dislike of the installation- looked junky, confusing, etc.
- 7 respondents suggested that the bike lane should go where the median is or that the bike lane should be on the outside of the parking lane. Some said they didn't see many bicyclists using the bike lanes.
- 30 people responded with concerns with parking during the demonstration- they felt like they parking zones were confusing, awkward or unsafe ("felt like I was parked in the middle of the street). Some people said there was "no place" to park, or that they were holding up traffic when trying to park.
- 3 people responded with concerns of not having enough room for emergency vehicles to get through with one lane.
- "Traveling in the Streetscape zone seemed more dangerous because of the parked cars, and motorists that were unaware that cars were parked would impede drivers in the drive lane."
- Traffic was more congested and it just felt extremely awkward (and dangerous) to park cars on the outside of those bike lanes."

### Positively

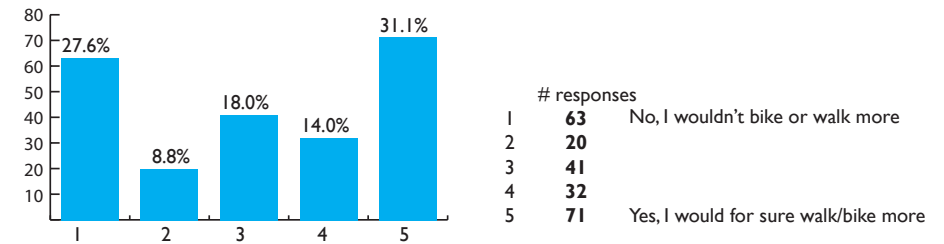
easier to see businesses, nicer to walk, safer to bike, increased optimism and pride

- Traffic was slower, which allowed pedestrians to feel safe and motorists were able see more of the businesses.
- Would like this but would need to think through the design more before implementation.
- Livernois felt safer for families to walk to shops on Livernois and for bikers to ride on the street.
- Some people said that the demonstration helped them notice new businesses and see the potential for future development on Livernois.
- "The street demo forced traffic to slow down and for people to be able to see my business."
- "I felt like the Livernois corridor could truly become a commercial corridor known for shopping similar to old Woodward in Birmingham."
- "I walked from my house to Livernois with my two grandchildren. We visited new shops that I had never entered and I have lived in the neighborhood for 30 years. Because Livernois was inviting. I [usually] would go to Ferndale, Royal Oak or Birmingham to eat and hang out."

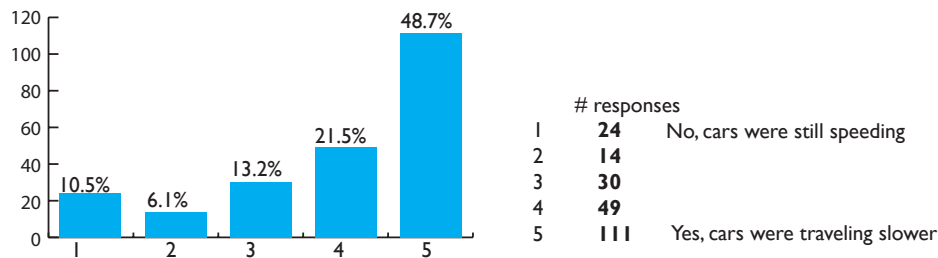
**8. CHANGE IN BIKE AND PEDESTRIAN TRAFFIC:** Most respondents noticed less (27%) or similar amounts (29%) of bike and pedestrian traffic and/or visitors to Livernois during the demonstration.



**10. CHANGING TRANSIT MODES:** Responses were mixed as to whether people thought they would walk or bike more if the Streetscape safety improvements were made.



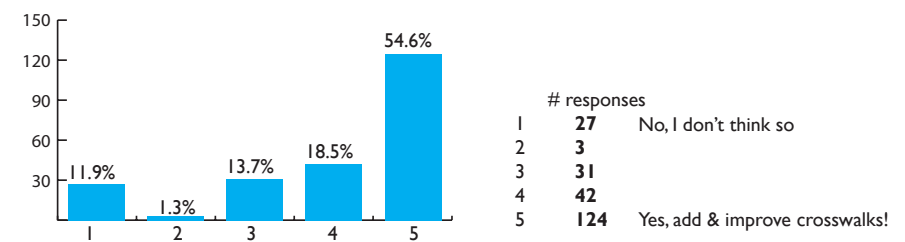
**9. IMPACT ON SPEEDING:** Most (70%) of respondents noted that there were less speeding cars along Livernois during the Streetscape demonstration.



**11. SIDE STREET IMPACT:** Respondents stated that Warrington Street was most affected by the installation through increased cut through traffic. (53 Mentions) Cambridge, Clarita, Stoepel, Oak, Saint Martins, Margareta, Santa Rosa were also mentioned

\*Many noticed the confusion at the start of the installation at 7 Mile and Livernois causing backups and speeding cars detouring through the Sherwood Forest, Green Acres, and University District neighborhoods

## 12. CROSSWALKS Over 70% of respondents said that improved crosswalks are a **good idea!**

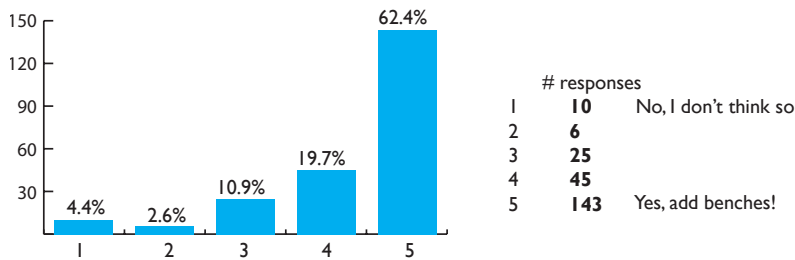


11. SIDE STREET IMPACT: Respondents stated that Warrington Street was most affected by the installation through increased cut through traffic. (53 Mentions) Cambridge, Clarita, Stoepel, Oak, Saint Martins, Margareta, Santa Rosa were also mentioned

\*Many noticed the confusion at the start of the installation at 7 Mile and Livernois causing backups and speeding cars detouring through the Sherwood Forest, Green Acres, and University District neighborhoods

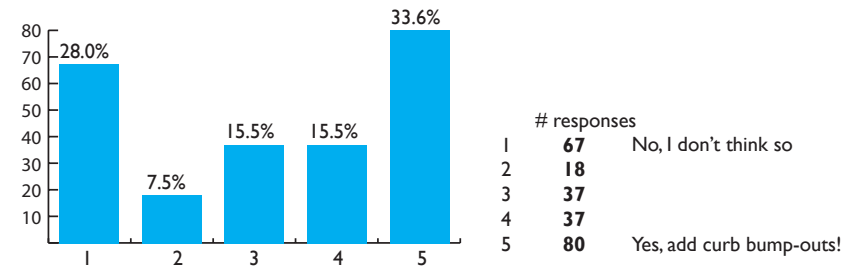
## 13. SEATING

Over 80% of respondents said that SEATING is a **good idea!**



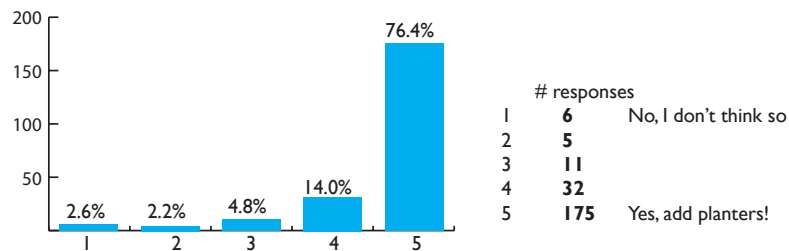
## 15. CURB BUMPOUTS

Support for curb bump-outs is mixed, but 49% of respondents said that curb bumpouts are a **good idea!**



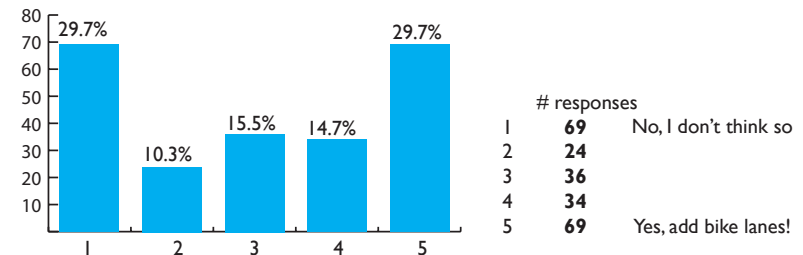
## 14. PLANTERS

Over 90% of respondents said that improved flower planters are a **good idea!**



## 16. BIKE LANES

Support for bike lanes are mixed, but 44% of respondents said that bike lanes are a **good idea!**

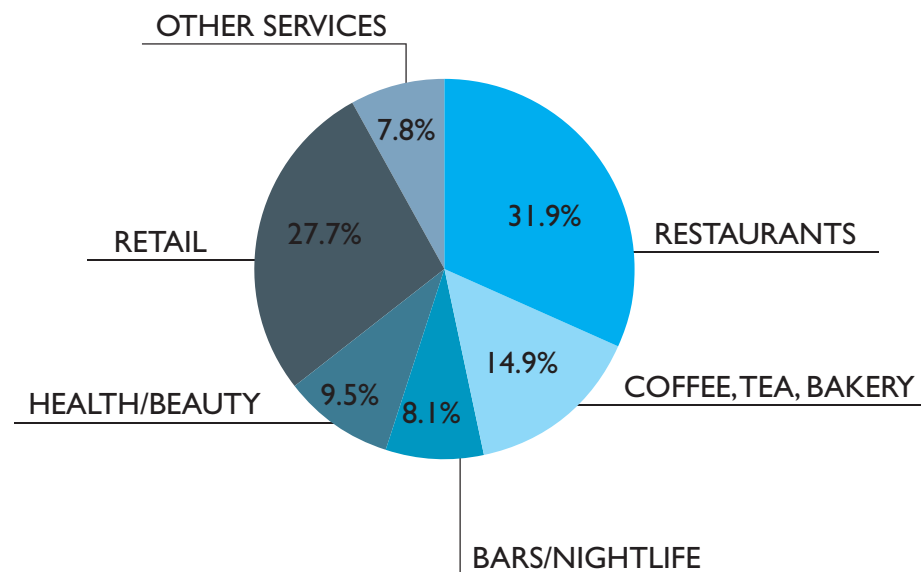




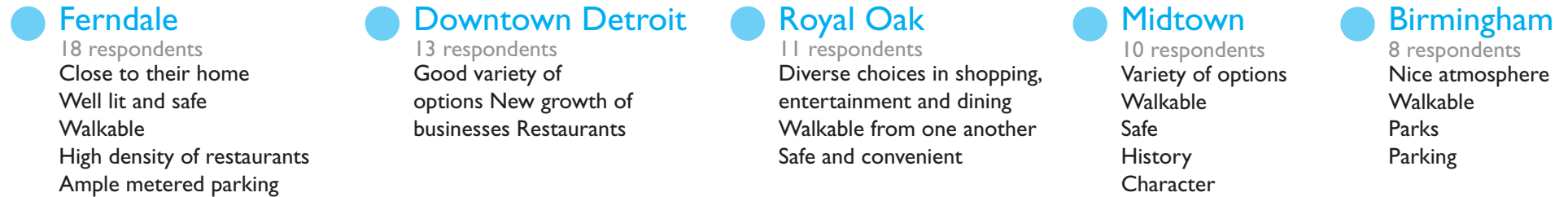
## 17. General comments regarding the Livernois Streetscape

- Many respondents stated that they would like to see more maintenance by the city and business owners to keep the sidewalks clean, upkeep of planters, and even pavement.
- Respondents were happy that this area has potential for change and are eager to start these changes as long as its well planned. Respondents also would like to see better maintenance of Livernois by the city and the business owners.
- Many respondents were in favor of narrowing the median.
- A number of people were supportive of bike lanes, but not if it removed a drive lane.
- Some respondents showed reluctance for change. They did not like the idea of taking Livernois down to one lane and they didn't want bike lanes added. However most exclaimed that they did not know how to navigate the Streetscape so perhaps with more engagement and education they will be more amiable to change.

## 18. Respondents stated they would like to see the following types of businesses on Livernois; (food options, coffee shops, bars/nightlife, health and beauty, retail, etc.



19. A large number of people (84) said that their favorite place to shop and dine in Metro Detroit was a particular commercial district.



## WRITTEN FEEDBACK:

- “It gave people the opportunity to ride their bikes and feel safe doing so and it also slowed down the traffic, which is needed on a busy street.”
- “I used Livernois to get to Ferndale instead of taking Woodward. This made me notice new businesses on Livernois that I wouldn’t have previously noticed.”
- “The street was too congested and dangerous for car drivers, bikers, and pedestrians.”
- “The increased traffic made it worse. It doesn’t seem that everyone knows how to use the bike lanes.”
- “Install more plants and art installations.”
- “The Streetscape would be better without the center grassy section that takes up too much of the street.”

## > SUMMARY

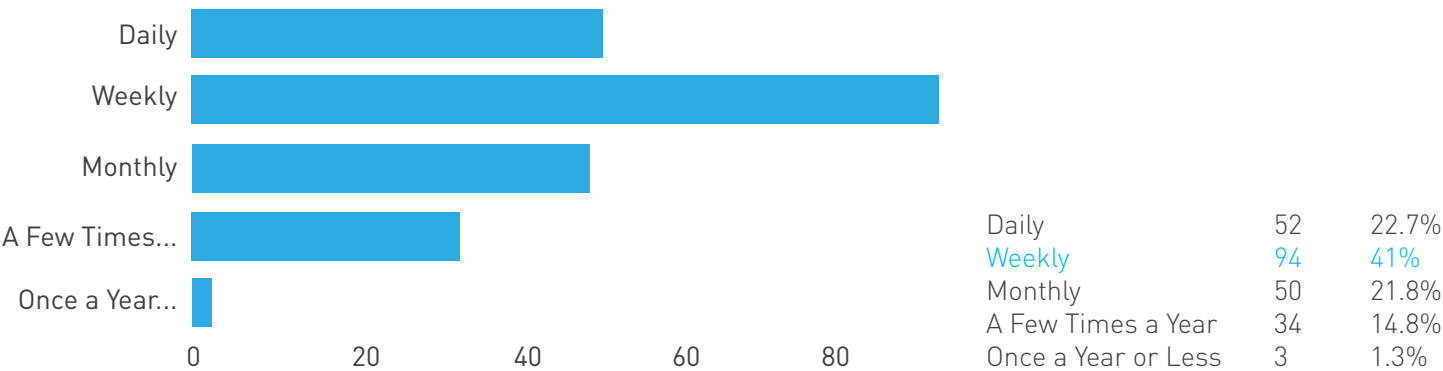
### 1. What neighborhood do you live in?

- Sherwood Forest
- University District
- University District
- Sherwood Forest
- Palmer Woods
- Bagley
- Martin Park
- Greenacres
- University district
- UDCA
- Rosedale Park
- Grandmont Rosedale
- Green Acres
- sherwood forest
- SHERWOOD FOREST
- Sherwood forest
- Sherwood
- Southfield
- University district
- Fitzgerald
- Bagley Community
- Royal Oak
- Bagley
- Grand Blanc
- Sherwood Forrest
- Sherwood Forest
- sherwood forrest
- 6 mile between Prairie & San Juan
- Sherwood Foresr
- Shrewood forest
- Sherwood Forrest
- Schaefer 7/8 Lodge Neighborhood Association
- Roseville
- Livernois and 7 Mile
- Bagley Community
- Downtown temporarily
- Frizgreald
- U of D
- university
- downtown detroit
- Fitzgerald-Marygrove
- The University District
- Udca
- Univ. Dist
- Marygrove
- University
- Golf Club
- University District, Oak Dr Block 7
- West Village
- university district
- pilgrim Village
- Sherwood
- University district
- Golf Course District
- Ave of Fashion
- Detroit
- Russel Woods
- Near Downtown
- Macomb
- Pontiac
- 8 & Gratiot
- Covant Gardens
- Plymouth, MI
- Avenue of Fashion
- Livernois Area
- Taylor, MI
- Clawson, MI
- Shuffen 7 Mile
- Oakland County
- Denby High School District
- Southfield
- University
- Redford Twp/ Professional Racquet Services 19444  
Livernois Ave
- Grandmont-Rosedale
- Don't live an any of the areas above

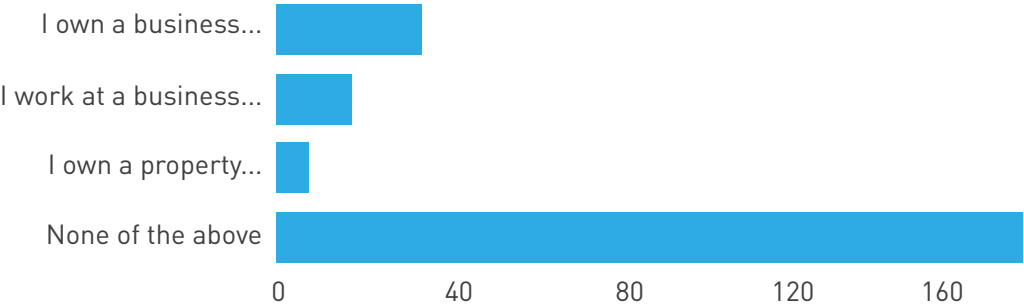


## > SUMMARY

### 2. How often do you visit businesses on Livernois?

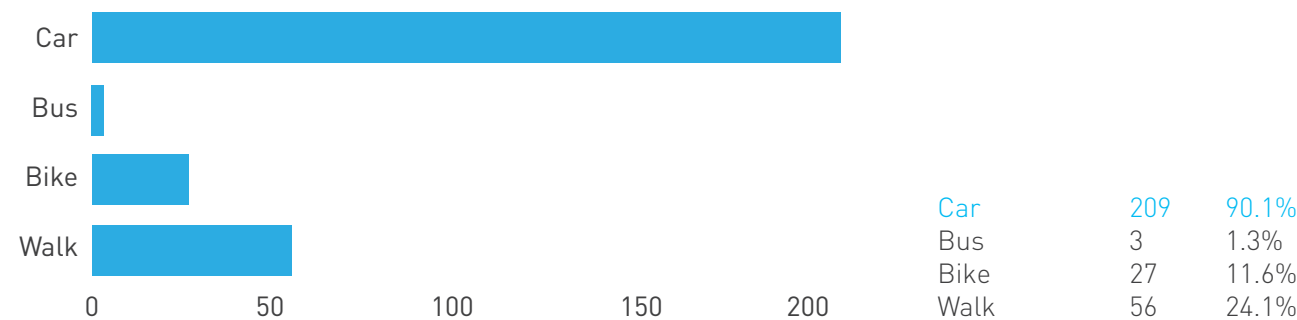


### 3. Do you own property or a business located on Livernois?



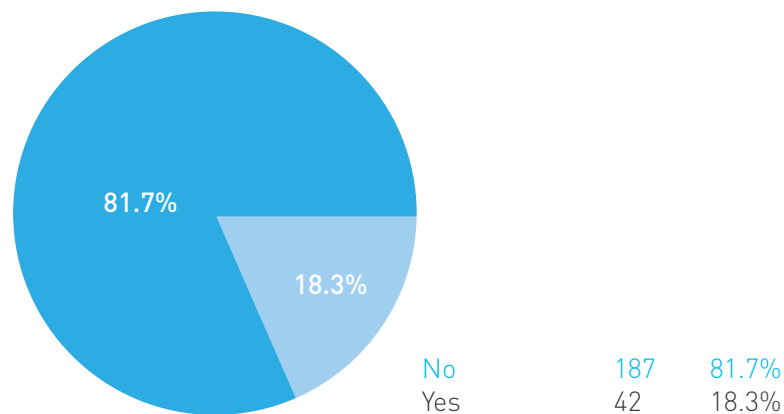
## > SUMMARY

4. How do you typically get to Livernois?



## > IMPACTS OF STREETSCAPE DEMONSTRATION

5. Did you use the bike lanes on Livernois during the streetscape demonstration?



> IMPACTS OF STREETSCAPE DEMONSTRATION

6. Did the streetscape demonstration change your experience of being on Livernois?



7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- “When visiting shops I felt safer and farther removed from traffic.”
- “More traffic, harder to use MI turnabouts, constant back ups.”
- “Traffic was slower, i can see the businesses easier.”
- “Increased level of optimism and pride.”
- “It slowed traffic down.”
- “It felt safe to ride on Livernois rather than cut through the neighborhoods. Was more likely to stop at the Livernois businesses.”
- “One lane for cars is not enough. It was jammed, slow, & frustrating.”
- “Crowded, less parking, unnecessary congestion.”
- “It was very difficult to park so i had to have friends park on Warrington and walk over to meet me at good cakes.”
- “I felt traffic was significantly slower on Livernois. It made walking on Livernois friendlier and feel safer.”
- “I was able to bike on Livernois rather than on the side street. I ride with my son on my bike daily to his preschool in Ferndale and normally have to use the sidewalks until I cross eight mile.”
- “I do not like riding on the sidewalk and would like a bike lane on Livernois.”
- “Much safer for my family”
- “I felt like the livernois corridor could truly become a commerical corridor known for shopping similar to old Woodward in Birmingham.”
- “Biking on that area was a dream. Traffic in my car wasn’t that bad either. Definitely manageable.”
- “Traffic was thicker and people didn’t understand the bike lanes which made me uncomfortable to use my bike.”
- “I think with the median taken out it would be better.”
- “I go biking and walking with my wife and two year old son. I think the bike lanes are great for safety. I also believe the lanes slow traffic, which will help bring business and safety to the environment.”



## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- “Uncertainty. Congestion. Frustration.”
- “It slowed down traffic, so bussness could be seen.”
- “Congested traffic, reduced to one lane.”
- “I am an acid jogger and could jog on Livernois safely with this demonstration.”
- “Too confusing and congested.”
- “No Place to park to get to the resturant and other bussiness.”
- “DID NOT SEE ANY IMPROVEMENT BEST BEFORE ISLANDS WERE IMSTALED”
- “Fucked up”
- “Traffic jam”
- “Congestion galore”
- “I think more bike lanes are always a better idea.”
- “Traffic moved slower and it was easier to bike in the street.”
- “Congested traffic near corners of 7 Mile.”
- “There was more traffic and parking diverted into my neighborhood.”
- “It’s worked out in Ferndale because there is not much traffic north of Eight Mile. I have and use a bike on almost a daily basis but I always ride on Oak and the other streets east of Livernois.”
- “It’s just safer. Not many people ride bikes and I think that creating bike lanes on Livernois will just accentuate the impatience of people driving cars.”
- “I don’t see the point.”
- “I like the idea of bike lanes but the traffic was terrible with only one lane.”
- “A better walking experience.”
- “I walked from my house to Livernois with my two grandchildren. We visited new shops that I had never entered and I have lived in the neighborhood for 30 years.”
- “The street was too congested and dangerous for car drivers, bikers, and pedestrians.”
- “Traffic in the left lanes became congested due to the bike land and parking. At one point there was street repair.”
- “Slow traffic”
- “Traffic was slower... cars were awkwardly parked and after a couple weeks the cones separating the bike lane damaged and missing, making the area look unkemp.”
- “Positive - further facilitated a safe, walkable community. Traffic slowed to a significantly more reasonable level.”
- “Traffic was congested. I avoided the area.”
- “Traffic was a mess. Nobody understood the program. It was dangerous Inside parking lane is not safe”
- “Traffic much more congested Only one auto traffic lane slowed travel.”
- “1) It was easier to cross street at Outer Drive/Livernois, 2) Nicer drive down Livernois.”
- “The street demo forced traffic to slow down and for people to be able to see my business.”
- “Yes, parking was a problem for some, and my particular block was usually the last one to comply with the demo streetscape. Once one car started parking correctly, others followed suit.”
- “Crowded”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- “Redundancy of bike lanes on both sides of street made it congested.”
- “One lane bottle-necked traffic. I hated hated the experience on driving on Livernois during that period. Visually the area looked junky & unappealing. The street in both directions is not wide enough because of the grassy median to allow for bike lanes that wide. If the goal was to get passersby to notice the shop, it failed. Drivers are concentrated on the bumper to bumper traffic.”
- “They were annoyed. I’ve lived in the area for 35 years and have not felt that Livernois was unsafe for bicylists and pedestrians.”
- “Didn’t like it ... thought the plan was not well thought out.”
- “Driving on Livernois was hazardous with the parked cars in the right lane, traffic was very slow.”
- “There was parallel parking vs angle parking; I like how Ferndale has the bike lanes created excitement, created additional ways to travel.”
- “More congestion, confusing, more traffic.”
- “It gave people the opportunity to ride their bikes and feel safe doing so and it also slowed down the traffic, which is needed on a busy street.”
- “I am cautious of the bikers but traffic is more congested.”
- “Too congested, not enough space for vehicles to park. Cars are parked half way into the street.”
- “Logistically, the traffic is too heavy to maintain bike lane and cars. need double lanes because of the increase in businesses to the area.”
- “Allowed more time to view the various businesses.”
- “Awkward placement of Bike Lanes and parking lanes were not easily received.”
- “The idea is good and the street needs both permanently, but the schematic needs to be rethought.”
- “I avoided Livernois.”
- “More congested and traffic backed up; awful.”
- “It was a bit congested with people trying to figure out the change. Traffic was slower.”
- “Surrounding residential had not traffic trying to negotiate Livernois changes.”
- “One lane of traffic didn’t. Work traffic was backed up one lane will never work the cars were always backed up you could not turn, at 7 & Livernois was unbearable I took other routes know what will never work.”
- “Traffic bottle necked. Too congested.”
- “I can see the potential of new development along livernois, with more traffic.”
- “Northbound traffic from McNichols was congested, making my commute home more frustrating.”
- “Traveling in the Streetscape zone seemed more dangerous because of the parked cars, and motorists that were unaware that cars were parked would impede drivers in the drive lane.”
- “It was more walkable and made me want to actually visit the stores on Livernois. I went more often to Livernois once the streetscape was put up than at any other time (I’ve lived in the area for 6 years).”
- “It felt more like a business district than a street designed for people to speed down.”
- “The increased traffic made it worse. It doesn’t seem that everyone knows how to use the bike lanes.”
- “It was congested. Not safe. More traffic. Too busy of a street to have 1 lane AND islands.”
- “Cars should park along the curb not in the right lane.”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- “Not enough room to make “Michigan lefts” in larger vehicles. Had to back up while completing turn.”
- “It was a BIG MESS. I stopped frequenting businesses on Livernois during the experiment.”
- “I found it congested and unsafe.”
- “Cars went slower and had to pay more attention.”
- “I used Livernois to get to Ferndale instead of taking Woodward. This made me notice new businesses on Livernois that I wouldn’t have previously noticed.”
- “I was able to interact with a greater number of people.”
- “Taking the Ave. down to one lane is a traffic cluster “bleep” and cars still seemed to park wherever they pleased.”
- “Traffic was bagged up. Especially on weekends when people would patronize businesses.”
- “It was harder to drive and awkward to park. But, it did seem more dense and lively.”
- “Loved seeing more people walking around & biking. We weren’t confident enough in people being used to the demonstration to bike with our baby trailer in the street though.”
- “Traffic was congested. There was no room in case of an accident, police emergency, or an ambulance.”
- “More pedestrian friendly.”
- “Confusing, was unable to tell driving cars or parked cars.”
- “Constant traffic backups before crossing Seven Mile. Parked cars were just too close to the street. Thought it was very congested and problematic for driving in the area.”
- “Almost felt I needed to change my route just to avoid the area.”
- “I didn’t like the bike lanes and the parking lane seemed out of place. The street felt crowded.”
- “Livernois is the only major street between the Lodge Freeway and 8 Mile Road. The bike lanes and buffers made it much too slow to drive through and the buffers were very unattractive didn’t like traffic bottlenecks it cause.”
- “The demonstration seemed to slow down traffic and create a more walkable downtown area.”
- “That’s great! Creating a more walkable Avenue is the direction we should be headed. I indicated that I drive to Livernois but that is primarily because I feel the avenue is not safely connected to the surrounding neighborhoods to bike there - traffic moves too fast on Livernois and 7 Mile. I’m not sure if there is any bike parking on the avenue.”
- “Terrible traffic back up due to only 1 driving lane. UGH!!”
- “We walked Livernois instead of parking and going to just one location. But, that was because of the jazz festival. Traffic increased greatly though, one lane of traffic doesn’t seem realistic.”
- “LESS SPEEDING. DRIVERS. People were forced to obey by posted speeds, I was able to ride by and see the developments and stores it’s like to visit that I was previously not aware of.”
- “I was able to ride my bike in the bike lane.”
- “Slower driving. Got a chance to survey the businesses. A Good thing.”
- “Helped slow traffic down.”
- “Took longer to drive Livernois so I avoided it as much as possible, but since I stay one street over it didn’t help any...”
- “I didn’t mind slowing since I’m seldom under a deadline.”
- “Overall I feel more comfortable being on Livernois and have begun to walk my dog along Livernois.”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- “Very confusing. I think this would be different if it was a permanent change, because I would ride by bike there more if there were such safe lanes.”
- “The traffic was too congested, forcing drivers to use the residential areas to go from McNichols to 8Mile.”
- “Was heavily congested, dangerous and seemingly confusing to some drivers.”
- “Felt safer riding my bike.”
- “Vehicle traffic on Livernois south and north of 7 miles was severely congested, taking quite a long time to get through the intersection. The merge lane south of 7 mile impacted Livernois as well as side residential streets. Many cars, including myself, were more likely to use neighborhood streets causing more traffic on those roads.”
- “People seemed confused by where to park, so there were cars parked consistently on Livernois.”
- “I like the idea of bike lanes on Livernois, but perhaps they should be on the median.”
- “Traffic congestion, motorists using the university district as a high speed bypass and putting children at risk.”
- “Traffic congestion led to more traffic in the University District.”
- “Traffic was horrific. I had serious issues going north on Livernois. A lot of traffic redirected itself onto Warrington, which also posed a problem. I live on Warrington.”
- “Traffic was awful throughout the experiment.”
- “There was limited traffic and there is already limited parking and the traffic was outrageous.”
- “There was limited parking!”
- “Traffic was confused and bad and it was a rather confusing and non-aesthetic way of doing things.”
- “Speeds were lower and it would be nice as currently if I bike I stay in the neighborhood streets and away from the major roads due to careless driving and passing at high speeds illegally on the right.”
- “Traffic was more congested and it just felt extremely awkward (and dangerous) to park cars on the outside of those bike lanes.”
- “It was more difficult to navigate and park.”
- “The change made no sense, horrendous!”
- “Traffic on Livernois was jammed up, every day was like rush hour on the expressway, bumper to bumper.”
- “My experienced was worsened by the traffic on Livernois. Having only one lane of traffic in each direction slowed traffic considerably. I personally never felt safe parallel parking because it obstructed the one lane if traffic. It also feels like a safety hazard as there is no way for a fire truck or paramedic to safely navigate this area without being held up in traffic.”
- “Lane Changes complicated driving.”
- “I think that the bike lanes are a great idea!”
- “Traffic in that area was so bad that I avoided it altogether and didn’t visit the shops and restaurants I normally frequent for that reason.”
- “Bike and pedestrian friendly spaces. Ring people together.”
- “Traffic was backed up. I avoided the area.”
- “It didn’t change my experience, but I did hear some negative comments about Livernois.”
- “Love the bike lanes, however the islands in the middle create more problems with traffic flow than improvements. Because of island in the middle of livernois traffic through the neighborhood is absolutely terrible. Warrington specific has become very unsafe with high speed pass through traffic.”



## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

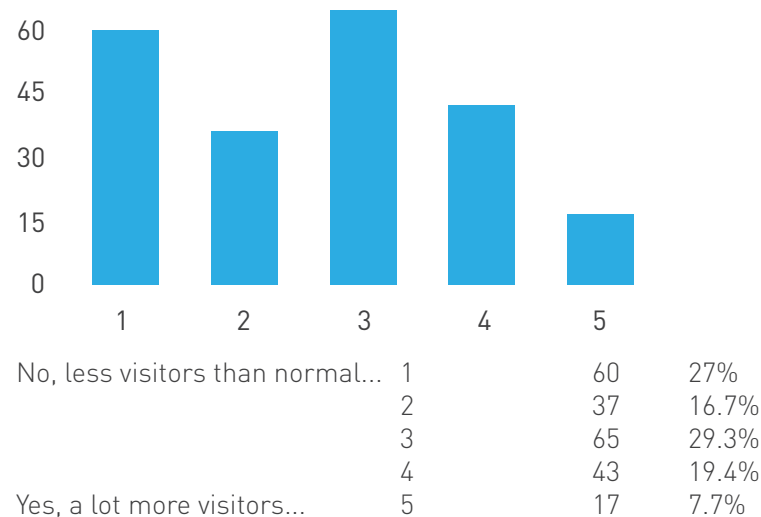
- “North/South traffic blocked east/west traffic at seven mile and Outer Drive.”
- “Much more driving congestion due to reduction to one lane.”
- “I drive down Livernois daily and during the entire time I saw only 2 bike riders during my northward trip at 9:00 am and none during my 12 pm trip. It slowed traffic down and seemingly to no avail.”
- “Traffic backups occurred during rush hour, or when cars stalled in traffic with only one lane open congested traffic, slower drive time.”
- “The protected bike lane and row of parking made it extremely difficult to see when coming out of the neighborhood on St. Martin. Also, reducing Livernois to 1 lane created a traffic backup at 7 mile and increased my 20 minute commute to and from work to 30-35 minutes. Lastly, car traffic began to avoid this area for the same reasons I assume which increased thru traffic on Warrington making our neighborhood less safe with speeding and traffic.”
- “Having the Parking lane moved farther from the curb made the street feel visually crowded and blocked my view of the stores. I feel a bike lane is important but would like to see a model more like Ferndale’s in which the bike lane is between the cars and the parking.”
- “The flow of traffic was extremely slow. I welcome the bike lane but could we consider how Ferndale has their bike lanes. Due to space, it seemed like you were parked in the street. The demonstration layout put the driver closer to the flow of traffic.”
- “It did not change. There was still the feeling of congestion and there was not a clear or reasonable explanation of the experiment to the general public.”
- “Vehicle congestion, bike lanes terrible for parking with blvd, witnessed several dangerous possible accidents.”
- “I often bike ride on Livernois, and I do not feel very safe riding with traffic between 7 and 8 mile.”
- “This type of streetscape is necessary!”
- “Congestion during rush hours and Saturdays. An overall tightening of the traffic pattern made driving Livernois an unpleasant experience.”
- “It was ridiculously slow and congested. I did not like it at all. I don’t feel that we need a bike lane.”
- “The space on Livernois is already tight and we do not need to make it smaller.
- Impossible to park. The bike lane is in the wrong location. Parking should remain at the curb with a wider bike lane between the parked cars and the traveling motorist. The bikes are so few that having the lane set up this way would improve parking and would still be safe for the bikers.”
- “Confusion, dangerous, trash scattered everywhere, traffic back ups.”
- “Difficult for SUVs to make a blvd turn and hang out into the one existing lane until clear. North bound lane transition @ 7 mile (split w/ a right turn lane or straight) was a nightmare as the right turn lane would try to go straight and run down delineator posts.”
- “During the streetscape demonstration I experienced a lot of congestion on Livernois. So much so that I avoided traveling through the fashion district due to the added congestion. To be honest this is the worst decision you could have made on Livernois.”
- “There was so much traffic that I avoided using Livernois during that period.”
- “Didn’t know about it.”
- “Got a closer (slower) look at the businesses - who’s there, the windowscapes, etc.”
- “The visibility and walkability enhanced the overall experience of the business and neighborhood.”
- “I didn’t feel it was safe for the bikers or drivers. When it was busy it caused backups.”
- “Had to park my vehicle in the middle of the street too much traffic, non-eventful!”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

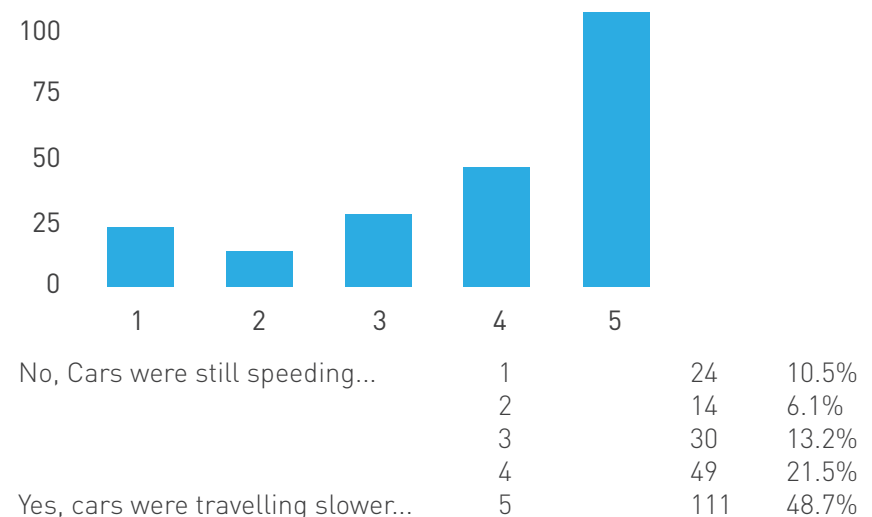
### 7. Please describe how your experience of being on Livernois changed during the streetscape demonstration.

- "I, along with my clients did not feel safe parking in the middle of the road."
- "The parking was horrible, I work on livernois, the clients were complaining concerning parking in the street clients were complaining."
- "It was very confusing. Clients received tickets and told us they were never coming back."
- "Traffic still speeding. Cars driving in parking lane, it was terrible!!!"
- "Traffic Back up. No place to park."
- "Everything was hectic. Parking was difficult. Livernois street is too small for a bike lane. Traffic was backup but did not increase in the business."
- "It was horrible and it made the street seem smaller."
- "Too many traffic jams- slow livernois was down to one lane, parking was a mess, too much traffic and still cant find parking."
- "I enjoyed some of the decorative changes, however, the bike lane was not a good idea considering the massive level of traffic and the fact the median is a factor. Clients complained of less parking spaces and safely exiting vehicles out into street. Dangerous for fire trucks and emergency car to pass by with one lane."
- "I realize that Livernois needs a designated parking area besides street parking."
- "Livernois was congested, I went down side streets being down to one lane to drive was very bad. I held up traffic trying to park."

### 8. Did you notice an increase in bike and pedestrian traffic and/ or visitors to Livernois during the demonstration?

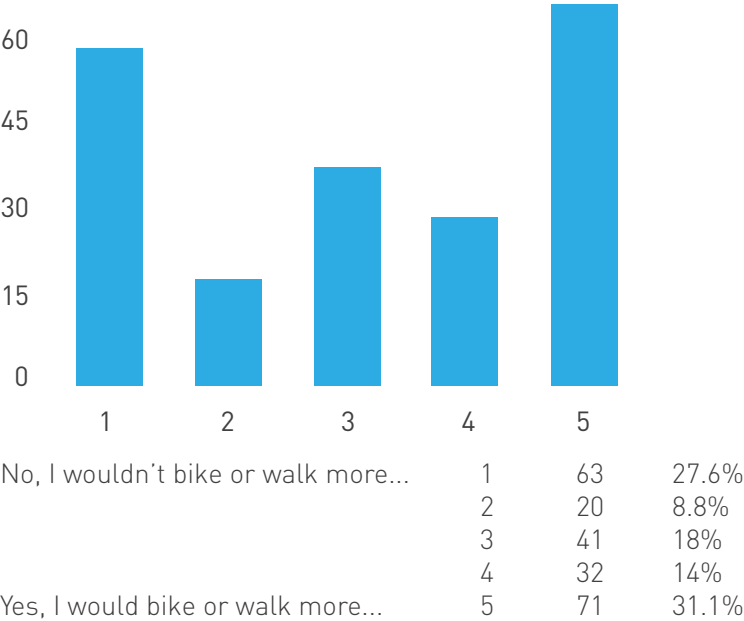


### 9. Did you notice an impact on the speed of cars traveling along Livernois during the streetscape demonstration?



## > IMPACTS OF STREETSCAPE DEMONSTRATION

10. If the streets were safer for bicyclists and pedestrians, would you be more likely to walk or bike to Livernois?



11. Did you notice any neighborhood streets affected by traffic during the installation? If so, which streets?

- Warrington
- Going North on Livernois at the Mile intersection
- Livernois
- Warrington and Stoepel
- Warrington had a lot more traffic south of 7 Mile
- Warrington
- Stoepel , Santa rosa
- Cambridge and St. Martins
- Side streets were cludderded with cars
- Warrington to Livernois. (St. Marten especially)
- McNichols
- Livernois in the Sherwood neighborhood
- Streets adjacent to Livernois, 7 Mile and Outer Drive

## > IMPACTS OF STREETSCAPE DEMONSTRATION

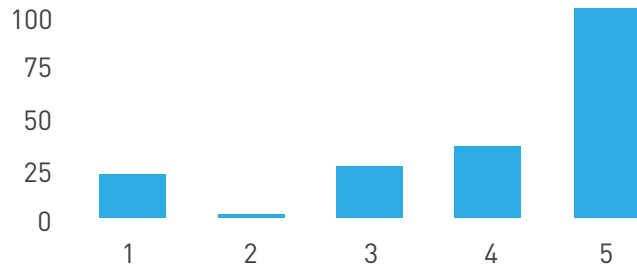
11. Did you notice any neighborhood streets affected by traffic during the installation? If so, which streets?

- W. Outer Drive and Livernois Ave.
- 6 mile and 7 mile and west of Livernois
- Outer Drive, Curtis, Pembroke and other side streets.
- Sherwood Forest & Green Acres due to traffic congestion on Livernois.
- Warrington, oak Dr and Clarita
- Clarita
- Yes, Warrington, Clarita, Margareta
- Clarita and Oak Drive
- Yes, mainly Warrington, both north and south of Livernois
- Warrington drive off Clarita
- Sherwood Forest
- Warrington Dr.
- Warrington, St. Martins, and Cambridge Rd.
- Martin
- Canterbury
- Sherrwood and Green acers neighborhoods
- East of Livernois and West of Livernois
- 7 mile
- Stopel & Warrington
- 7mile/Livernois
- Stopel
- Cambridge, Outer drive, Stopel
- Outerdrive
- Warrington
- Livernois/8 mile



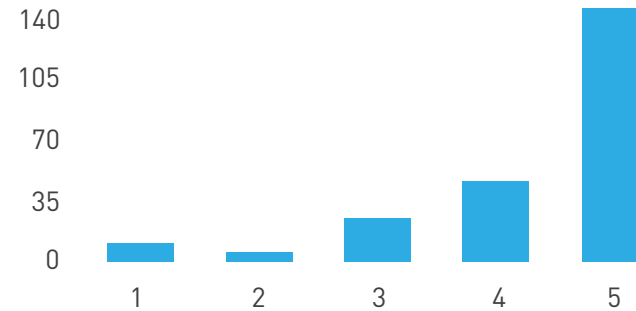
## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 12. Does Livernois need more and improved crosswalks?



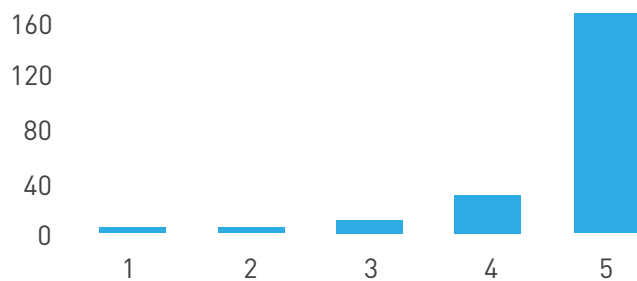
No, I don't think so...	1	27	11.9%
	2	3	1.3%
	3	31	13.7%
	4	42	18.5%
Yes, add and improve crosswalks...	5	124	54.6%

### 13. Are benches on Livernois a good idea?



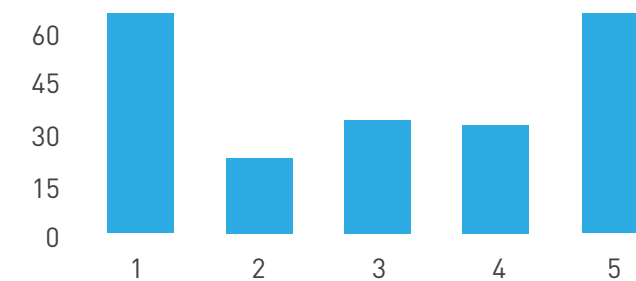
No, I don't think so...	1	10	4.4%
	2	6	2.6%
	3	25	10.9%
	4	45	19.7%
Yes, add benches...	5	143	62.4%

### 14. Does Livernois need more, improved flower planters?



No, I don't think so...	1	2	0.4%
	2	3	0.6%
	3	11	4.8%
	4	32	14%
Yes, add Planters...	5	175	76.4%

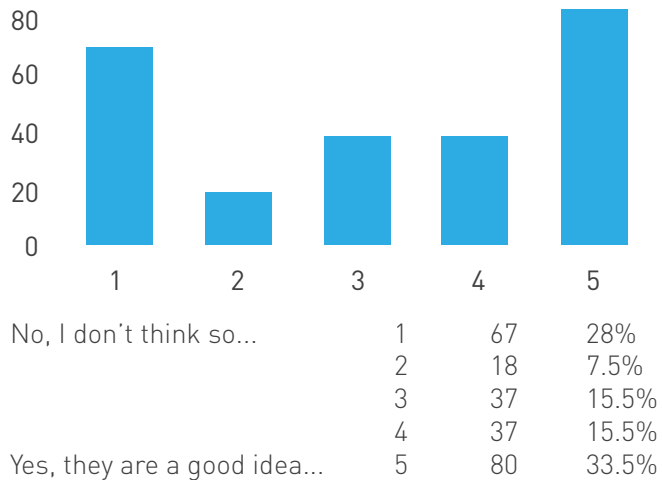
### 15. Are bike lanes on Livernois a good idea?



No, I don't think so...	1	69	29.7%
	2	24	10.3%
	3	36	15.5%
	4	34	14.7%
Yes, add bike lanes...	5	69	29.7%

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 16. Are curb bumpouts on Livernois a good idea?



### 17. Do you have any other comments regarding the Livernois streetscape?

- "I'm all for anything that makes the area feel less industrial and more "down town-y" as long as it is maintained and clean."
- "Slower traffic, cleaner streets and planters will make shopping much more desirable!"
- "A more even walkway would be appreciated. I have almost fallen because of uneven and hazardous sidewalks."
- "There's a strong need for bike parking along Livernois"
- "A bike lane is okay but with it so large there is just not enough room for the Livernois traffic."
- "There is no historical pattern of significant bicycle traffic on Livernois requiring bike lanes."
- "With cross walks bicyclists could continue to use side streets to access Livernois without impacting traffic or impeding business on the Avenue."
- "Business owners need to take better care of their properties. Even during light up livernois there were tons of weeds in front of businesses."
- "Livernois needs beautification but no modifications to the streets and surely no decrease in the number of lanes."
- "The median in the center made it very difficult to park and get around with the bike lane."
- "If the median stays, it should have more landscaping and better maintained."
- "Be sure bumpouts do not interfere with the already tight turns through the median."

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 17. Do you have any other comments regarding the Livernois streetscape?

- “Bike lanes on 7 mile to Palmer park from Livernois.”
- “For the sake of the 21st century, and knowing how well this works in Europe, I hope the bike lanes go through. People who live in these areas tend to be closed minded and have less cultural experiences with what the future could really be like.”
- “Keep it up! We are new additions to the neighborhood and really enjoy seeing this progress.”
- “Needs parking.”
- “The bike lanes are a not needed.”
- “More pedestrian friendly ideas is just what the Avenue needs to improve foot traffic and business.”
- “I would be in favor of bike lanes had the city not installed the medians. The medians narrowed each side to only two traffic lanes. Installing bike lanes narrows it to one lane. That’s not enough lanes for a major thoroughfare like Livernois.”
- “I feel the bike lanes cater to a small minority of people. I’ve found bikers to be rude and inconsiderate. And the Slow Rollers are the worse. And since when do bikers have the luxury of disregarding street lights and signs?”
- “The side walls need to be pushed out to allow for more pedestrian traffic. Also the bike lanes need to be protected by planters.”
- “Adding the medians to Livernois was a mistake. We should have finalized plans/installations before boulevards were installed.”
- “These changes force more traffic and strangers into the residential area. Also, more trash was observed.”
- “What’s the point of a curb bumpout if it forces the bike rider into vehicular traffic? Maybe I’m missing something.”
- “Thanks for considering the neighbors in the planning process.”
- “The streetscape would be better without the center grassy section that takes up too much of the street.”
- “I like the idea of planters. However, if the city is not going to service them and keep them free of weeds and trash, then no. The city does not have a good track record keeping track of the grass let alone a flower. Weeds grow and clippings are left in the street. I see other communities and I grow angry at the lack of care and a unprofessional job.”
- “Convenient Parking is key. Some streetscape ideas will dump traffic onto residential streets that are adjacent to Livernois.”
- “1) Flower pots are not necessary, but store owners need to do a better and more collaborative job on landscaping of current flower beds imbedded in the sidewalks. 2) I would prefer to see the bike lanes setup on left hand side of parking and to install curb bumpers. Look at configuration on Livernois between 8 & 9 Mile Roads. 3) Would really like to see sidewalks extended to allow for cafe seating outside. 4) Would like to see alley way configured similar the alleys in Midtown near Avalon/Motor City Brewing area.”
- “The sooner you get started on the changes the better!”
- “I understand trying to make the avenue of fashion more user, visitor friendly, but bicycle lanes are not the answer.”
- “Curb homeless sleeping on benches.”
- “Bike lane on one side ONLY and ANGLED parking on one side ONLY”
- “The median has severely limited lanes on Livernois- from three lanes in each direction, to two. If bike lanes are added, there will be one lane in each direction. The street is too busy to be a one lane street in each direction. If the median was taken out (which was totally unnecessary and the community fought against) then bike lanes would be feasible. If bike lanes are permanently installed, I will avoid using the street, because of the bottled-necked, bumper-to-bumper traffic that would ensue. I must add that as a long-time resident, I feel decisions are made to put money in the pockets of contractors who get the jobs, not to follow residents’ wishes or needs. I appreciate this opportunity to vent; however, I feel that the powers to be will ultimately do what they want. Asking for my opinion in just an exercise for you

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 17. Do you have any other comments regarding the Livernois streetscape?

- so you can say you solicited neighborhood voices. I totally feel helpless and put upon.”
- “Maintenance plan or strategy needs to be placed as many of the City’s efforts have wasted due to lack of maintenance of the public spaces.”
- “Let’s get started!!!!!!!!!!!!!!!!!!!!!!”
- “All the questions asked and Ideas are great and I hope to see the improvements in the near future.”
- “I think slanted parking and a more narrow median would be beneficial.”
- “As businesses increase, the need for parking spaces and lanes to drive in will be needed. There is a low % of people who ride bikes in the area.”
- “Install more plants and art installations.”
- “All of the above are good ideas that are sorely needed. The bump outs could also be the solution to differentiating bike lanes and park. I would also consider the addition of permanent works of art from Local (CITY OF DETROIT ARTISTS) that reflect the community. The bench seating sample above is perfect. Planters should be properly placed and xeriscaping plants that bloom yield great florals and vibrant colors would be sublime. Getting the U of D School of Design, community gardeners, and local artists ( talk to Tony Hughes) together to plan the street from the McNichols to 8 Mile and preferably (Lofge to 8 Mile Road) would be a winning collaboration. The Knight Challenge is ongoing right now and a half million for that would be money VERY WELL SPENT. I would look at offering existing and new businesses seeking to stay or locate on the Ave some professional design services via the U of D School of Design.”
- “A very bad idea on top of another ill advised idea of a median on a street that is too narrow. It is more hazardous and congested.”
- “Yes please do something about that awful display of clothes at the African boutique on Livernois/7mile; we can not be world class with that look.”
- “The streetscape should use a buffer between the driver and the traffic.”
- “Remove that eyesore of an island or median that is in the middle of the street. It has too many signs on it and is ugly, takes up too much space and make me drive too far to get to where I want to go. I HATE IT!!!!”
- “Let’s get it done.”
- “I am happy that these changes are being considered. I look forward to the results of this survey, if possible. I do think that while we are working to increase safety, and esthetics in the area, we should consider that a few changes might have a greater impact than many changes. Too many changes can have unpleasant consequences for the community.”
- “I love all of the above ideas. The one thing that I would love is the addition of background music, similar to what Ferndale has on 9 mile. I would also like the sidewalks to be wider so that more people can walk and restaurants/ businesses can set up tables.”
- “Remove the median.”
- “Keep up the good work! Our neighborhood is getting better and better.
- Why do you always limit development from 7 mile to St Martins, what about 7 to 8 mile Any part that takes 2 lanes down to one is a nightmare.”
- “More trash cans!”
- “Again, all for the improvements - let’s just ensure the neighborhoods benefit & do not suffer.”
- “This entire project has been a BIG mess beginning with the island in the middle of Livernois & then the additional “bike lanes.”
- “Add planters and benches. Remember Livernois has more traffic than 9 mile in Ferndale. You can’t try to mimic that area because the traffic flow is totally different. I never did see any bikers in the bike lane. Not one time. I Drive up and down Livernois everyday going to the post office and various business. Please for safety sake leave it as is.”
- “What is needed on Livernois is more parking.”



## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 17. Do you have any other comments regarding the Livernois streetscape?

- “The businesses that have been in operation on Livernois have been doing a wonderful job!”
- “They’re great, but there are still a lot of empty store fronts that have trash in them and there is a fair amount of overgrown landscaping - walking around in this is just not pleasant, especially with traffic rushing by at 40+ mph.”
- “If only the median could be removed!!”
- “Solidify the businesses south of mcnichols. Fix the dangerous intersection at mcnichols and livernois. Far too dangerous turning left in a car, let alone trying to cross as a pedestrian or bike.”
- “Some of the trees need to get thinned out. New sidewalk would improve the look in front of many of the stores.”
- “The streetscape on Livernois is a terrible idea. If you want to do that remove the island. I’ve been in this neighborhood 30 years and it’s always been shopping and didn’t have a problem with pedestrians. The problem came when that island was put into an already small street. If you do that garbage it will make it unsafe for pedestrians especially if an emergency should happen. Because making it one lane of travel emergency vehicles can’t get through. If you want a bike lane eliminate parking on Livernois. Or remove the island so the street can have the turn lane and that extra lane of travel that was removed.
- “The temporary project stayed up too long and was not maintained. Many near accidents due to parking confusion. If project remains for an extended time semipermanent products and signs should be installed.”
- “It’s a valuable experiment.”
- “I’m excited to see more positive changes with Livernois. I’m hopeful the upgrades towards a safer streetscape will attract more businesses and visitors to the area.”
- “Free parking”
- “Having Bike Lanes and Curb Bumpouts are good ideas, BUT Livernois should still have two lanes going north and south for cars. One lane in each direction (north and south) is not enough and it made traffic too cumbersome. Use the yield crosswalk signs that most college campuses use to slow cars down.”
- “Bike lanes would be great in the median instead of on the side of the streets.”
- “The median inhibits the space for bicycles, vehicles, and pedestrians to use Livernois.”
- “The bike lanes were always empty - the traffic calming was nice - except for the side effects in the nearby residential areas.”
- “I understand the need for bike lanes, but the protected bike lane did not work for Livernois. If Livernois were three lanes, each way, then a bike lane would work. There was nothing that could be done for the DDOT bus that had to wait due to a disabled vehicle. I was very glad I was not behind the bus. This disabled vehicle COMPLETELY halted traffic traveling north on Livernois.”
- “If bike lanes are added, need to incorporate them with changes to median so that traffic is not impacted along Livernois.”
- “MORE parking = MORE crosswalks”
- “Please do not go ahead without local input! The solutions used in the test ARE NOT WORKABLE! We will need a different plan to improve - but please continue on this route!!”
- “It is unfortunate that a median was placed on Livernois and removed one of the driving lanes. If this had not occurred maybe we’d have a lane that could be used for cycles but now I am against any bike lanes on Livernois.”
- “Livernois needs better parking options, and more of them.”
- “Only concern is with street parking there needs to be two lanes and the current center median is a bit too wide.”
- “What we really need are more intervals for turning around on Livernois. Cars have to go way too far before an option to make a Michigan-left is presented.”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 17. Do you have any other comments regarding the Livernois streetscape?

- “I think people will still speed because that is just the norm here. I live on a narrow residential street and people drive unbelievably fast. I think speedbumps and enforcement would be more effective to prevent speeding.”
- “Why not put bike lanes on streets that aren’t main thoroughfares where there are fewer cars and it is less terrifying to ride?”
- I think the bike lane is a bad idea in general given the amount of traffic on Livernois. It was however poorly implemented which caused it to be viewed so negatively. Having lived in Atlanta for many years I’m familiar with bike lanes and they aren’t usually as large as the one on Livernois. They also are normally outside the parking lane rather than inside the parking lane. If the bike lane is approved the only safe way to do it is by removing the medians on Livernois so that we can keep 2 lanes of traffic in each direction and by putting a narrow bike lane (large enough for one biker at a time) on the outside of the parking lane.”
- “I would not like to see the changes made as a test be made into a permanent change. The traffic that ends up on the side streets is dangerous for our children. Instead of speeding on Livernois people now speed on side streets.”
- “I think the streetscape would be more attractive if the center islands were reduced or eliminated, widening Livernois. Streetscape beautification can happen without cutting driving lanes. The median has already cut the positive traffic flow on Livernois. That was also an ill conceived change.”
- “I live in the first block of Warrington, north of seven mile road . I would appreciate your efforts on Livernois more, if you would be considerate enough to include a plan to keep drivers turning on our street to avoid the traffic slow down created by the bike lanes from doing so. My street is being use as an alternative route.”
- “I think the center section would be better used as parking if a reasonable safe and easily accessible configuration could be designed.”
- “Due to having only one available lane, emergency vehicles were slowed considerably, which could prove to be catastrophic.”
- “I would like to see banners acknowledging location or neighborhood. In addition, more uniformed storefronts Livernois should have a more intimate small downtown feel with twinkle lights like. a plaza in Europe.”
- “More trash cans & lighting.”
- “I’m not interested in bike lanes if they take space away from parking and driving. Why not figure out a way to incorporate bike lanes into that God awful median so that Livernois can at least have 2 lanes of traffic in each direction.”
- “No further reduction of driving lanes. It causes an increase in use of the neighborhood streets being used to bypass increased traffic on Livernois there’s not a lot of sidewalk width (store fronts are very close to streets) and can be hectic during sidewalk events (we want it to be like that all the time or at least on weekends). Some sidewalks are uneven (brick v concrete) can cause a tripping hazard if not maintained well.”
- “Metered parking was painful, and wish it would’ve been installed once the area really was reestablished and took off (many complaints from bus/res).”
- “I don’t mind having a biker lane but this was an awful plan. If you were trying to make a michigan uturn there was no room for a bigger car to make a because there were cars parked on the outside lane. There was a problem with the buses. First since there is only one lane the busses now hold up traffic and the passengers had to walk between parked cars to board the bus.”
- “More bike racks.”
- “Make the median narrow - it’s too wide for a bike lane too.”
- “I’d like to see more parking, even slanted parking.”
- “Needs to be a safe environment for all.”
- “Bike lanes in median.”
- “We were lucky we didn’t have an emergency- fire, accident; or a stalled car- results would be horrifying.”

## > IMPACTS OF STREETSCAPE DEMONSTRATION

### 17. Do you have any other comments regarding the Livernois streetscape?

- “Have to see if its beneficial to business.”
- “The bike lanes are dangerous and the drivers don’t care they tried to run over a biker.”
- “The bike lane is a hardship for business & elderly (dangerous). Traffic is messed up. This is dangerous and our money is spent to our detriment. We need fire & safety patrols not flowers.”
- “The center median is just awful. Too far to make your turns.”
- “The Ave of Fashion would like to see more landscaping fas Flower.”
- “It’s not a good idea.”
- “Love the idea of bike lanes, but with the median on livernois, they cause many traffic jams bike lanes wound’t be so bad if the median wasnt there unhappy with the current design. Good idea, but current design not approved.”
- “It looked terrible. I lost business as a result. I’ve been here since 1979. It will not work.”
- “Livernois needs to have two lanes if at all possible.”
- “After the informative meeting, I believe the change will be good for the area it didnt work.”
- “Not safe. Drove customers away.”
- “I love the idea of bike lanes, two lanes of traffic and a parking lane together backs up traffic and discourages people from driving down livernois.”
- “I don’t like livernois down to 1 lane- more traffic going into neighborhoods & crime risk for emergency vehicles against livernois taken down to one lane, which creates problems for emergency vehicles.”

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- More restaurants
- Restaurants
- Drive Thru coffee shop, children’s toy and or modern natural baby, young adult work appropriate clothing, shoe store, Detroit themed gift shop. Restaurants and bars.
- Affordable mens clothing, deli, health food store
- “Ethnic restaurants, apparel, cafe/bookstore, health food store, wine shop, specialty market, tea house, casual dining, lounge, high end consignment store, dentist, dermatologist, spa, health.”
- club, Health food store
- “Health food/organic store. Coffee and/or tea shop. Vegan/Vegetarian restaurant. Upscale quality boutiques. Flower shop, Chocolate store, Fairtrade, Bread Bakery.”

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- Motor City Brewing, more black-owned local businesses rather than chains.
- More restaurants and retail.
- More service businesses.
- Restaurants/bars, coffee shop, co-working space, home decor
- I think Livernois is headed in the right direction with the increase of restaurants, boutiques, and coffee houses.
- Nail salon, coffee tea shop, kids play place, a music club for live music with space that does not just cater to jazz.
- "Bars, diversity in types of restaurants, ice cream shop with better hours, clothing stores and businesses for daily necessities."
- "Restaurants and a real coffee shop. Bars, galleries and a nicer grocery store."
- Sushi
- "Upscale to mid range dining and retail/Something that fosters diversity."
- Galleries, coffee shops, gyms, yoga studio, pop up shops, restaurants.
- Restaurants, Retail, and other needed bussness for the community.
- More boutiques, specialtie stores.
- Brewery and restaurant.
- "A cafeteria-style restaurant that is fast and convenient with a family atmosphere."
- "A serious shoe store and a serious ladies clothering store."
- Home Decorating
- Restaurants, cafes tech labs for residents without computers etc.
- Private Lounge that serves alcohol.
- The ones here are great! I sincerely hope there are no plans to push anyone out!
- Less hair salons and fashion stores. More restaurants, coffee shops, book stores, pool hall/ping pong table, arcade with drinks. Also, the businesses need to be smaller. A lot of current businesses could fit 2 or 3 businesses in their space.
- Specialty shops like the kind that are there - just more of them.
- "Premier retail shops without nicely appointed windows. NO MORE RESTAURANTS-THEY ATTRACT RODENTS!"
- Fruit and Vegetable Market, Coffee and Pastry Shop, Small Cafes (with outdoor seating), More coffee shops and restaurants.
- "I would like to see a Resturant that sold nothing but healthy salads, a Parnera Bread Resturant."
- Ice cream shop
- "I think we have a nice variety now. Some buildings need to be upgraded or removed."
- "I am satisfied with the businesses that are there."
- Shoe store
- Maybe an ice cream shop.
- Restaurants, Pubs (Irish Bar, Piano Bar, MSU Bar)
- "Ones similar to those already there (eateries, ice cream, sit-down restaurants, upscale."
- "Boutiques, books, greeting cards and gifts. Discourage businesses that hang clothes outside."



## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- Book stores
- Need a parking structure, more dining choices and more retail.
- “Bookstore, Community/Co-working space, Sandwich Shop, Health Food store, movie theatre, Mani/Pedi shop similar to “Ten” downtown, ice cream shop.”
- Restaurants, clothing stores, bar.
- “More restaurants, coffee, shoe, floral, book store, childrens store, more men and womens clothing stores, home furnishing, music store, hat shop, ANY TYPE of specialty store!”
- “More foot traffice type of businesses, ie starbucks, sit down restaurants, but we need more parking for vehicles.”
- A variety including casual dinning and coffee shops.
- Upscale retail
- Shopping, dining, movie theater.
- Mixed services, offices, shops, galleries, resturants.
- “Youth targeted businesses. There are not enough facilities aimed at youths which would of course cut down on a lack of direction in older youths and potential problems.”
- More sit-down dining.
- A department Store
- “Coffee shop, home goods type shop, more sit down resturants (not carry out), clothing store not related to “exotic dancing” but to the 9 to 5 people.”
- “More, restaurants, shopping, entertainment, spas,gyms, housing, office. specialty grocery store.”
- “Open seating restaurants, live jazz on the ave, coffee shop, vegan restaurant, health food store.”
- Wi-Fi Hub and Cafe
- Fresh produce and deli mart
- “Gigar Bar, Wine Bar, More upscale shopping, more eateries (NO MORE SOUL FOOD RESTAURANTS PLEASE WE CAN DO BETTER THAN THIS). Collective of local jewelry and fashion designers, yoga studio, gourmet food emporium, childrens’ toy store, childrens’ shoe and clothing store, Spanish eatery, French cafe, QUALITY Cajun eatery.”
- Flower/card shop; gourmet food market, wine shop.
- “A wi-fi spot for non brick and mortar businesses or students after libraries have closed.”
- “A bookstore and a falafel/shwarma restaurant. We need an independent movie rental place like Blockbuster.”
- Mix-use, mix-income
- A small theater (for plays), casual family dining, a spa, and a nice community retreat.
- Bakery (bread). Butcher. Vegetable stand. Bookstore (which we almost had!). Higher-end clothing shops (think Flo in midtown, rather than club wear: flowingflava.com). Wine shop/bar.
- Coffee shop. Adult fitness studio (yoga, pilates). Restaurants open for dinner (sushi, Thai, etc.)
- More places to eat, places to pick up small household items, yoga studio, coffee shop
- Coffee shops; bookstores
- Coffee shops, Breweries
- Healthy food options, micro brewery

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- More jazz clubs
- Mexican restaurant, bar, retail
- “More restaurants! Coffee shop. Entertainment (places with trivia, billiards, family friendly, theater/movie, meat market/butcher, farmers market).”
- “Restaurants, ice cream & coffee shops, boutiques, places to take the family after work on a weeknight.”
- Bars and restaurants
- Boutiques
- “Quality restaurants, upscale clothing boutiques, parking structures.”
- “All types--would like to see coffee shop and more restaurants.”
- “Brewery, restaurant, maybe a small wine bar, definitely something with outdoor seating, maybe a comedy club or another performance venue, high quality nail spa/salon.”
- Restaurants and retail open passed 5pm.
- “The shops and services I normally have to leave the city for like coffee and sandwich shops, social establishments such as a cigar lounge, a Kinkos or similar independent, retail for families, LESS braid shops - we have quite enough and less business owners that display their wares like it's a flea market.”
- Craft shop and a home goods shop.
- “It's already too many. You need to enforce the way they keep their property up so the roadent problem can go away.”
- “Family friendly options, Healthy food options, sidewalk cafes, quality clothing stores Gift shop, florist, bookstore; commodities are for malls and Amazon.”
- “Fast Casual” restaurants (Chipotle, Panera, Mod Pizza etc.)
- “Cafe, healthy food restaurant, quality Chinese or Thai food, small neighborhood market like the Western Market or Farmer's Hand in Corktown- with healthy prepared foods, Whole Foods.”
- Coffee shop would be welcome. Something other than hair care stores.
- Coffee shop and restaurants.
- More diverse offering of restaurants and clothing options.
- Family oriented businesses.
- Florist, restaurants, high end retail.
- Dinning In. Health stores.
- Primarily what's there now: restaurants, personal service, retail.
- Restaurants, coffee shops, book stores. “Just the kind of places you can hang out and wander.”
- Restaurants with prompt service.
- Brand name retail and restaurants e.g. Gap and Chipotle.
- Craft Brewaries, Restaurants.
- Restaurants, cafes, theaters, good mix of retail.
- Restaurants, Starbucks, golf shop, adult apparel, bike shop.
- Dollar store, Parking...

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- More restaurants, a craft store, and second hand store.
- Restaurants, both casual dining and takeout.
- Restaurants and places to sit outside.
- Business that provide a means to be entertained.
- More food places, good fast food place.
- Amazon or Apple Store
- Deli's and coffe shops, elegant eatery restaurants, clothing stores, drug stores and grocery store.
- Nothing selling liquor!
- Small restaurants, fewer hair and nail salons.
- Something kid friendly, play area; kid boutiques.
- More diverse dining options, coffee shop, etc...
- Bookstore, cafe, clothing shops, more restaurants.
- Florist
- Coffee shop, bookstore, florist, dining.
- Yoga
- Night life, clothing, coffee shops, smoothie store, family entertainment.
- Mosre restaurants, bakeries, cafes, health food stores, etc.
- "High-end gym or workout studio. Activities for adults, BYOB painting, a good bookstore. High end mani/pedi salon. I'd like to see some franchise business on Livernois, Batteries Plus, You Break We Fix Phone store, Lululemon, mid-grade food franchise, e.g. Chipotle, 5 Guys Hamburgers."
- More Restaurants - the attraction of food would create a steady flow of visitors - much greater
- "Would love to see more retail, dining, and services (yoga/zumba/dance/ community gatherings) and less hair salons. We need more anchor stores, franchise, or names that people know sprinkled in with locals that we love. PLEASE have more events like Light Up Livernois, Jazz on the Ave, sidewalk sales, historical walking tours, or other seasonal events to draw foot traffic (quarterly at minimum). We need a newsletter with coupons or store features/ events to circulate as everyone is not on social media unfortunately. Also, store fronts need an update (contact store/property owners, banks, and designers for assistance)."
- Coffee shops with WiFi and restaurants that stay open later.
- More restaurants like 1917 and kuzzos and family oriented stores.
- Bookstores, wine bar, art house movie theater, pop up food vendors in pocket parks or parking lot.
- More clothing and furniture boutiques and resturants.
- Gym's, groceries.
- Coffee shop, sushi bar, fitness room

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 18. What types of businesses would you like to see on Livernois?

- Custom shops
- Fast food restaurants, quality retail outlets.
- “More retail businesses. Shoes, Fashion, more dinning, more healthy food restaurants.”
- Florists, vegetarian restaurants, bookstore, wine bar.
- Flower shop, hookah bar, convenience store.
- “Things we dont have! Like more good food choices and childrens toys and house ware!”
- “More restaurants, more clothing stores, coffee shops”
- Shopping, food, music.
- Bookstore, another restaurant
- “I would like to see a more diverse business district to attract all races.”
- Parking structure
- A night club, movie theatre, flower shop, bar/pub
- Coffee shops, workout, major restaurant chain

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- “Downtown Detroit, Ferndale and Royal Oak. I love that you can take an afternoon, have lunch on a patio, walk around and pop into some cool shops, pick up a bakery fresh snack or ice cream cone, do a little shopping. Then maybe grab a drink and find a spot for dinner.”
- City bird, great unique gifts.
- Kuzzos
- Ferndale, Campus Martius, Corktown, Birmingham.
- “Socratea/great atmosphere & friendly people for a tea drinker. Savvy Chic in the Eastern Market. Karen Brown the owner now how to treat her customers and she has quality, beautiful products.”
- “I love shopping at Eastern Market. “
- Green dot stables
- Birmingham Michigan
- “I enjoy the Avenue of fashion. I love finding unique in my neighborhood. I’m also dedicated to supporting small black owned businesses.”
- La dulce vita on woodward, “great atmosphere great food.”
- Ferndale, Midtown, Birmingham, Eastern market. The market, shops and restaurants are great.
- Eastern Market
- Birmingham. Walkable, has parks and parking seems to be easier 9 Mile / Ferndale



## > COMMERCIAL DEVELOPMENT QUESTIONS

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- Great Cakes N Bakes, just yes.
- Selden standard. I like the food and the casual ambiance.
- Rust belt market, Kuzzo, found sound, public house, toast. Great local businesses with good product and environment.
- "Run Detroit for my running needs. Mcshanes in corktown to dine."
- "I eat a lot at Ponderosa. No muss, no fuss."
- "Downtown Ferndale for both shopping and dining because of the location from my home."
- ROMA CAFE
- Anitas Kitchen
- Mike's Market
- Midtown. Very diverse
- Sherwood Coney Island
- "Livernois, West Willis and Canfield along Cass Corridor - convenience, variety, and neighborhood feel. Not too fancy but diverse and fun."
- "Downtown Ferndale. There is a high density of restaurants that offer different food. The area is also well-lit and very walkable. You can go to the area without a specific destination and just go where the night takes you. I am not much of a shopper. My travels there are primarily for dinning/drinks."
- Dine: downtown Detroit for the variety and atmosphere.
- Shop: Somerset for the variety of quality shopping.
- Art galleries, boutiques and restaurants in commercial areas; Downtown and Midtown.
- LDV - quality Mike's, Savon Market, Meijers - "price, selection, and convenience."
- Hot Sams and Monroe Street Steakhouse (great lamb chops and service).
- 1917 Bistro- Good food and I can walk there.
- More dine-in restaurants, national chain shopping stores.
- Panera Bread
- Mexican Village, I like Mexican food.
- American Bistro 1917. "It's in the neighborhood, good food, friendly staff, and pleasant surroundings."
- "I have eaten at Kuzzos on Livernois however, I usually visit restaurants in neighboring cities."
- Royal Oak - convenience
- "Savannah Blue Restaurant downtown and shops on Kercheval near Grosse Pointe. Food is good; parking convenient; shopping area clean, modern, and contemporary signage- no home made lettering scrawled on store fronts."
- LDV
- 1917 bistro. For its atmosphere and people.

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- “Enjoy Ferndale and Royal Oak because of walkability and diverse choices in restaurants.”
- Ferndale - ample metered street parking.
- “Whole Foods, Good Cakes and Bakes, Avalon Bakery because of the quality of their products, cleanliness and quaint atmosphere.”
- The Village Grosse Pointe.
- Meijers, it’s convenient.
- “We go the applebee’s on 8 mile and woodward, easy parking and in and out is great, also union street for the very same reason.”
- Downtown and Mid town
- Royal Oak Downtown
- Livernois: clay studio, fiber studio, framing studio, restaurants.
- “My favorite area is the cultural center area because of my love for the arts.”
- Ferndale and Royal Oak. Plenty of sit-down and ethnic varieties of dining
- Meijer, very convenient.
- It’s Ferndale - “safe, convenient and walkable.”
- The Avenue of Fashion, because it is so “close to home. I like the variety of shops and dining.”
- Granite City Jefferson/ Location,atmosphere, food, drinks, and service.
- Kuzzo’s is great, Simply Casual, Granston Bullard
- Flo’s Boutique; La Feria tapas
- “Wherever parking is free mostly or plentiful.”
- “I really do not have a favorite place but I enjoy walking to Noni’s in my neighborhood.”
- Kuzzo’s, great food.
- Shop: The Source Bookstore. “Welcoming people and great customer service. Great products.”
- “The store is clean and organized. Interesting community events. ( I find that too many places on Livernois smell slightly moldy and lack curb appeal, save for Good Cakes & Bakes, Noni’s & Kuzzo’s.) Dine: The Imperial. Good food at a low price. Interesting indoor/outdoor areas.”
- Seva, The Block, Flo - “the ambience is nice, there’s parking.”
- “Kuzzos is one of my favorite places the food great and there is a cool since of community.”
- Birmingham
- “I love City Bird because of their products and that they are within walking distance to other stores and restaurants.”
- “Noni’s for a quick bite. Pet Supplies Plus for my dogs.”
- “Partridge creek - it’s outside, good shop options. I also love Flos in midtown because of how unique their items are. “

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- “Love to eat at Shangria la, Traffic Jams, and Supinos - great food, great quality, cool spaces, and reasonably priced.”
- Ferndale - love the walkable downtown & variety of offerings.
- Somerset Mall
- Shopping-Meijer. Dining-The Whitney.
- Cass/willis area midtown.
- “In metro Detroit? I try various restaurants all over except for downtown Detroit because of the high cost to park. Went to Granite City to eat last week and had to pay \$10.00 for parking. Could have gone to the same restaurant on Big Beaver road and parked for free. That \$10.00 could have been used for food or drink.”
- “Any place that has convenient parking. I have mobility problems and cannot walk long distances, so nearby parking is a necessity.”
- “Shopping - peacock room & Frida in Detroit, dining - typically go downtown. Some favorites include Downtown Louie’s Lounge, Batch, La Dolce Vita (not downtown), midtown / downtown due to variety of options.”
- “Varies on time of year but eating and walking the riverwalk is pretty great.”
- “Dining is downtown right now because of all the options.”
- Good Cakes
- “I don’t shop or dine on Livernois. No place to park.”
- 1917 Bistro, Green Bliss and Simply Casual
- Royal Oak. “Variety of restaurants, and the walk from parking to restaurant is interesting.”
- “I’m in my 30s and live an active/social life so Downtown Rochester, Birmingham, RO, Ferndale, Midtown, Corktown. They are all walkable or bikeable districts. They are relatively safe. And they have options with food and shops.”
- Ferndale, “because it’s close and I like the restaurants and stores.”
- Savon and Kuzos
- LeDoche Vita on Woodward/6 Mile
- Royal Oak: “There are multiple places to quickly eat, shop and be entertained, and the area has ample parking.”
- Cornerstone on Woodward In downtown
- Good cakes and bakes
- Downtown Royal Oak, “Diverse businesses and restaurants, very walkable and safe.”
- Bucharest Grill. “Great food and reasonably priced.”
- “I dine at Z’s Villa frequently. I shop at Great Lakes Crossing and online.”
- “Traffic Jam, Mario’s, and Romas as they have good food and great atmosphere.”
- Eastern market

## > COMMERCIAL DEVELOPMENT QUESTIONS

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- “I have several in Ferndale, mid-town, Royal Oak, Birmingham, and of course on Livernois Avenue.”
- “Midtown is a nice combination of everything you need. Downtown Ferndale is a nice runner-up because it has everything you’d want and easy parking.”
- “Western Market and Local Kitchen, both are in Ferndale. Fresh produce is great at Western, and Local is a casual place with great service.”
- “I like going to areas where multiple needs are addressed in one succinct area. e.g. At the corner of Orchard Lake & 14 Mile, I can go to the drug store, get groceries, have a nice meal, and shop for clothes, shoes, makeup and accessories for my entire family.”
- Ferndale
- Downtown. New growth, restaurants, businesses.
- Ferndale. “It’s easy-going and there are good options.”
- Tennis and Golf store, JAlexanders, Mitchells Seafood, Blackfinn, etc.
- “I like to shop at Kohl’s, Carson’s, and Western Market.”
- “Restaurants among them: Kuzzo’s and Noni’s; For shopping, I like second-hand shops. For dining, I like Craftwork and 1917 Bistro.”
- Downtown Royal Oak. “There are many business (restaurants, bars, and shops) all in walking distance of each other where we can park and walk where we need to. The selection of places to eat, shop, and just hang out is good as well.”
- Bakers, “close to home.”
- “Places where parking is free ,or convient”
- Red Lobster, “because I like sea food.”
- Sinbads on the river. “Variety of shopping and dining establishment where a sense of community is achieved.”
- Olive Garden
- Punch Bowl Social. “excellent for people watching.”
- “Downtown Detroit , and Troy because of the variety and the diversity of people.”
- Midtown--”walkable, variety of shops close together, history and character.
- NONIES GRILL; 1940 RESTAURANT; MY FAVORITE - KUZZO’S - GOOD FOOD.”
- Somerset Mall for the “variety and high end merchandise.”
- Kuzzo-friendly, good food.
- Trinosophes: “great food, coffee, music, art, and a record store
- Sherwood Coney.”
- 1917-”the owner, food, fun-movies at night+chess for kids. Style Gallery-the owner (now relocated but not sure why). Detroit Fiber Works-items, variety, art classes, etc.”
- Panera Bread, Detroit Seafood Market
- “I love the Hudson cafe great food. My son loves the Nike store.”



## > COMMERCIAL DEVELOPMENT QUESTIONS

### 19. What is your favorite place to shop and/or dine in metro Detroit and why?

- Bakers, The Bistro, Art Galleries on Livernois. "Close to home and supporting same business."
- Imperial. "I love the tacos and consistency."
- 1917 Bistro, "very friendly atmosphere."
- Texas de Brazil; "great food."
- Ice cream, dance studio.
- 1917 Bistro; "the ambiance and food."
- Somerset-"Clean & Beautiful."
- Kuzzo's
- TJ Maxx
- TJ Maxx, Marshalls, DSW Shoes, Fridays dinning, footlocker, fiber works; "unique and nice atmosphere."
- Mr. & Mrs. Bensons
- 1917 Bistro- "the menu has variety."
- Narien
- Ave of Fashion
- Andiamos (downtown); "I love the scenery and atmosphere."
- 1917 Am Bistro. Eric's I've Been Framed Shop.
- Cafe Vid in Birmingham. "I love the atmosphere and you can dine outdoor year round."
- 1917 Bistro



**SPACKMAN  
MOSSOP <sup>AND</sup>  
MICHAELS**