An aerial perspective rendering of a proposed neighborhood development. The scene is dominated by lush green trees and vegetation. In the upper left, a row of small, light blue, modern houses is visible. A winding path or road cuts through the center of the area. In the lower foreground, several large, light blue, rectangular building footprints are shown, some with small human figures for scale. To the right, a series of white, rectangular structures are arranged in a row. The overall impression is one of a planned, green community.

JULY 2024

GREATER WARREN/ CONNER NEIGHBORHOOD FRAMEWORK PLAN

**CITY OF DETROIT
PLANNING AND DEVELOPMENT DEPARTMENT
ANTOINE BRYANT, DIRECTOR**

ACKNOWLEDGMENTS

THIS PROJECT WOULD NOT BE POSSIBLE WITHOUT THE INVALUABLE CONTRIBUTIONS FROM RESIDENTS AND NEIGHBORHOOD GROUPS THROUGHOUT THE PLANNING PROCESS.

City of Detroit

- Planning & Development Department (PDD)
- Housing & Redevelopment Department (HRD)
- Department of Neighborhoods (DON)
- General Services Department (GSD)
- Detroit Land Bank Authority (DLBA)
- Department of Public Works (DPW)
- Detroit Department of Transportation (DDOT)
- Detroit Water & Sewerage Department (DWSD)
- Detroit Economic Growth Corporation (DEGC)
- Detroit City Council District 4, Office of Council Member Latisha Johnson

Consulting Partners

- OHM Advisors
- Woods & Watts Effect
- livingLAB
- Spalding DeDecker
- Jumi Consulting Group

Community Partners

- 5th Precinct Police - Community Relations Council
- Brilliant Detroit
- Canfield Consortium
- Chandler Park Conservancy
- Chandler Park Neighborhood Association
- Detroit Greenways Coalition
- District 4 Community Advisory Council
- Eastside Community Network
- FCA Neighborhood Advisory Council (NAC)
- Friends of Parkside
- Good Stock Neighborhoods
- LEAP Neighborhood Improvement Committee
- Lemay Block Club / Living the Good Life Block Club
- Little Detroit Community Garden
- MACC Development
- Sanctuary Farms
- Sidewalk Detroit

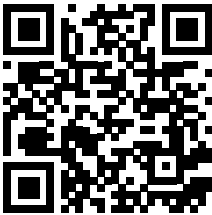


Back Cover: East Canfield Art Park south of Brewer Park, owned and managed by the Canfield Consortium. Photo source: OHM Advisors

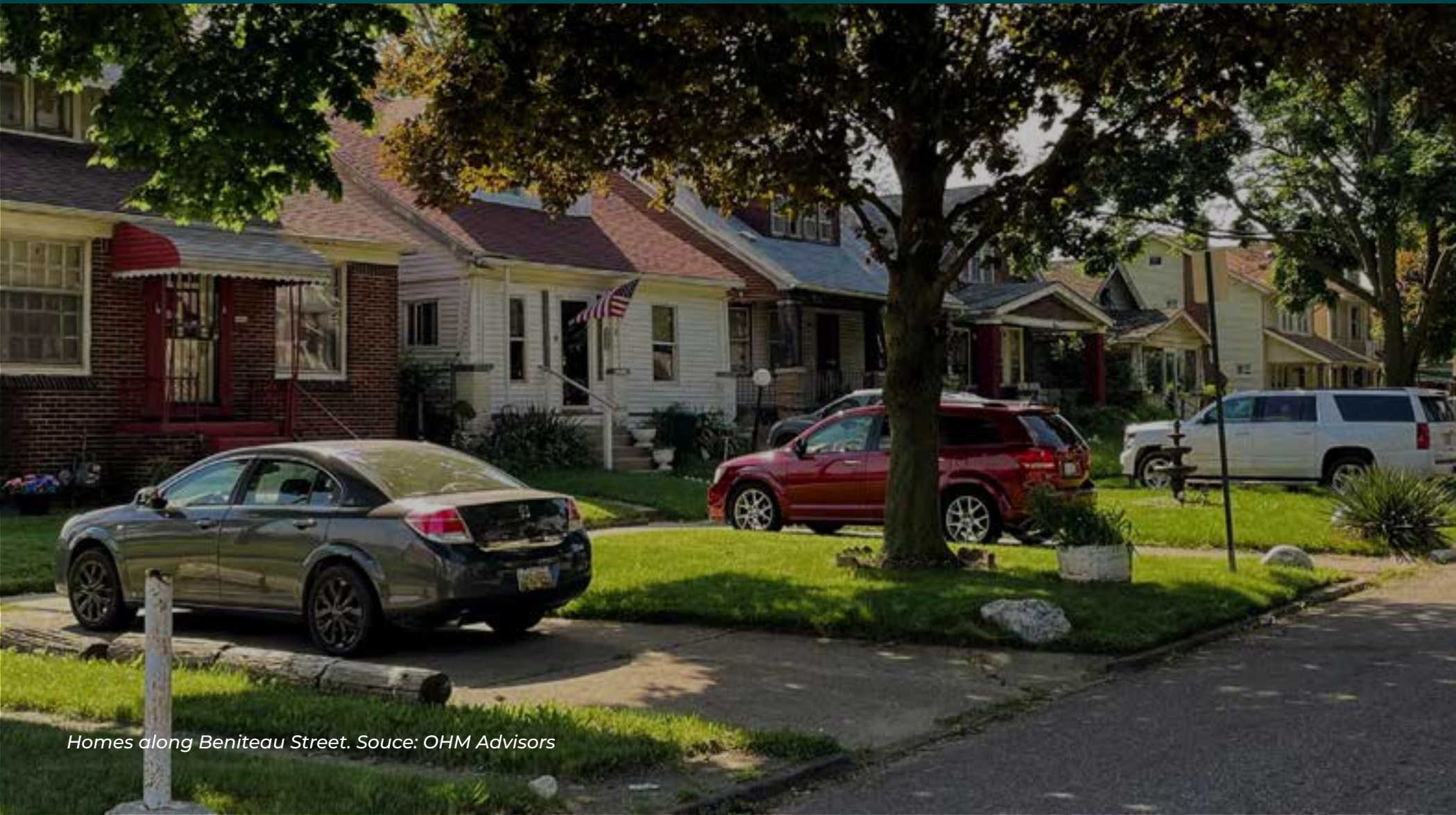
CONTENTS

5	OVERVIEW
8	NEIGHBORHOOD STABILIZATION STRATEGIES
14	COMMERCIAL & ECONOMIC DEVELOPMENT STRATEGIES
16	MOBILITY & STREETS STRATEGIES
18	PARKS & OPEN SPACE STRATEGIES
20	CLIMATE RESILIENCE & HEALTH EQUITY STRATEGIES

For more information, please visit the [project website](#), scan the QR code, or contact the project team at GreaterWarrenConner@DetroitMI.gov.



01 EXECUTIVE SUMMARY



Homes along Beniteau Street. Source: OHM Advisors

The Greater Warren/Conner neighborhoods are shaped as much by their past as their aspirations. Residents have long been fighting against environmental injustice as a result of the heavy industry and highways that have fragmented the community for decades.

The Greater Warren/Conner Neighborhood Framework Plan strives to address the needs and concerns of existing residents while serving as a guide to achieving their desired quality of life through community change. Developed by the City of Detroit in tandem with residents, this Plan serves as the community's road map toward a more resilient and equitable future.

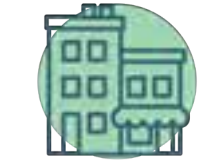
What is a Neighborhood Framework Plan?

The Neighborhood Framework Plan is a collaborative effort between the City of Detroit and local residents, to ensure that the area thrives and the community advances alongside the rest of the city. The key recommendations across five focus areas were developed to guide community advancement. The specific recommendations in each focus area are summarized on the following pages and shown in the accompanying map.

Focus Areas



NEIGHBORHOOD STABILIZATION



COMMERCIAL & ECONOMIC DEVELOPMENT



MOBILITY & STREETS



PARKS & OPEN SPACE



CLIMATE RESILIENCE & HEALTH EQUITY

Building Activity through Focused Investments

The recommendations for the Greater Warren/Conner Framework Plan revolve around improving both the existing amenities and infrastructure of the neighborhoods, as well as identifying strategies to enhance the local economy, address ongoing environmental issues, and improve the overall quality of life for residents.

Given the limited amount of secured funding to implement the Plan’s strategies, it is recommended that future investments are located near each other within the neighborhood. This ensures that the strategies support one another and maximize the positive impact to the area.

Future Conditions



NEIGHBORHOOD STABILIZATION

- » Infill Housing
- » Vacant Land Activation
- » Redevelopment of Parkside Homes



COMMERCIAL & ECONOMIC DEVELOPMENT

- » Commercial Infill
- » Activation of Commercial Nodes



MOBILITY & STREETS

- » Street Safety Interventions
- » Bus Stop Improvements



PARKS & OPEN SPACE

- » Engagement and Redevelopment of Brewer Park
- » Chandler Park Dome
- » Connections to Chandler Park

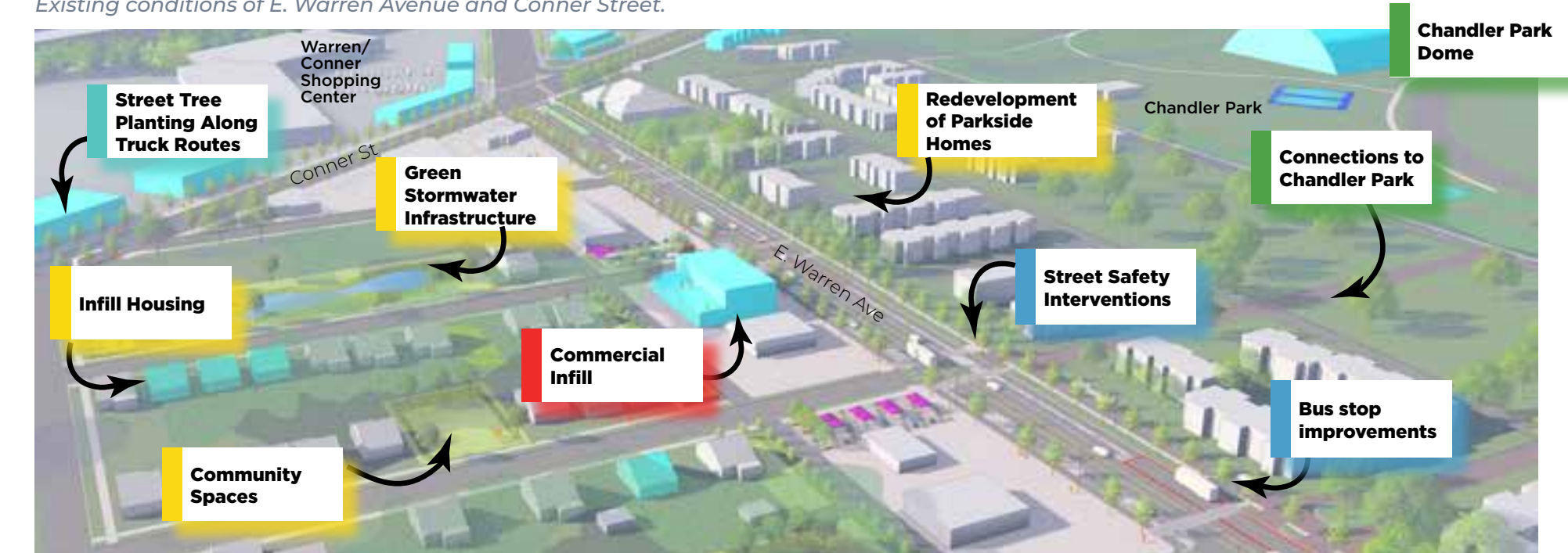


CLIMATE RESILIENCE & HEALTH EQUITY

- » Green Stormwater Infrastructure
- » Street Tree Planting Along Truck Routes



Existing conditions of E. Warren Avenue and Conner Street.



Proposed improvements at E. Warren Avenue and Conner Street.



Neighborhood Stabilization Framework Strategies



Source: The Detroit News



Source: BridgeDetroit



Source: Model D Media

“Neighborhood Stabilization” is the process of improving the residential areas in the community through three strategies: **sustain**, **stabilize**, and **stimulate**. Each of these strategies is intended to be deployed in specific areas of the neighborhood, depending on the quality and availability of the existing housing stock, and also combined with other investments in mobility, sustainability, parks, vacant land activation, and commercial development.

The Plan’s key strategies to support Neighborhood Stabilization efforts include the following:

Focus stabilization efforts on **rehabilitating homes** within established housing markets, near neighborhood anchors and along primary commercial corridors.

Focus **commercial and retail development efforts** near these areas to ensure access to everyday goods and services.

Implement Green Stormwater Infrastructure (GSI) initiatives on vacant parcels to alleviate and reduce flooding impact.

Support for greenway investments like the Canfield Connect, Iron Belle Trail, and Conner Creek Greenway routes in Chandler Park, East Canfield, Fox Creek, and West End will elevate desirability and value of existing properties around them.

Leverage partnerships and expertise with organizations to provide additional funding and support for home repair and renovations.

Establish a community land trust to manage clusters of vacant land in proposed Resilience Zone areas to create new affordable home-ownership opportunities, preserve open space, support urban agriculture, and other community-driven projects

Integrate community-preferred land activation strategies to **create Resilience Zones** in suitable areas with large clusters of vacant land in Fox Creek, Riverbend, and East Canfield Villages.

Infill development should be focused first on the **Targeted Multi-family Housing Area** at Mack Avenue and Alter Road to better leverage current incentive programs.

Home Rehab and Stabilization Zones





SUSTAIN and STABILIZE: Residential Stabilization Zones

There are a variety of tools that the City of Detroit and its partners can use to support neighborhood stabilization efforts within Greater Warren/Conner. The proposed Rehabbed & Ready Program aims to **sustain** the Chandler Park neighborhood’s housing market by rehabilitating vacant homes to move-in ready condition, attracting homebuyers and increasing property values. In the East Canfield Village and West End neighborhoods, the **stabilization** strategy should be employed to preserve viable homes for future renovation, alongside targeted demolitions, side lot activations, and property marketing. Stabilizing existing structures and clearing blight can improve the neighborhood’s conditions in the short term and set it up for future investment, with a focused approach on single blocks recommended to maximize investment value.



Before and after of a Rehabbed and Ready home in the Crary St. Mary’s neighborhood in Northwest Detroit. Source: Curbed Detroit



STIMULATE: Resilience Zones

New residential development is needed in the neighborhood to address the lack of livable housing and the high amount of vacant land. Opportunities to **stimulate** new residential development can happen through the redevelopment of Parkside Homes, within the Target Multi-family Housing Area (TMHA) near Mack Avenue and Alter Road, as well as through small-scale infill development within the neighborhoods. A strategy for infill development is proposed through the establishment of Resilience Zones that activate publicly owned vacant land with productive uses. These areas co-locate urban farming, green stormwater infrastructure, public space, trails, and residential development to catalyze targeted areas in the neighborhood, and could be located within the established TMHA for additional support.

PATHWAYS



Source: Orlando Urban Trail

COMMUNITY SPACES



Source: Project for Public Spaces

AGRICULTURE



Source: SW Gardens

LOW MOW



Source: Natural Community Services

ECO+ LANDSCAPES



Source: City of Detroit

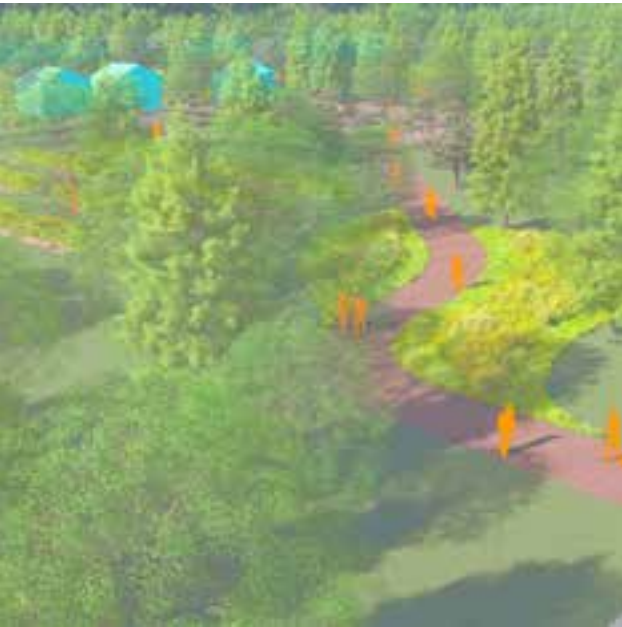
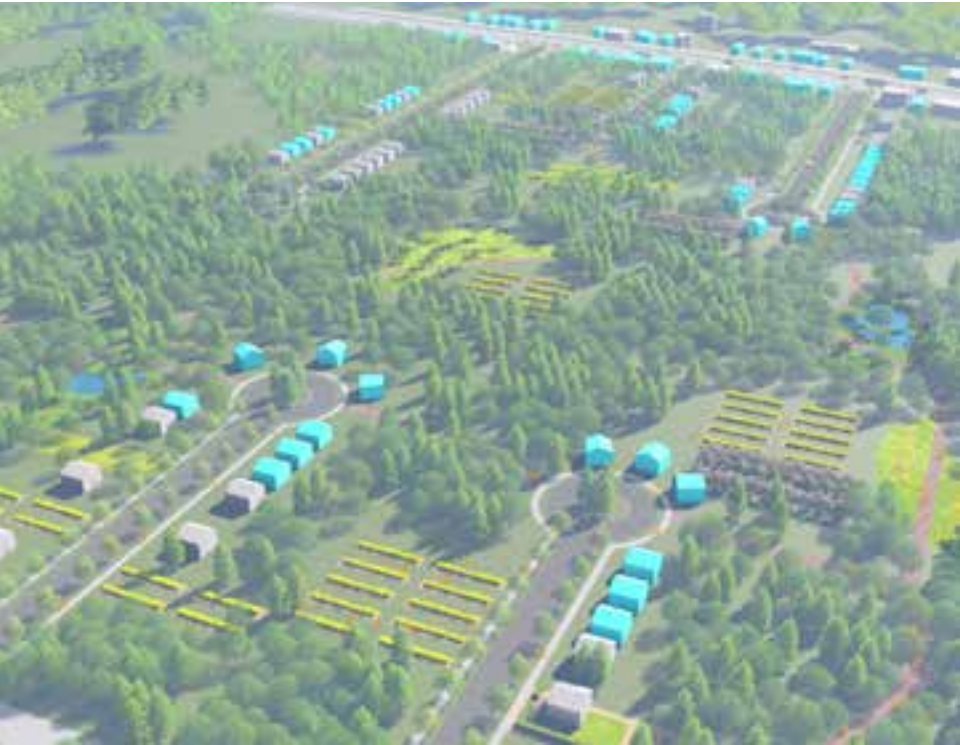
EDGE TREATMENTS

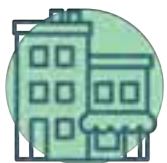


Source: American Meadows



The vision of Resilience Zones in Greater Warren/Conner.





Commercial and Economic Development Framework Strategies



Source: Green Hills, TN

The once bustling commercial corridors in the Greater Warren/Conner neighborhood have experienced shifts due to population decline, evolving retail trends, online shopping, and economic factors. In Detroit and elsewhere, retail landscapes have transformed significantly since these neighborhoods were established. Instead of replicating extensive retail fronts, the emphasis is now on revitalizing neighborhood-focused commercial nodes, integrating essential services into a vibrant, walkable, and interconnected environment.

The Plan's key strategies to support Community and Economic Development efforts include the following:

Invest in the identified commercial nodes to establish local retail and commercial areas with services and stores that enhance the quality of life for residents.

Implement activation efforts first in primary nodes with existing business activity, then underutilized property areas, with food truck vendors, local markets, event programming, infrastructure upgrades, and landscaping enhancements.

Prepare commercial nodes to be 'redevelopment ready' through building demolition and rehabilitation, blight reduction, landscaping, and infrastructure improvements.

Re-zone former school properties slated for demolition to facilitate residential and mixed-use development compatible with surrounding neighborhoods.



Source: Detroit Metro Times



Source: Model D Media

Primary Nodes

CONNER STREET & E. WARREN AVENUE



MACK AVENUE & ALTER ROAD



Secondary Nodes

E. WARREN AVENUE & CHALMERS STREET



SHOEMAKER STREET & FRENCH ROAD





Mobility and Streets Framework Strategies



Source: Denver Streets Partnership



Source: USDA Forest Service



Source: Cincinnati Design Awards

Streets and roads are critical pieces of infrastructure in any community. They not only provide opportunities for travel and connections, but are also vast public spaces that provide opportunities for local businesses, community interaction, and beautification. Improvements to the transportation network is focused on improving safety and connectivity for all users.

The Plan’s key strategies to support Mobility efforts include the following:

Support Safe Streets for All (SS4A) initiatives and pursue funding to fill gaps in improvements.

Support pedestrian safety, accessibility, and connectivity throughout the area.

Apply safety improvements to I-94’s bridges and service drive.

Plant green buffers of native vegetation for sound and pollution reduction on publicly-owned properties along the highway.

Reduce highway through-traffic on residential streets.

Implement a **new Chandler Park Drive design** to create a street that acts as both a connection through the park and as part of the park itself.

Explore the potential for a transit hub at the intersection of Conner Street and E. Warren Avenue to enhance transit use.

Coordinate bus stops improvements to include benches, bus stop signs, and route information.

Reduce the impacts of truck routes through the community with industrial buffering standards along roadways.

Proposed Improvements

CROSSING SIGNALS



Source: FHWA

CROSSWALKS



Source: NYC Street Design Manual

CURB BUMP OUTS



Source: NACTO

PROTECTED BIKE LANES



Source: Bicycling Magazine

PAINTED BIKE LANES



Source: OHM Advisors.

PAINTED CONCRETE



Source: City of Detroit



Parks and Open Space Framework Strategies



Source: OHM Advisors



Source: Minneapolis Parks and Recreation Board



Source: WFMU 89.1

Safe, active, and beautiful parks are a key feature of every great neighborhood. The Greater Warren/Conner neighborhood is fortunate to have Chandler Park nearby, one of the City's destination parks. However, the other parks and recreational assets in the neighborhood also deserve investment, including Brewer Park, the Conner Creek Greenway, and the Canfield Connect. The Parks and Open Space strategies will extend beyond the existence and maintenance of parks, to integrate gathering spaces into the everyday, nearby experience for residents in Greater Warren/Conner.

The Plan's key strategies to support Parks and Open Space efforts include the following:

Preserve park land for recreational purposes and open space through zoning changes or grants.

Implement the short-term, passive **improvements to Brewer Park** to activate the park and preserve space for future uses.

Implement the long-term **amenity and infrastructure improvements to Brewer Park** as identified in a future park master plan.

Enhance neighborhood connectivity and accessibility through a trail system, including the community-proposed Canfield Connect trail.

Ensure safe passage across Warren Avenue and improve connectivity to the neighborhoods north of Brewer Park.

Strategically enhance Iron Belle Trail visibility on Warren Avenue and **Conner Creek Greenway** on Conner Street through safety features, signage, and recreation amenities.

Brewer Park Concept Plan

Brewer Park is targeted for improvements from the General Services Department (GSD) in the future. Prior to that larger investment, initial improvements are recommended to activate the park and surrounding neighborhood.

More details on the Brewer Park concept plan are in the Strategies section.





Climate Resilience and Health Equity Framework Strategies



Source: SSDN



Source: The Detroit News

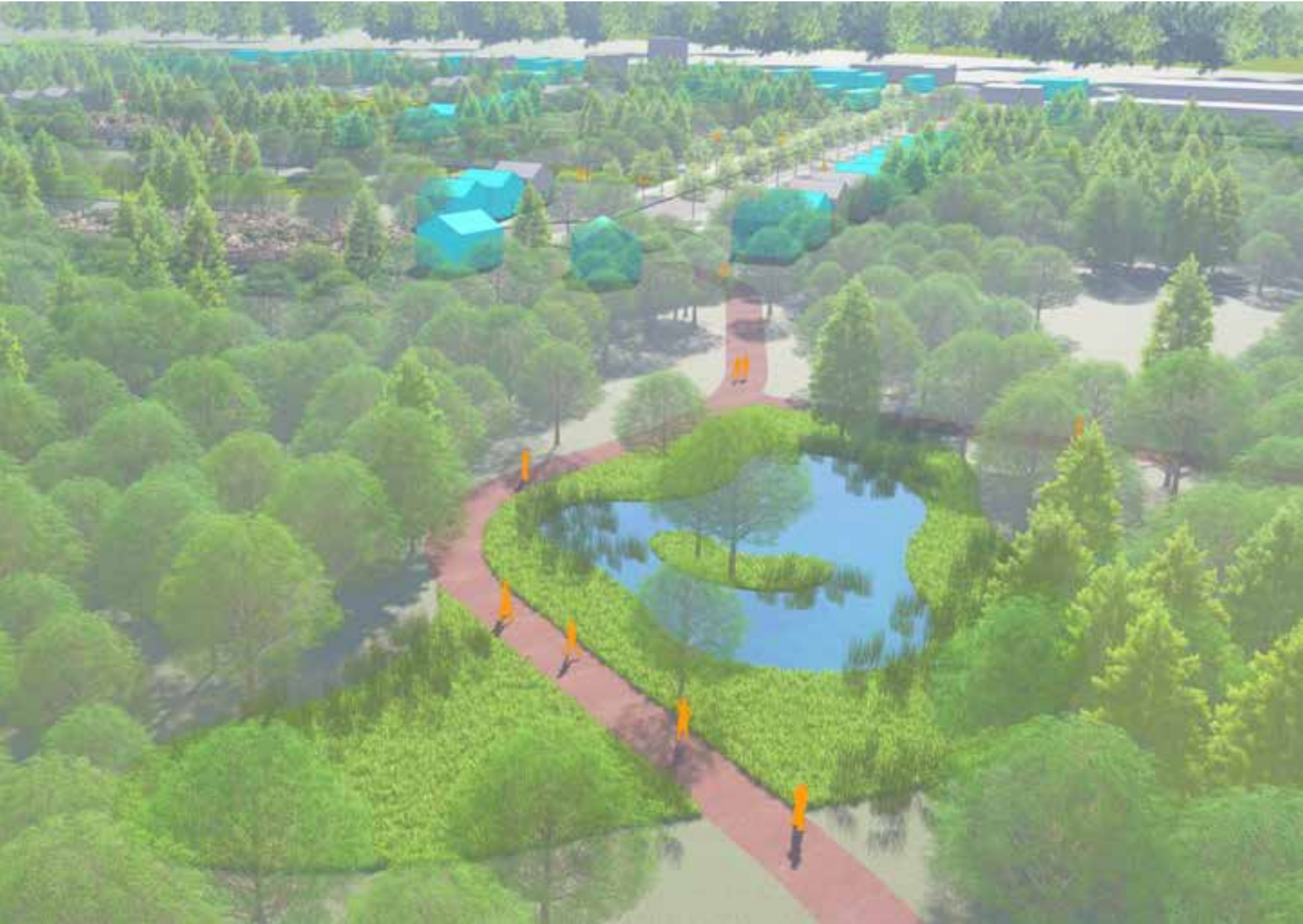


Source: Planet Detroit

The Greater Warren/Conner neighborhood has long been negatively affected by flooding, air pollution, and urban heat, and is more vulnerable to the affects of climate change. The Plan’s recommendations support the need for additional community safeguards from air pollution, environmental disasters, and the cooperative use of vacant land. These recommendations are intended to work in conjunction with the recommendations for the previous four focus areas.

The Plan’s key strategies to support Climate Resilience and Health Equity efforts include the following:

- Invest in existing community buildings** to provide resilience hub services.
- Support and **expand urban farming practices**.
- Dedicate City staff and funding to support **flood mitigation initiatives**, including the dedication of runoff storage space and green stormwater infrastructure.
- Pursue federal, state, and local grants** to monitor and reduce indoor and outdoor air pollution.
- Grow and maintain the tree canopy** along streets and on publicly owned property, using species suitable for filtering particulate matter and reducing air pollution.





This Page Intentionally Left Blank

INTRODUCTION

Project Purpose

The Greater Warren/Conner Neighborhood Framework Plan lays out a 10-year framework for improvement projects in the neighborhoods recognized as East Canfield Village, West End, Fox Creek, Chandler Park, Chandler-Park Chalmers, and Riverbend. The area is bound to the north by I-94, to the east by Alter Road, to the south by E. Vernor Highway and Mack Avenue, and to the west by McClellan Avenue.

Some recommendations may have an immediate impact on the surrounding area, while others may take longer to see a tangible outcome. The plan responds to both citywide priorities – **flooding, affordability, safe streets, connectivity, resilience, and equity** – as well as the unique challenges residents face in each neighborhood.



Greater Warren/Conner Study Area Map.

Process and Schedule

Developed under the guidance of the City of Detroit Planning and Development Department (PDD), the plan identifies five key Focus Areas crucial to improving the quality of life, resident health, and overall vibrancy of the neighborhood. These Focus Areas represent the avenues where the City of Detroit, as well as State, community, philanthropic, and private partners, can invest to make actionable change in the neighborhood.

1. **Neighborhood Stabilization**
2. **Commercial and Economic Development**
3. **Mobility and Streets**
4. **Parks and Open Space**
5. **Climate Resilience and Health Equity**

Specific recommendations were developed for each Focus Area, building on resident feedback and coordinating strategies across Focus Areas and City Departments to stretch public investment as far as possible. Additional recommendations were developed to utilize the \$800,000 allocated to the Greater Warren/Conner community through the City's Community Benefits Ordinance (CBO) process.

The planning process consisted of four key phases:



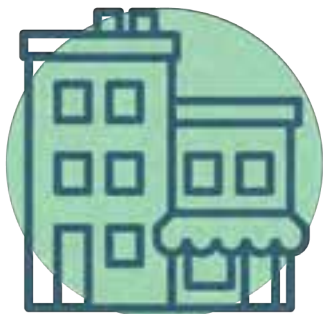
Focus Areas

The Greater Warren/Conner Neighborhood Framework Plan addresses the existing conditions, community priorities, and recommendations for the following Focus Areas:



NEIGHBORHOOD STABILIZATION

Neighborhood Stabilization is a comprehensive strategy focused on the improvement of residential areas within the community, informed by current housing conditions. Building off of neighborhood anchors and other activity in the area, targeted areas for improvement are identified. To support a more inclusive future, strategies include the development of new and diverse housing stock and addressing housing affordability.



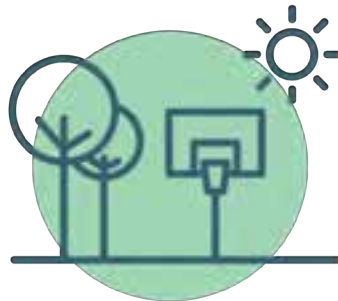
COMMERCIAL AND ECONOMIC DEVELOPMENT

Commercial and Economic Development is a strategy to support the available employment opportunities and characteristics in the neighborhood while attracting new shops and businesses to primary and secondary commercial nodes throughout the community. This Focus Area prioritizes businesses and services that meet community needs, with consideration for preserving and enhancing salvageable buildings in Greater Warren/Conner.



MOBILITY AND STREETS

Mobility and Streets is a strategy that incorporates the City’s comprehensive Safe Streets for All (SS4A) initiative to enhance roadway safety throughout the neighborhood and to better serve all users within the community. This Focus Area not only outlines projects already planned by the City and/or MDOT, but also identifies opportunities for essential connections for residents, improved transit stops, addresses pedestrian and bicyclist safety, and strategic truck traffic patterns. The Mobility and Streets Focus Area is closely tied to Commercial and Economic Development and Climate Resilience and Health Equity.



PARKS AND OPEN SPACE

Parks and Open Space is a strategy that outlines the community’s vision to enhance local parks with programming and physical improvements like walking paths, picnic amenities, playscapes suitable for a range of age groups, and basketball courts. Chandler Park, a major regional park in Greater Warren/Conner, has already benefited from significant investment over the past decade. The Focus Area emphasizes equitable investment in parks that have yet to receive improvements, and aligns the improvement with those identified in the Parks and Recreation Strategic Plan (PRSP). This Focus Area also explores opportunities to create safe routes and community access points to parks.



CLIMATE RESILIENCE AND HEALTH EQUITY

The Climate Resilience and Health Equity strategy looks to help address residents’ heightened exposure to climate change and health risks. This Plan is committed to providing strategies that enhance the community’s resilience to climate challenges, concurrently enhancing the overall environmental and community health within the neighborhood. A network of resilience hubs is proposed to provide essential services during climate-related events, such as extreme heat and cold, flooding, and power outages. The adoption of productive land uses, the mitigation of industrial impacts, and the establishment of a resilience hub network are core components of this approach. In combination, the strategies will improve the ecological and social performance of the resilience network and play a crucial role in diminishing pollution levels.

Community Benefits Agreement (CBA) Funding Implementation

In 2019, Stellantis (formerly Fiat Chrysler Automobiles or FCA) partnered with the City of Detroit to expand the Jefferson North Assembly Plant at Mack Avenue and Conner Road. Stellantis invested \$1.6 billion to convert the existing Mack Avenue Engine Complex into a production plant for the Jeep Grand Cherokee and an additional \$900 million to modernize the Jefferson North Plant. The tax benefit provided by the City required Stellantis to participate in the City's Community Benefits Ordinance (CBO) process and provide benefits to the surrounding neighborhoods.

A \$800,000 community benefits fund was established through the Community Benefits Agreement (CBA) between Stellantis, the Neighborhood Advisory Council, and the City of Detroit. This fund is intended to be used for neighborhood improvements and for the enhancement of the quality of life of those who may have been impacted by the facility's expansion. A major part of the Greater Warren/Conner Neighborhood Framework Plan revolved around identifying the areas where residents would like to see the money spent.

Options for a variety of investments to the neighborhood were presented to residents, stakeholders, local leaders, and business owners throughout the planning process to gather feedback about what would be most impactful to the neighborhoods.

Throughout the public engagement process, residents consistently voiced strong desire for more home repair grants to address the maintenance needs of many of the homes in the neighborhood. Over the first three public meetings, the community survey, and the community ambassador program, most votes and responses pointed to the need for home repairs in the community. Residents were also interested in setting aside a small portion of the overall funding to support community programs through grants and establishing a fund to help small businesses in the neighborhood.

The final consensus from residents in the Greater Warren/Conner planning area was to dedicate \$600,000 of the funding to supporting home repairs with the remaining \$200,000 to support community improvements that align with feedback received from residents during the neighborhood planning process.

Summary of Engagement Results

PROJECT CATEGORIES	TOTAL VOTES	
Home Repair Grants	143	17%
Small Business Support	97	11%
Corridor/Streetscape Improvements	96	11%
Community Grants	80	9%
Educational/Youth Programs	78	9%
Green Stormwater Infrastructure	66	8%
Traffic Calming	61	7%
Park Upgrades	58	7%
Indoor Air Quality Mitigation	55	6%
Tree Planting	39	5%
Mobility Enhancements	35	4%
Murals & Public Art	23	3%
Retail Space	15	2%
Resilience Hubs	9	1%

CBA Community Investment Options

HOME REPAIR GRANTS



YOUTH PROGRAMS



BLOCK CLUB SUPPORT



IDENTITY AND WAYFINDING



COMMUNITY MINI GRANTS



MICRO RETAIL SPACE



INDOOR AIR QUALITY



SMALL BUSINESS SUPPORT



PARK ACTIVATION



STREETSCAPE IMPROVEMENTS



EXPANSION OF REHABBED & READY



PUBLIC ART



GREENWAY DEVELOPMENT



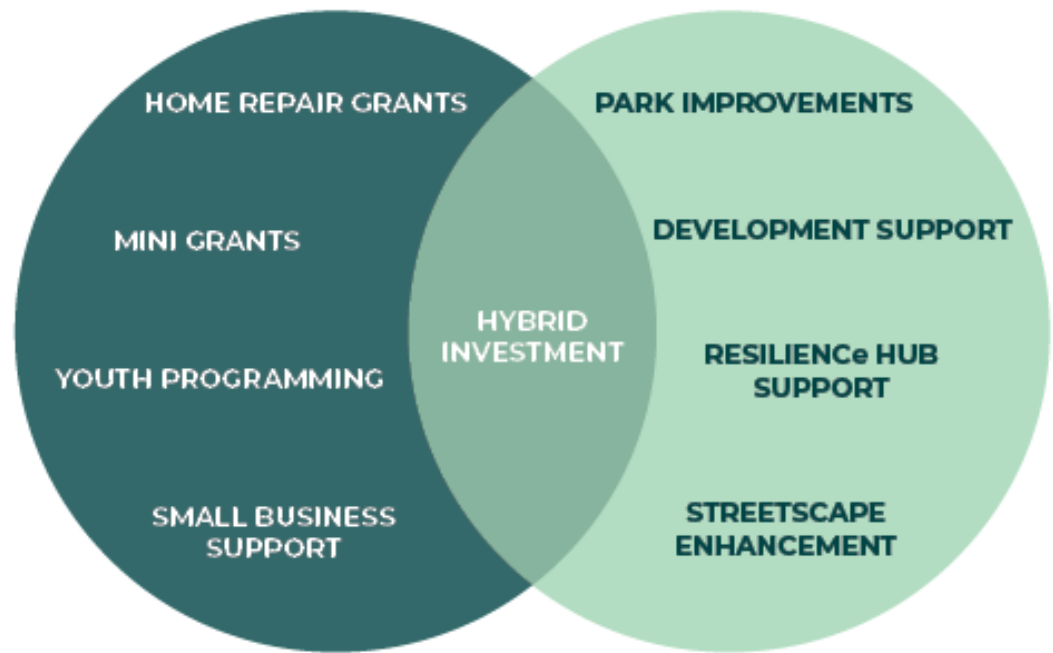
DEVELOPMENT FINANCING SUPPORT



COMMUNITY BENEFITS IMPACT FUND PROJECTS

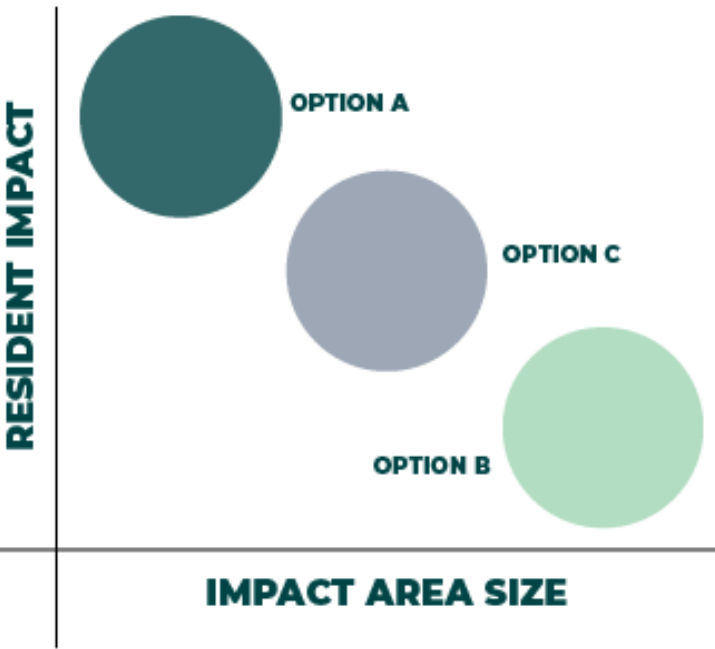
The Stellantis (FCA) Community Benefits Agreement specified **\$800,000** to be spent on neighborhood improvements. Based on feedback from the community, two options for distributing the funds have been identified.

RESIDENT FOCUSED IMPACT



Hybrid of Options A and B

NEIGHBORHOOD FOCUSED IMPACT



Graphic presented to Greater Warren/Conner residents explaining the differences between the potential CBA funded projects. Residents ultimately chose a hybrid option.



A Greater Warren/Conner resident votes on their preference for how to distribute the CBA funding. Source: City of Detroit

Community Engagement Process

Through the community engagement process, residents and stakeholders were encouraged to think about everything that impacts their daily well-being and future. Through collaboration, community-preferred recommendations could be identified and ranked by priority. A more detailed summary of each community engagement touchpoint can be found in **Appendix B**.



Attendees at a community meeting. Image Source: City of Detroit

Key Themes Heard During Engagement

- Improve basic city services
- Need for home repair grants
- Remove blight (homes and vacant land)
- Infrastructure needs updating
- Few amenities in the neighborhood
- Air quality and truck traffic are major concerns
- Strategies to utilize vacant land for productive use
- Opportunities to reduce flooding

4 PUBLIC MEETINGS

194 IN-PERSON MEETING ATTENDEES

110 VIRTUAL MEETING ATTENDEES



102 SURVEY RESPONSES

13,700 POSTCARDS MAILED

345 E-NEWSLETTER SUBSCRIBERS

8 COMMUNITY AMBASSADORS



Attendees at the community meetings. Sources: City of Detroit

Previous Plans: Building On What Came Before

Over the past 15 years, numerous planning efforts have aimed to shape a unified vision for Detroit’s east side, including Greater Warren/Conner. This Plan builds upon those efforts, highlighting the community’s ability to enact positive change. **Appendix A** provides further information on these planning efforts.

Key Themes

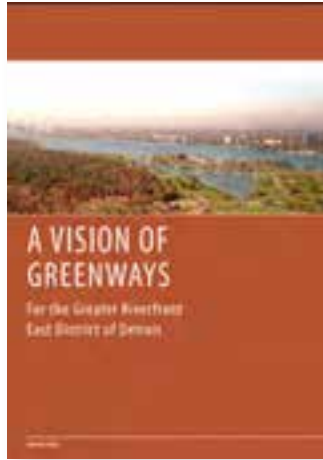
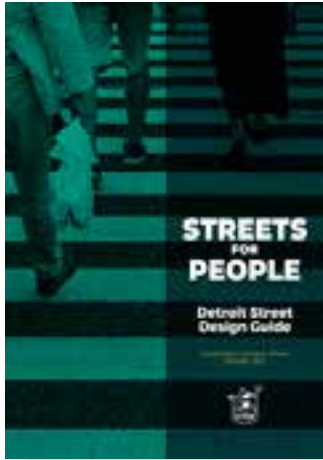
- Vacant land reclamation and redevelopment
- Affordable infill housing
- Greenway development
- Mack Avenue streetscape improvements
- I-94 bridge improvements
- Safe Street for All (SS4A) interventions
- Investment into Chandler Park
- Investment of the Community Benefits Agreement (CBA) funding



Source: East side Community Network



Source: JIMA Studio



The 200-acre Chandler Park Marshland. Source: OHM Advisors

03

NEIGHBORHOOD CONDITIONS

History

Up until the early 20th century, Greater Warren/Conner existed on the outskirts of Detroit, with the area east of Conner Street under the jurisdiction of Grosse Pointe. [1] The landscape underwent significant transformation with the rapid urbanization of Detroit and the earlier seeds of industrialization along Conner Creek, leading to a surge in industrial, residential, and commercial development.

In 1917, Detroit fully annexed the area. Between 1912 and 1914, the Detroit Terminal Railroad was constructed around central Detroit which connected various industrial facilities on the ring around Detroit. During this period, the area gained distinction with the establishment of Chrysler Automotive plants along the Detroit River and Conner Creek. Conner Street intersects the study area, named after a submerged natural waterway which held historical significance for both Indigenous people and European settlers.

The natural environment and demographics of the neighborhood have also undergone substantial changes since the early 1900s, influenced by national trends such as a growing senior population, declining household sizes, and increasing educational attainment. While numbers can never define the lived experiences of the population of East Canfield Village, West End, Chandler Park-Chalmers, Chandler Park, Fox Creek, and Riverbend, they can provide a data-driven narrative as another piece of information to help guide the Plan and its recommendations.



Homes within the Greater Warren/Conner community.
Source: OHM Advisors

Demographic Profile: Key Takeaways

Population Trends. Greater Warren/Conner witnessed a steady and significant 35% decrease in population between 2010 to 2020, compared to a 5.5% decrease city-wide during the same period. Projected data indicates a continued decrease of 0.5% from 2020 to 2027, presenting an opportunity for potential stabilization in the coming five years. [2]

Families and Households. The neighborhood witnessed a 28% decrease in total households from 2010 to 2020, aligning with the population decline. Most households are relatively small, with 1, 2, and 3-person households being the most prevalent. This presents an opportunity to introduce varied housing typologies to the neighborhoods. [3]

Workforce. Between 2021 and 2022, the employed population aged 16 and over experienced a drastic shift, rising from 26% in 2021 to 84%, indicating a substantial 58% increase. This is likely attributed to the improved post-COVID economic conditions in the City of Detroit and within Southeast Michigan, as well as from the employment expansion at Stellantis auto manufacturing plants. Unemployment in the City of Detroit dropped from 39.4% in May 2020 to 8.1% in May 2022, following a similar trend as the neighborhood conditions. As of 2024, 316 residents within the neighborhood work at the Mack Avenue Assembly plant and an additional 703 residents are employed at other Stellantis Plants around the region. [4]

Socioeconomic Status. Socioeconomic Status (SES) is a measure of income, educational attainment, and occupation, and is related to quality-of-life indicators. Nearly half (46%) of individuals in Greater Warren/Conner live below the poverty line, a rate significantly higher than the citywide average in Detroit, where 34% of residents are below the poverty line. The median household income in Greater Warren/Conner (\$24,400) is lower than the median income in the City of Detroit (\$34,700) and significantly lower than in Wayne County (\$52,800). These incomes directly correlate to educational attainment data. Educational attainment of a high school diploma or higher is 10% lower than the rest of the City of Detroit. [5]








Corrigan Playground. Source: OHM Advisors







Sidewalk Festival in Brewer Park. Source: Sidewalk Detroit

Demographic Profile



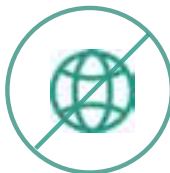

					
	POPULATION	BLACK OR AFRICAN AMERICAN POPULATION	MEDIAN AGE	UNDER 18	OVER 65
GREATER WARREN/ CONNER	10,664	92%	37	3,022	2,225
CITY OF DETROIT	636,787	78%	35	156,754	91,780

Source: 2022 ACS 5-Year Estimates, Tables B01003, DP05.

				
	TOTAL NUMBER OF HOUSEHOLDS	AVERAGE HOUSEHOLD SIZE	MEDIAN HOME VALUE	MEDIAN HOUSEHOLD INCOME
GREATER WARREN/ CONNER	4,430	2.4	\$42,789	\$24,392
CITY OF DETROIT	249,518	2.5	\$63,865	\$66,700

Source: 2022 ACS 5-Year Estimates, Tables S1101, DP04, S1901.

SOCIAL VULNERABILITY INDEX (SVI)

				
	EDUCATION (HIGH SCHOOL DIPLOMA OR HIGHER)	HOUSEHOLDS WITHOUT ACCESS TO A VEHICLE	HOUSEHOLDS WITHOUT INTERNET	HOUSEHOLDS LIVING AT OR BELOW THE POVERTY LINE
GREATER WARREN/ CONNER	70%	38%	30%	46%
CITY OF DETROIT	81%	22%	20%	34%

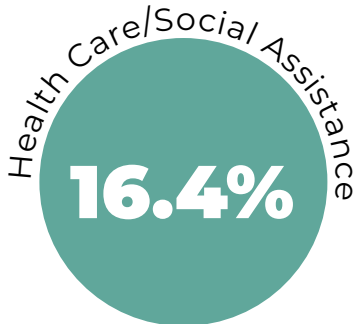
Source: 2022 ACS 5-Year Estimates, Tables S1501, S2504, S2801, S1901.

GREATER WARREN/CONNER EMPLOYMENT

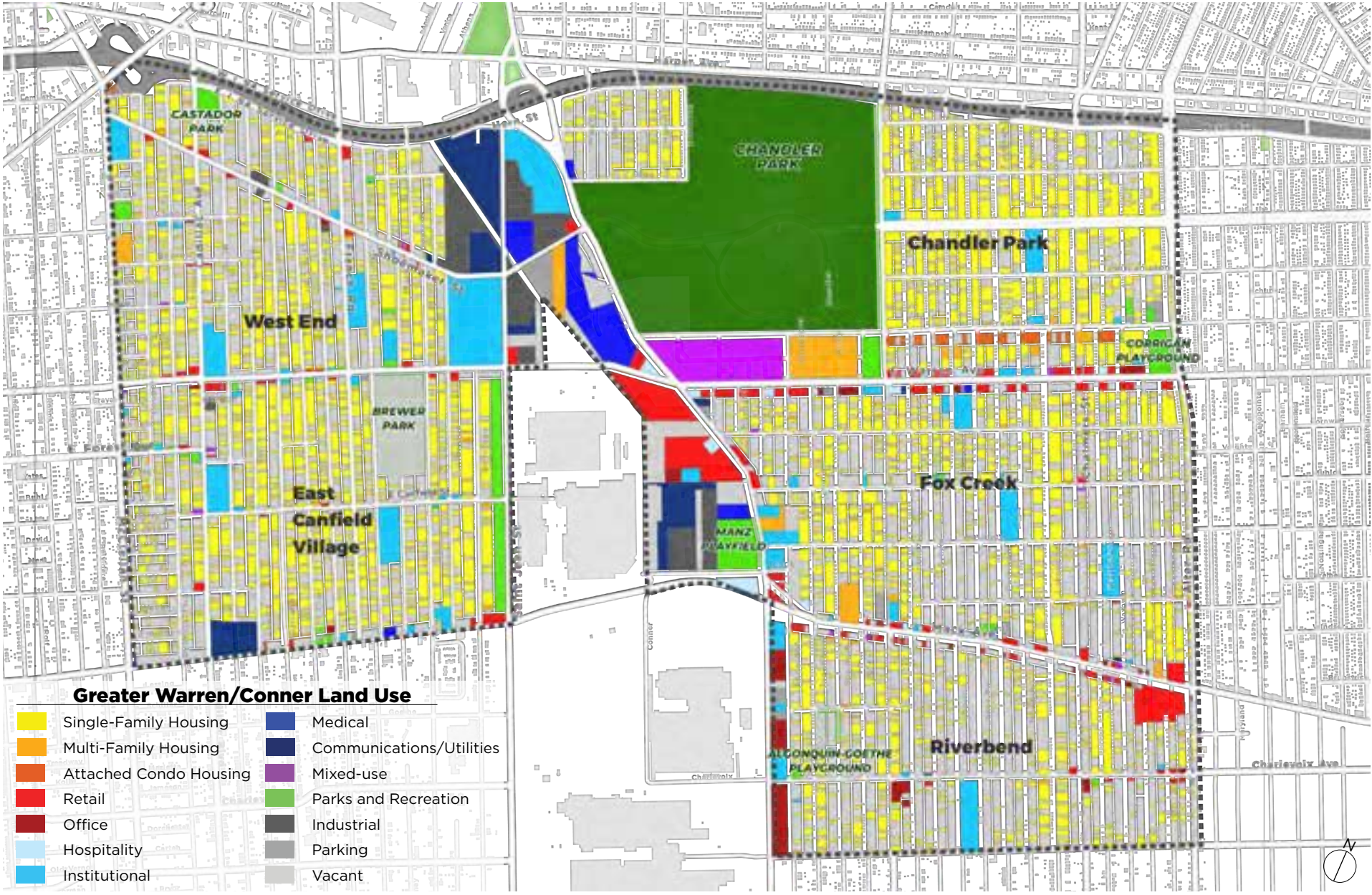
84%

Greater Warren/Conner workers are essential workers.

EMPLOYMENT RATE



Source: 2022 ACS 5-Year Estimates, Table DP03. ESRI Business Analyst, Business Summary.



Data Source: City of Detroit

Land Use

In Greater Warren/Conner, there are many single-family homes surrounding the large industrial area of the Stellantis Assembly Plants. The table below shows the breakdown of land uses in the neighborhood by percentage of the total.

While the Stellantis Plants are located outside of the planning study boundary, it does account for a large amount of land within the center of the neighborhood. In total, Stellantis occupies roughly 129 acres within the neighborhood while the neighborhood itself encompasses about 2,440 acres. Stellantis accounts for over 5.25% of the neighborhood (not including the plants that extend south of Mack Avenue).

Additionally, the neighborhood has been negatively impacted by population and business loss over the years, resulting in a significant amount of vacant land and structures in the neighborhood, most of which are owned by the Detroit Land Bank Authority (DLBA). Vacancies are spread throughout each of the neighborhoods, but much more concentrated in the Riverbend and Fox Creek neighborhoods.

CATEGORY	PERCENTAGE
Vacant	38.6%
Single-family housing	27.6%
Parks and Recreation	14.9%
Retail and Mixed-use	6.6%
Institutional/Public Uses	5.2%
Industrial	2.3%
Multi-family housing	2.1%
Parking	0.6%



The Stellantis Plants are centrally located in the area.
Source: OHM Advisors



Vacant land in Riverbend. Source: OHM Advisors



Brewer Park. Source: Google Earth

Recent City Improvements

Over the past decade, the City of Detroit has made steady progress in Greater Warren/Conner, investing in various programs intended to address blight, stabilize homes, improve schools and neighborhood amenities, and support small businesses.

Since 2015, nearly \$51 million has been directed toward the Greater Warren/Conner neighborhoods.

1,223

Blighted Home Demolitions

110

Planned Demolitions

313

Homes Repaired or Stabilized

468

Public Home Sales

Investments in the Neighborhood

\$26.2M

Total Demolition Cost since 2014

\$21.5M

Total Investment in Chandler Park

\$4.3M

Roadway Improvements

\$1.6M

Vacant Home Rehab & Stabilization

\$7M

Career Technical Education Programs

\$150K

Green Stormwater Infrastructure

\$1M

Planned for Blight Removal

\$2.1M

Home Repair Programs

\$190K

Motor City Match Small Business Support



Neighborhood Stabilization

Detroit has traditionally been a city characterized by high home ownership, with homes often passed down through generations, maintaining a familial ownership structure. This began to change starting in the 1970s, and in 2012, Detroit's homeownership rate dipped below 50% for the first time. [6] In the Greater Warren/Conner community, 48% of the homes are owner-occupied, aligned with homeownership trends across the city. There is a significant portion of longtime residents in the neighborhood. Roughly 22% of residents moved into their homes in 1989 or earlier, indicating long-term ties to the neighborhood.

Two of the largest challenges with improving the neighborhood conditions are: the availability of quality homes in the area, and the economic challenges of improving an underperforming housing market. Throughout the Greater Warren/Conner neighborhood, there are areas of high vacancy, empty lots, blighted structures, and few nearby amenities. These conditions build on each other and affect the overall quality of life within the neighborhoods.

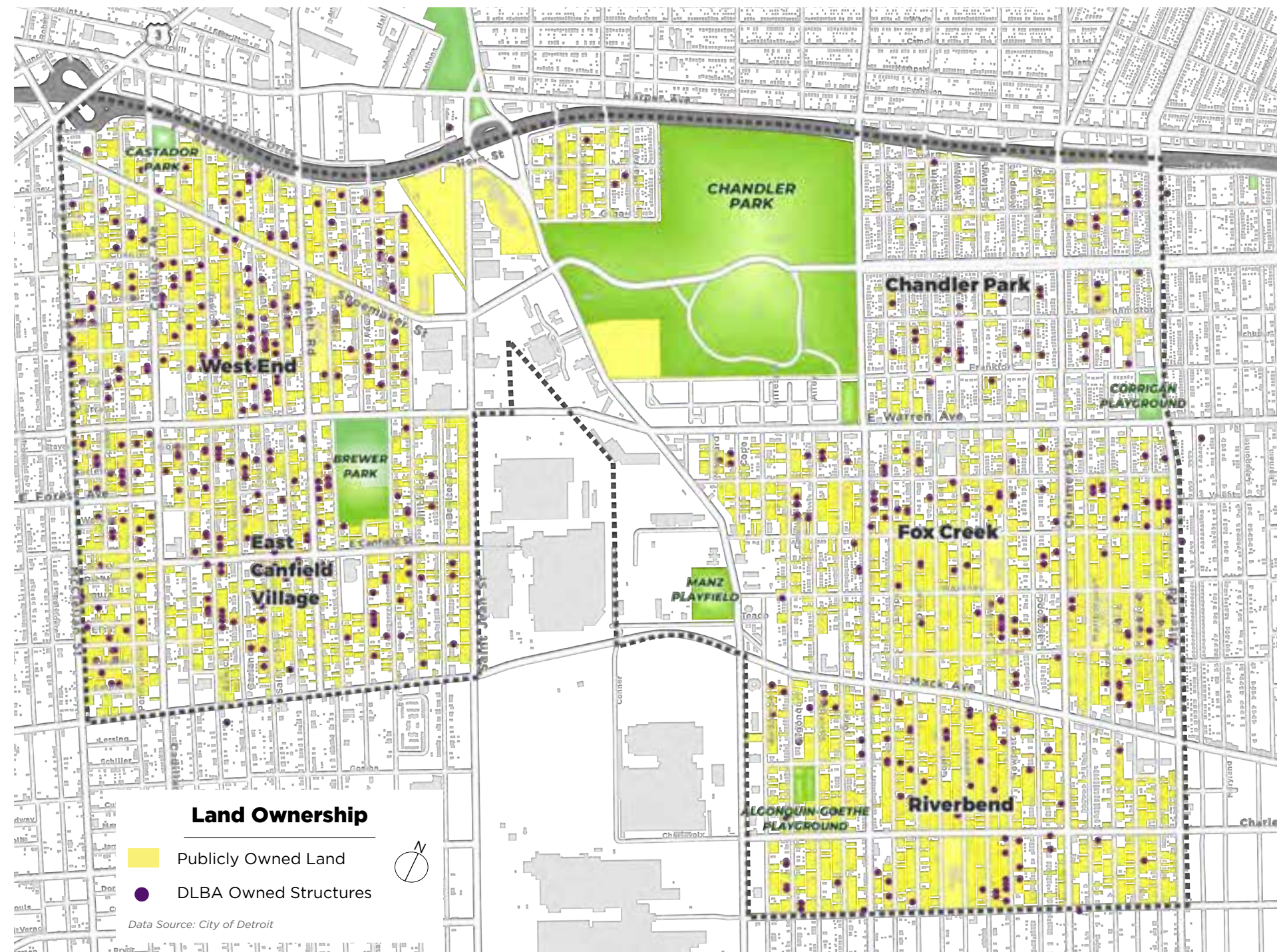
Housing Diversity. The housing landscape predominantly consists of two-to-three-bedroom, single-family homes (59%) between 1,200 and 2,000 square feet in size, many of which were built between 1940 and 1959. A few multi-family units exist in the neighborhood as well, providing some diversity of housing types for residents. The aged housing stock often necessitates home repairs, posing financial challenges for low to moderate-income residents and impacts residents' overall safety and well-being. [7]

Occupancy. As of 2023, the DLBA is the largest landowner in the neighborhood. Of the 7,303 vacant residential properties in Greater Warren/Conner, 59% of these are publicly-owned by the DLBA. Most of these parcels and properties came to the DLBA through property tax foreclosure and were forfeited to the agency.

Median Home Values. In 2022, the neighborhood reported a median home value of \$42,800, which has been influenced by factors such as high vacancies and limited economic diversity in the area. The City of Detroit, on average, has a median home value of about \$64,000 and the Detroit metro region is roughly \$230,000. [8]

Many residents who own their homes do not have the available income to address the ongoing maintenance needs that come with home ownership. As these homes fall into disrepair, a vicious cycle continues where more maintenance is needed for safe habitation. Costs for home repairs compound as deferred maintenance continues to accumulate.

Home Sales Trends. While the median home value helps understand the overall appraisal conditions in a neighborhood, it does not tell the whole story about how much homes are worth in a specific area. Data from recent sales in Greater Warren/Conner was compiled for the last five years to determine where housing is in high demand and how the values have changed over time. Trends observed from this data can help determine where the housing market is strongest, where demand is concentrated, and, ultimately, where public investments in home rehabilitation and structure stabilization would provide the most value.





Neighborhood Stabilization

	Chandler Park (Census Tract 5026)	Fox Creek (Census Tract 5027)	Riverbend (Census Tract 5128)	East Canfield Village (Census Tract 5141)	West End (Census Tract 5143)	Greater Warren/ Conner
Median Housing Value	\$54,300	\$38,400	\$55,600	\$34,800	\$32,400	\$42,789
Total Housing Units	1,546	1,278	1,357	1,021	1,040	6,242
Occupied	1,212	958	958	716	879	4,723
Vacant	334	320	399	305	161	1,519
% Occupied	78%	75%	71%	70%	85%	75%
% Vacant	22%	25%	29%	30%	15%	25%

Source: 2022 5-Year ACS Estimates, Tables B25077, DP04.

The data in the above table and on the following page illustrates the real estate market conditions in the Greater Warren/Conner neighborhood. Overall, median housing values in 2020 ranged from about \$32,000 to over \$55,000, Chandler Park and Riverbend have the highest home values of the neighborhoods. Chandler Park has also seen the most private sales activity recently, likely due to the move-in ready housing stock, proximity to Chandler Park, and E. Warren Avenue and less overall vacancy in the surrounding area. Since 2017, both private and public sales prices have risen consistently, matching the national trend. The trend upwards of private sales prices from an average of \$10,450 in 2017 to nearly \$48,000 in 2022; this trend in sales price is essential to a functional and stable real estate market.

Rent Burden. Many renters in Greater/Warren Conner are rent-burdened, meaning that these households spend more than 30% of their income on rent. About 63% of all renters in the area spend more than 30% of their income on rent. More significantly, 1/3 of residents allocate 50% or more of their income or more toward rent, underscoring the heavy financial strain experienced by renters in Greater Warren/Conner compared to the rest of the city. In 2022, an estimated 55% of Detroit renters spent over 30% of their income toward rent, and 31% paid 50% or more of their income toward rent. [9] With renters making up over half of the Greater Warren/Conner population, addressing this issue is crucial to ensuring access to quality, affordable housing that meets their needs.

10-Year Housing Projection. A slight increase of about 100 housing units is anticipated, or 2%, is anticipated through 2032. With 87% of existing units built before 1980 and 53% before 1950, the community could benefit from a mix of renovated housing and new construction to fill existing vacancies. With mostly one- and two-person households and an average household size of 2.4 people, there is a need for two-to-three-bedroom homes ranging from 1,200-1,600 square feet to align with market demands.

Publicly Owned Vacant Land Distribution. The Greater Warren/Conner neighborhood (2,362 total acres) has a significant amount of vacant land (757 acres, or 32%) distributed throughout the neighborhoods. The highest concentrations of vacant publicly owned land can be found in the Riverbend (42% of all parcels) and Fox Creek (35% of all parcels) areas on either side of Mack Avenue. Strategies to activate vacant land are recommended for these zones. Additional pockets of concentrated vacancy exist on the west side in the East Canfield Village area (30% vacant).



Homes in the Chandler Park neighborhood. Source: OHM Advisors

Total Home Sales (2017-2023) in the Greater Warren/Conner Neighborhood

Sale Type	Total Sales (2017-2023)	Average Sales Price	2017	2018	2019	2020	2021	2022
Private Owner	185	\$33,362	\$10,450	\$25,674	\$27,140	\$31,874	\$46,241	\$47,894
DLBA Own It Now	375	\$1,957	\$1,480	\$1,213	\$1,753	\$2,251	\$2,225	\$2,856
DLBA Partner Sales	127	NA						

Source: Detroit Land Bank Authority, City of Detroit



Commercial and Economic Development

Supporting and preserving commercial resources in Greater Warren/Conner is critical to increasing livability and fostering a sense of place while furthering the community's economic goals. The Market Analysis provides insights into existing commercial resources as well as the strategic opportunities to build upon these assets in the coming years. Emphasis is placed on grocery stores, restaurants, health centers, and pharmacies, which are important for communities like Greater Warren/Conner to thrive.

Within Greater Warren/Conner, there are four main commercially zoned corridors, each with varying degrees of active businesses, vacant buildings, vacant land, and public ownership. The commercial corridors in the neighborhood are:

- Conner Street from I-94 to E. Vernor Highway
- Warren Avenue from McClellan Street to Alter Road
- Mack Avenue from McClellan Street to Alter Road
- Shoemaker Street from McClellan Street to Lillibridge Street

Overall, the areas with the largest concentration of active businesses occur along Conner Street at E. Warren Avenue, as well as E. Warren Avenue and Mack Avenue east of Conner Street. Many of the retail businesses available provide the everyday services that residents in the neighborhoods need, including hardware stores, groceries, party stores, auto repair shops, gas stations, and clothing stores. There are few restaurants, coffee shops, and other gathering places, where residents can meet and interact with each other.





Commercial and Economic Development

There are 496 vacant commercial properties in the community along all four of the major commercial streets in Greater Warren/Conner; E. Warren Avenue, Mack Avenue, Shoemaker Street, and Conner Street. E. Warren Avenue and Conner Street have the most active commercial properties, while Shoemaker Street and Mack Avenue contain large pockets of vacant land. For example, along Shoemaker Street, there are 49 publicly owned vacant commercial properties ranging in size from 1,700 to 20,000 square feet.

In the Greater Warren/Conner neighborhood, the average sales price for commercial properties stands at \$343,600, with an average cost per square foot of \$48.72. Commercial listings exhibit varying conditions, ranging from well-maintained establishments to those in need of rehabilitation. The average price per square foot is about \$7 higher than the City of Detroit average, likely due to the size of the buildings and the proximity to Stellantis. Additionally, there are few commercial properties available for sale, which may be skewing the data away from the City average.

The spectrum of building types includes neighborhood commercial structures, retail plazas, and mixed-use properties featuring commercial spaces on the ground floor and residential units on the upper floors. At vacant properties, there is also untapped potential for new leasing opportunities, particularly for big box stores, with net rental rates at \$7 per square foot.

	GREATER WARREN/CONNER	CITY OF DETROIT
Median Sales Price	\$343,600	\$131,000
Average Price per Square Foot	\$49	\$41

Source: Cushman & Wakefield Detroit Marketbeat Reports, Q3 2023.



Mobility and Streets

Taking cues from the Detroit Streets for People Design Guide (2021), [10] the major street networks in Greater Warren/Conner were categorized as Arterial Commercial/Industrial, Collector Commercial, Collector Residential, or Parkway. The Streets for People guide recognizes that the streets “fundamentally impact people’s quality of life, health, mobility, and safety.” Understanding this network assisted in creating recommendations for improvements to ensure safety, connectivity, accessibility, mobility, and quality of life improvements for all who live, visit, work, or traverse the neighborhood.

Transit. Mobility is a challenge for residents within Greater Warren/Conner. An estimated 38% of households in the neighborhood do not have access to a vehicle. The area is served by DDOT bus routes along Shoemaker Street, Cadillac Avenue, E. Warren Avenue, Connor Street, and Chalmers Street. Given the proximity of many resident-focused amenities and the high percentage of residents without a vehicle, creating a more robust functional transportation network is essential to the health of the community.

Truck Routes. The neighborhood is home to a number of truck routes that serve the Stellantis plants and other industrial suppliers in area, the main route being Conner Street. The diesel-powered semi trucks that pass through the neighborhood are a major contributor of emissions and noise pollution that affects the quality of life of residents.

Bike Infrastructure. Individuals traveling by bicycle have access to protected bike lanes on Conner Street, which makes up the Conner Creek Greenway, and portions of E. Warren Avenue. There are additional bicycle facilities along portions of Shoemaker Street, St. Jean Street, and Mack Avenue, which are disconnected from the others but have the potential to make safe connections between Greater Warren/Conner and other areas within the City.



Mobility and Streets

Of particular importance in Greater Warren/Conner is the Iron Belle Trail, a state-wide initiative which, when completed, will span 2,000 miles from Ironwood in the western Upper Peninsula to Belle Isle in Detroit. The proposed route within the neighborhood includes portions of Conner Street, Shoemaker Street, and E. Warren Avenue along Brewer Park. A large, pedestrian/bike-only dedicated bridge will be built by MDOT over I-94 at Conner as part of the Iron Belle Trail.

Additionally, the Canfield Consortium, an active neighborhood group, is engaging the City of Detroit to establish a walking and biking route in East Canfield Village from Brewer Park to Pingree Park. This initiative aims to not only improve neighborhood connectivity and mobility, but also reactivate commercial corridors, enhance neighborhood beautification, secure funding for infrastructure improvements, and provide community-driven access to key locations.

Despite some improvements and facilities in Greater Warren/Conner, there are physical barriers that severely impact mobility, safety, and accessibility. Stellantis divides the area and disrupts neighborhood cohesion. The recent expansion of Stellantis resulted in decommissioning St. Jean Street, a City-owned street, and removing the Conner Creek Greenway from its original location. The completion of I-94 at the northern edge of Greater Warren/Conner during the 1950s also displaced residents and businesses. These physical elements collectively contribute to difficulty and safety concerns associated with pedestrian and non-motorized connections.





Wide pathways through Chandler Park. Source: OHM Advisors



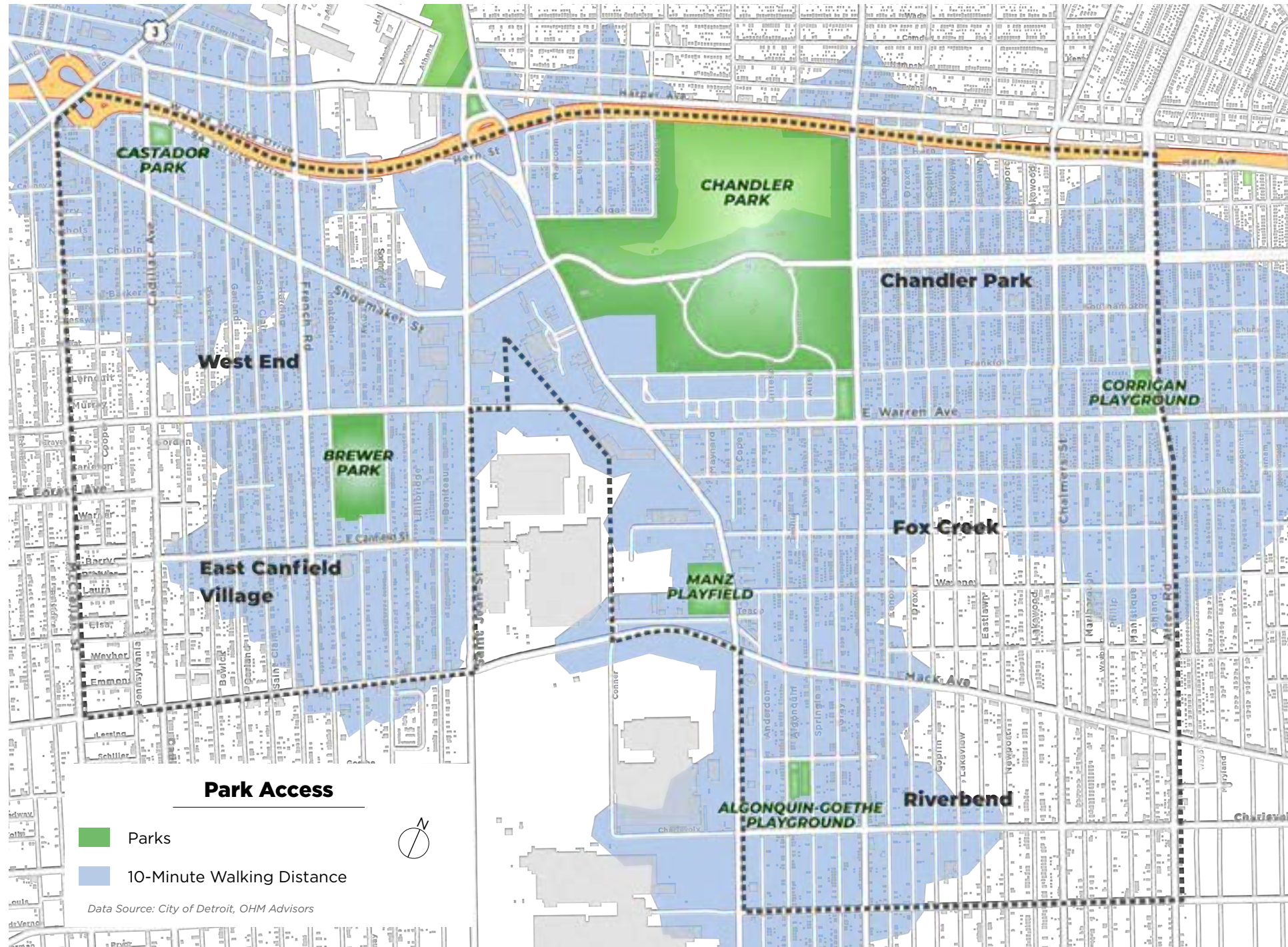
Parks and Open Space

Within Greater Warren/Conner, there are five parks maintained by the City of Detroit General Services Department (GSD). These parks encompass 113 acres of parkland, translating to about 1/10th of an acre of parkland per person. The 110-acre public Chandler Park golf course is also in the neighborhood. Nearly all residents (90%) live within 0.5 miles (or a 10-minute walk) of green space and parks, surpassing the city-wide average of 83%. However, despite the high accessibility, many of the parks in Greater Warren/Conner lack amenities or have amenities in need of repair or replacement.

The City of Detroit's Parks and Recreation Strategic Plan (PRSP) recognizes that the parks in Greater Warren/Conner have not been fully improved. This underscores the importance of not only ensuring proximity to green spaces, but also investing in the enhancement and maintenance of these areas to maximize their positive impact on the community's well-being and recreational experiences.



Detroit Pistons sponsored basketball courts in Chandler Park. Source: OHM Advisors



Parks and Open Space



Algonquin-Goethe Playground

NEIGHBORHOOD Riverbend

SIZE 2.69 acres

**RESIDENTS WITHIN
A 10-MINUTE WALK** 936

PARK TYPOLOGY Active walkable park

- CHARACTERISTICS**
- » Basketball courts (replaced in 2018)
 - » Play equipment
 - » Benches, picnic tables & grills



Brewer Park. Source: Google Maps

Brewer Park

NEIGHBORHOOD	East Canfield Village
SIZE	8.18 acres (+10.6 adjacent vacant acres owned by the DLBA, former demolished Joy Middle School)
RESIDENTS WITHIN A 10-MINUTE WALK	1,493
PARK TYPOLOGY	Active walkable park
CHARACTERISTICS	<ul style="list-style-type: none">» Passive green space and trees» Vacant with no amenities and no regular maintenance



Castador Park. Source: Google Maps

Castador Park

NEIGHBORHOOD	West End
SIZE	1.74 acres
RESIDENTS WITHIN A 10-MINUTE WALK	1,313
PARK TYPOLOGY	Active walkable park
CHARACTERISTICS	<ul style="list-style-type: none">» Vacant with no amenities and no regular maintenance



Parks and Open Space



Chandler Park Skatepark. Source: OHM Advisors



Chandler Park. Source: OHM Advisors

Chandler Park

NEIGHBORHOOD	Chandler Park
SIZE	200 acres total: 90 acres parkland and 110 acre public golf course
RESIDENTS WITHIN A 10-MINUTE WALK	3,137
PARK TYPOLOGY	Regional park <ul style="list-style-type: none">» Major destination park» Golf course» Wayne County Family Aquatic Center» Rentable pavilions» Skatepark (completed in 2021)» Artificial turf football, soccer, and lacrosse field (expanding and being enclosed with a year-round dome in 2024)
CHARACTERISTICS	<ul style="list-style-type: none">» Basketball courts (2, refinished in 2021)» Tennis courts (4)» Pickleball courts (4)» Softball and T-ball diamonds (2)» Play equipment» Outdoor fitness equipment



Corrigan Playground. Source: OHM Advisors

Corrigan Playground

NEIGHBORHOOD	Chandler Park-Chalmers
SIZE	3.24 acres
RESIDENTS WITHIN A 10-MINUTE WALK	1,753
PARK TYPOLOGY	Active walkable park
CHARACTERISTICS	<div>» Gazebo</div> <div>» Softball and T-ball diamond</div> <div>» Play equipment</div>



Manz Playfield. Source: SAY Detroit

Manz Playfield

NEIGHBORHOOD	Conner Creek Industrial
SIZE	7.37 acres
RESIDENTS WITHIN A 10-MINUTE WALK	932
PARK TYPOLOGY	Active walkable park
CHARACTERISTICS	<div>» Baseball diamond</div>



Climate Resilience and Health Equity

Neighborhood resilience is the result of a system that supports residents’ needs daily and in emergencies. [11] In the context of Climate Resilience and Health Equity, this Focus Area extends to understanding the repercussions of climate change-induced events, particularly flooding and extreme heat, on individuals, communities, businesses, and public infrastructure. This impact is often more severe for communities with limited infrastructure investment, posing heightened risks for vulnerable populations such as the sick, elderly, those without access to vehicles, individuals with disabilities, and those with lower incomes.

Central to this analysis is the commitment to equity, which ensures fair treatment, access, and opportunities while actively identifying and dismantling barriers that hinder the full participation of communities in Detroit. Building neighborhood resilience relies on a comprehensive system that caters to residents’ daily needs and ensures preparedness for emergencies. Further, Greater Warren/Conner not only faces disparities in infrastructure investment, but air quality, contributing to the negative health outcomes that amplify the challenges faced by its residents.

Living with Water. Urban development buried the historic waterway Conner Creek, contributing to flooding in Greater Warren/Conner. [12] Flooding and significant damage from the increasing frequency and intensity of storms are more tangible today than ever. According to the data from 2012 and 2020, the Greater Warren/Conner neighborhoods are within a hotspot of flooding on the west side of Stellantis. [13] To address recurrent flooding in the neighborhood will require sustainable development practices and innovative stormwater management strategies moving forward.



Climate Resilience and Health Equity

Environmental and Public Health Concerns. The most socially vulnerable community members are often most at risk of environmental threats. According to the Environmental Protection Agency's Environmental Justice Screening and Mapping Tool, [5] residents in the Greater Warren/Conner neighborhood have a high risk of exposure to Particulate Matter 2.5, Ozone, Diesel Particulate Matter, Lead Paint, and Hazardous Waste, and to the health conditions correlated with these exposures. These exposures are within the 90th to 100th percentiles in comparison to the State of Michigan's averages and national averages.

The census tracts around the Stellantis Impact Area have some of the highest asthma hospitalization rates in the city and nearly three times the rates of the rest of the State of Michigan, according to data collected by East side Resident Environmental Health Working Group. [14] The vulnerability of this community to COVID-19 has also been increased by exposure to increased pollution. Further, it is important to note that these areas are medically underserved, which is a critical service gap for these residents.

Urban Heat. The urban heat island effect refers to the increased temperatures that highly urbanized areas experience due to the replacement of natural land cover with concrete, buildings, and roadways that absorb and retain heat. Greater/Conner is highly subject to the urban heat island effect given the large expanse of concrete and industrial buildings that are located in the central portion of the neighborhood. Additionally, population loss and economic conditions, coupled with various tree diseases in the surrounding neighborhoods, have contributed to a reduction in the tree canopy over the years.

The reduced tree canopy, large expanses of parking areas, massive buildings, and proximity to large roadways have resulted in areas of Greater Warren/Conner that experience much higher temperatures than the rest of the City of Detroit. Street trees, green roofs, additional vegetation, and reductions in concrete areas can help mitigate the effects of urban heat during the hottest summer days.

FRAMEWORK STRATEGIES

This section includes overarching strategies, and specific recommendations created from utilizing those strategies, to further the goals of the Neighborhood Framework Plan.

Goals

As a collaboration between the City of Detroit and the Greater Warren/Conner Neighborhood, the Plan seeks to achieve the goals as stated in the Executive Summary:

- Ensure the Greater Warren/Conner neighborhood thrives alongside the City of Detroit.
- Address historical divisions created by the development of heavy industry and highway transportation through the community.
- Guide the community toward a more resilient and equitable future.

Strategies

The recommendations detailed in this chapter were created using multiple strategies designed to increase stability, connectivity, resilience and sustainability throughout the Greater Warren/Conner neighborhood. They revolve around both improving the neighborhoods' existing amenities and infrastructure, as well as enhancing the local economy, addressing environmental issues, and improving the overall quality of life for residents.

Recommendations

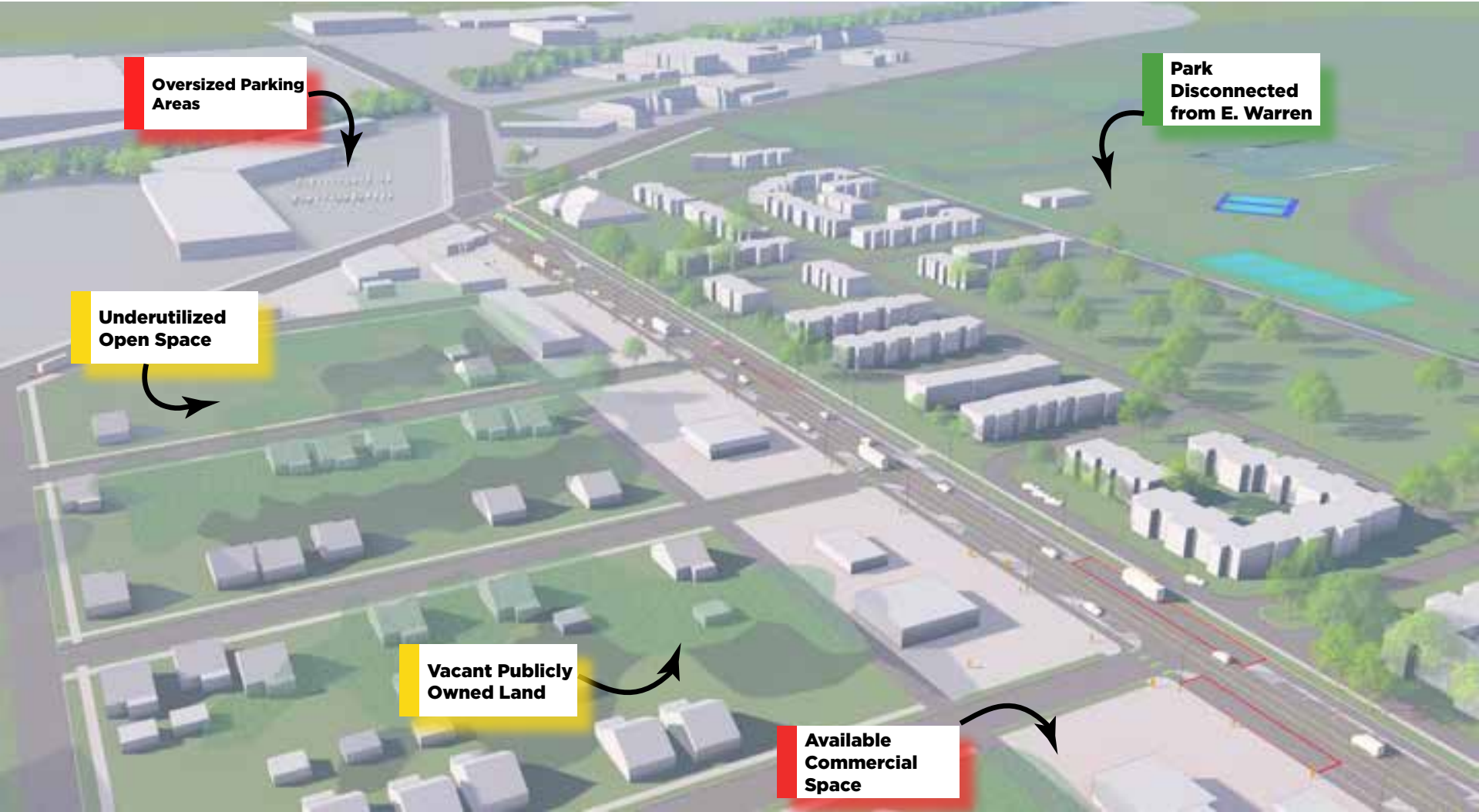
The Plan recommendations that follow are presented in separate sections according to the specific Focus Areas as stated in the Executive Summary, including: **Neighborhood Stabilization, Commercial and Economic Development, Mobility and Streets, Parks and Open Space, and Climate Resilience and Health Equity.** Within each section are recommendations that might apply to all of the neighborhood, to certain neighborhood sections, or both. The included maps are shown to illustrate the locations for the suggested recommendations.

The rationale for this Framework Plan presentation is due to the relatively large size of the Greater Warren/Conner neighborhood and the significant changes to its land use, population, and investment over the previous decades. In order to address the scale of re-investment needed, a primary strategy in developing the plan recommendations was the establishment of activity nodes throughout the Neighborhood. By co-locating future investments and targeting areas where improvements from different Focus Areas can build on each other and have spillover effects, the goal is to benefit a larger portion of the neighborhood.

An example of this strategy is to focus Rehabbed & Ready housing revitalization efforts around the Hamilton Academy Elementary-Middle School in Chandler Park to attract more families to the area surrounding the school. Hamilton Academy will benefit from additional students who are able to walk to school and there will be fewer vacant homes nearby improving safety in the neighborhood. This in turn, attracts more residents to the neighborhood, boosts enrollment at the school, and expands its status as a quality educational facility. The cycle can repeat if positive progress is made each time.

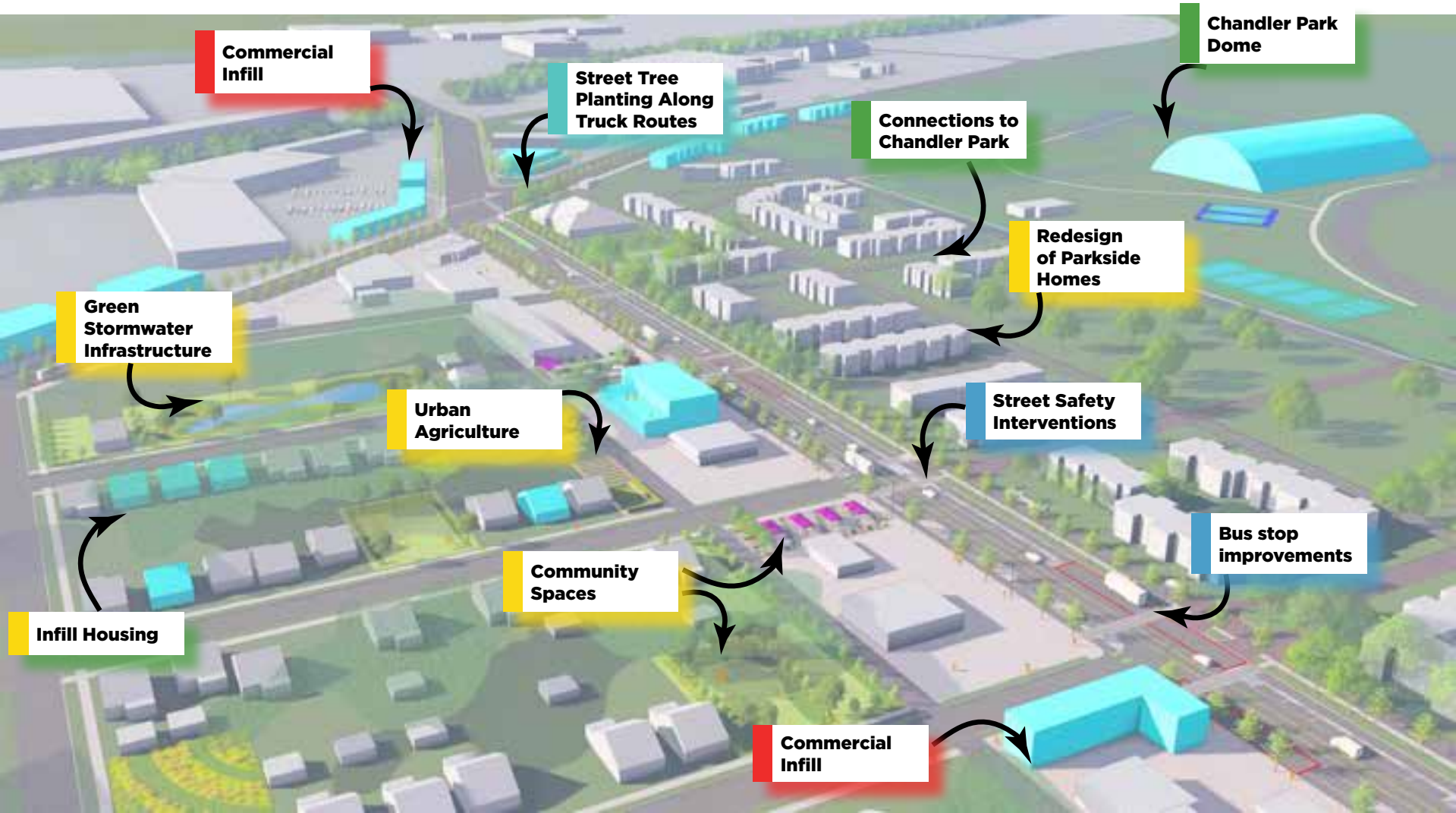


The graphics below intend to illustrate the benefits of co-locating the neighborhood investments recommended in this Framework Plan. The goal with this strategy is to first build off of the existing activity in the area, capitalize on any planned improvements, and identify the locations where the Framework Plan recommendations are preferred. Long term, these coordinated investments are envisioned to attract residents and visitors to an area to support the community's amenities and businesses.



Current conditions of E. Warren Avenue at Conner Street.

- | | | |
|--|---|--|
| Neighborhood Stabilization | Mobility & Streets | Climate Resilience & Health Equity |
| Commercial & Economic Development | Parks & Open Space | |



Potential future conditions of E. Warren Avenue at Conner Street.

- | | | |
|--|---|--|
| Neighborhood Stabilization | Mobility & Streets | Climate Resilience & Health Equity |
| Commercial & Economic Development | Parks & Open Space | |



Neighborhood Stabilization Recommendations

In the context of this Plan, “Neighborhood Stabilization” is the process of improving the residential areas in the community through three strategies: **sustain, stabilize, and stimulate**. Each of these strategies is intended to be deployed in specific areas of the neighborhood, depending on the quality and availability of the existing housing stock, and combined with other investments in mobility, sustainability, parks, vacant land activation, and commercial development. The strategies identified in this plan are intended to improve housing choices and quality of life for current residents while attracting new residents to the area.



Sustain – Public investments in home rehabilitation should be directed to the most stable areas in the neighborhood to maximize the return on investment and build nodes of activity.



Stabilize – Areas with less stability and higher concentrations of existing publicly owned structures should be targeted for demolition and stabilization programs to catalyze the area for future development.



Stimulate – New housing should be targeted at key locations, including the redevelopment of Parkside Homes, within the Targeted Multi-family Housing Area near Mack Avenue and Alter Road, as well as through small-scale infill development in areas with high concentrations of vacant land. Infill should be tied to the productive uses of vacant land near public amenities, retail opportunities, and transportation options to catalyze future private investment.

The goal with these strategies is to enhance housing options and residents’ quality of life, while also drawing in new residents. By combining public infrastructure and community investments with efforts to stabilize the neighborhood, positive effects will be generated in each targeted area.



Neighborhood Stabilization Recommendations

The Neighborhood Stabilization Focus Area recommendations provide a guide for where and how to deploy residential focused strategic investments, targeted rehabilitation efforts, and collaborative partnerships within Greater Warren/Conner. The proposed solutions aim to address blight, promote economic development, and enhance overall community well-being in the neighborhoods.

- Focus stabilization efforts on **rehabilitating homes** within established housing markets, near neighborhood anchors and along primary commercial corridors.
- Focus **commercial and retail development efforts** near these areas to ensure access to everyday goods and services.
- **Implement Green Stormwater Infrastructure (GSI) initiatives** on vacant parcels to alleviate and reduce flooding impact.
- **Support for non-motorized investments** like the Canfield Connect and Iron Belle Trail routes in Chandler Park, East Canfield, Fox Creek, and West End will elevate desirability and value of existing properties around them.
- **Leverage partnerships and expertise with organizations** to provide additional funding and support for home repair and renovations.
- **Establish a community land trust** to manage clusters of vacant land in proposed Resilience Zone areas to create new affordable home-ownership opportunities, preserve open space, support urban agriculture, and other community-driven projects
- Integrate community-preferred land activation strategies to **create Resilience Zones** in suitable areas with large clusters of vacant land in Fox Creek, Riverbend, and East Canfield Villages.
- Infill development should be focused first on the **Targeted Multi-family Housing Area** at Mack Avenue and Alter Road to better leverage current incentive programs.

Neighborhood Stabilization Tools

There are a variety of tools that the City of Detroit and its partners can use to support neighborhood stabilization efforts within Greater Warren/Conner. The programs below are intended to provide both financial resources and property deals to entice investment in neighborhoods around Detroit.

Detroit Neighborhood Initiative (DNI) [15] offers financial incentives for home buyers that complete the NACA pre-purchase home buyer program. Incentives include no down payment, no closing costs, low fixed interest rates, and renovation funding within the mortgage. This program could be used to support homeowner renovations of existing homes in the neighborhood.

The **Detroit Down Payment Assistance Fund** [16] provides up to \$25,000 in down payment and closing cost assistance to qualifying residents.

The **Michigan Home Loan Program** [17] has expanded to be offered statewide, providing up to 10,000 in down payment assistance to qualifying homebuyers.

The **Detroit 0% Home Repair Loan Program** [18] offers 0% interest loans from \$5,000 to \$25,000 to help homeowners invest in and repair their homes. Example projects include correcting health and safety hazards (required), electrical repairs, furnace replacement, roof replacement, plumbing, door and window replacement, and porches and structural support.

Detroit Land Bank Authority (DLBA) [19] acquires, manages, and sells tax-foreclosed and abandoned properties, often at discounted rates, to responsible buyers who commit to rehabilitating them. Some of the following programs are listed below:

Own It Now: As-is houses for sale on the DLBA website. The first offer on a property starts a 72-hour countdown clock, when the time expires the highest bidder wins.

Rehabbed and Ready: This program sells professionally renovated homes to stabilize the housing market and bring new residents to a neighborhood.

Marketed Properties: Program offering opportunities for new builds, single-family homes, commercial and residential properties, and land-based projects. All potential buyers must submit proposals to purchase.

Buy-Back Program: Provides a pathway to homeownership for precariously housed individuals and families living in houses owned by the Land Bank.

Nuisance Abatement: Program helping neighbors hold private owners of blighted and vacant structure accountable by allowing the Land Bank to legally seize blighted vacant property.

Side & Neighborhood Lots: Adjacent DLBA lots or those within 500 feet of Detroit homeowners with a Principal Residence Exemption are available for purchase.

Community Development (Community Partners): Community Partners can use DLBA properties for home rehab projects, urban gardening, green space projects or any use that enhances quality of life in neighborhoods.

Infill Lots: The DLBA sells vacant lots for the development of new housing, which will target 1-2 lot projects to create opportunities for local small-scale developers.

Homestead Lots: This program will include adjacent vacant lots in a land assemblage of Homestead Lots that include an existing residential structure of 1-4 units.



Before and after of a Rehabbed and Ready home in the Crary St. Mary’s neighborhood in Northwest Detroit. Source: Curbed Detroit



Neighborhood Stabilization Recommendations

Sustain Home Rehabs in Chandler Park

Focusing efforts of the existing Rehabbed & Ready Program could be used to rehabilitate select homes in the Chandler Park neighborhood and help to revitalize the housing market in the area. The proposed program would focus on rehabilitating existing vacant homes to move-in ready condition, thereby attracting home buyers and increasing property values in the community. An initial investment should be targeted in the Chandler Park neighborhood, where there is higher potential with market demand and denser blocks, in addition to amenities such as Chandler Park and the nearby E. Warren Avenue commercial corridor.

A cluster of DLBA-owned structures are found in Chandler Park between Frankfort and Southampton Streets, which could be potential rehab opportunities. The Proposal N Demolitions and Stabilizations [20] dashboard shows that seven homes within this area have been secured for resale and three have been prepped for resale, demonstrating stability in the housing market and significant investment in the neighborhood already.

Stabilize Home Stabilization in East Canfield Village

A significant number of vacant properties are concentrated under the ownership of the DLBA in the East Canfield Villages neighborhood near French Road and Brewer Park. A strategy to preserve viable homes for future renovation should be employed in this area, alongside targeted demolitions, side lot activations, and property marketing. Given the current market conditions, the DLBA and other City partners would have to highly subsidize home rehabs in this area, resulting in fewer home rehabs and less impact to the neighborhood.

By stabilizing existing structures and clearing blight through this portion of the Greater Warren/Conner community, progress can be made to set East Canfield Village up for future reinvestment, while improving the quality of the neighborhood in the short term. A similar approach to the Sustain zone where projects are concentrated along a single block is recommended to maximize the value of the investment made.





Neighborhood Stabilization Recommendations

Stimulate Targeted Multi-family Housing Area

The City of Detroit's Targeted Multi-family Housing Areas (TMHA) are locations throughout the City that are recommended for investments in higher density multi-family housing developments. The goal of these areas is to locate residential development in walkable urban nodes that provide access to everyday services, public transit, neighborhood amenities, and employment to both existing and future residents of the neighborhood. Additionally, within the TMHAs, affordable housing units will be preserved and new units incentivized.

Within the Greater Warren/Conner neighborhood, there is a designated TMHA in the southeast quadrant near the intersection of Mack Avenue and Alter Road. This TMHA is bounded by Mack Avenue on the north, Alter Road on the east, Lakewood Street on the West, and continues south to the Detroit River outside of the neighborhood.

Development of new housing within the Greater Warren/Conner neighborhood should be first targeted within the current TMHA to build off of the current market conditions, connect to existing nearby retail and amenities, advance the previous planning of the Housing and Revitalization Department (HRD), and utilize development incentives currently offered by HRD and other City of Detroit programs.

Stimulate Redevelopment of Parkside Homes

The Parkside Homes are a public housing development operated by the Detroit Housing Commission. Over the years, many of the units have fallen into disrepair and tenants have moved out, reducing the population. However, an effort to redevelop the complex into an updated and modern community has begun. Given the focus on E. Warren Avenue and Conner Street as part of this plan, there is the opportunity to reimagine how Parkside is oriented to enhance the two commercial corridors and better connect residents to Chandler Park.

Currently, the Parkside Homes buildings are set back roughly 50 feet from the sidewalk, separated by a fence, and oriented perpendicular to E. Warren Avenue. This reduces Parkside's connection to the main streets and greater neighborhood, makes walking and biking more difficult for residents (many of which lack access to a vehicle), and contributes to the feeling that Parkside is not part of the community.

The upcoming redesign and redevelopment of Parkside Homes should focus on establishing a consistent street wall along both E. Warren Avenue and Conner Street giving both corridors a sense of enclosure, improving walkability, and better connecting to the rest of the neighborhood. Efficient access to Chandler Park should also be prioritized from the Fox Creek neighborhood to the south, especially for non-motorized users.

The City of Detroit should consider a traditional main street zoning overlay along this segment of E. Warren to ensure that future development and redevelopment meet a design standard that encourages walkability, street activity, and connectivity within the neighborhood. E. Warren Avenue within the Morningside and East English Village neighborhoods currently has a traditional main street overlay zoning designation that reduces the need for parking, allows for a greater mix of uses along the corridor, and establishes urban design standards to establish an identity of the street.

Parkside Homes Design Standards



CONTINUOUS BUILDING FRONTAGE



APPROPRIATE SCALE



Mixed-use BUILDINGS



REAR PARKING ORIENTATIONS



CONNECTED PATHWAYS



CONNECTED STREETS



Neighborhood Stabilization Recommendations

Stimulate Resilience Zones

The Greater Warren/Conner neighborhood is unique from other Detroit neighborhoods in that there are large amounts of non-contiguous vacant land owned by the DLBA. Within these areas, there are still pockets of occupied homes. In addition to the large amount of available vacant land, the proposed Resilience Zones are located in areas that are also at a higher risk of flooding, experience elevated air pollution levels, and are more likely to experience urban heat island effects in the summer.

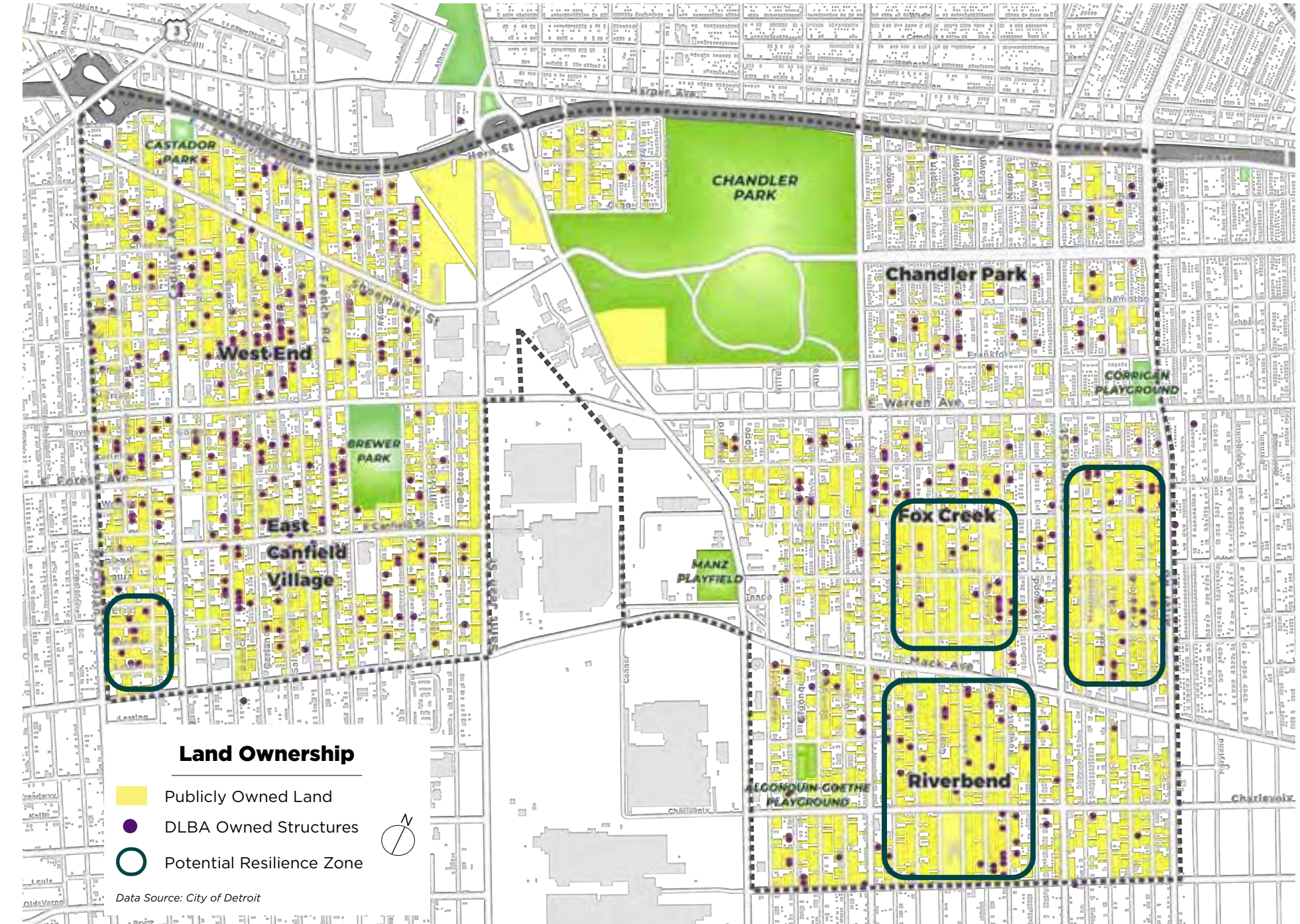
The Resilience Zones are inspired by efforts to blend the need for climate resilience and health equity with neighborhood stabilization strategies. They are designated areas within Greater Warren/Conner that may feature clusters of existing and infill homes near green stormwater systems, forests, and urban farms, all connected via walking paths.

The resilience zone concept is inspired by a growing understanding that development centered on social and environmental health can produce multiple benefits for individuals and communities. Resilience zones offer an opportunity to create a livable community with a commitment

to sustainability, dedicating spaces for agriculture, reforestation, bird habitat, stormwater management, arts and entertainment, and community spaces. The landscape, businesses, public transit, and community spaces will be made easily accessible through walking trails to all members of the community. Planning efforts will also include affordable and diverse housing development and investment into mixed-uses along commercial corridors to create synergy in the area.

Locations

Within Greater Warren/Conner there are four distinct areas where the concentration of vacant publicly-owned land is highest. Large clusters of vacant land suitable for the creation of Resilience Zones are in Fox Creek, Riverbend, and East Canfield Village.

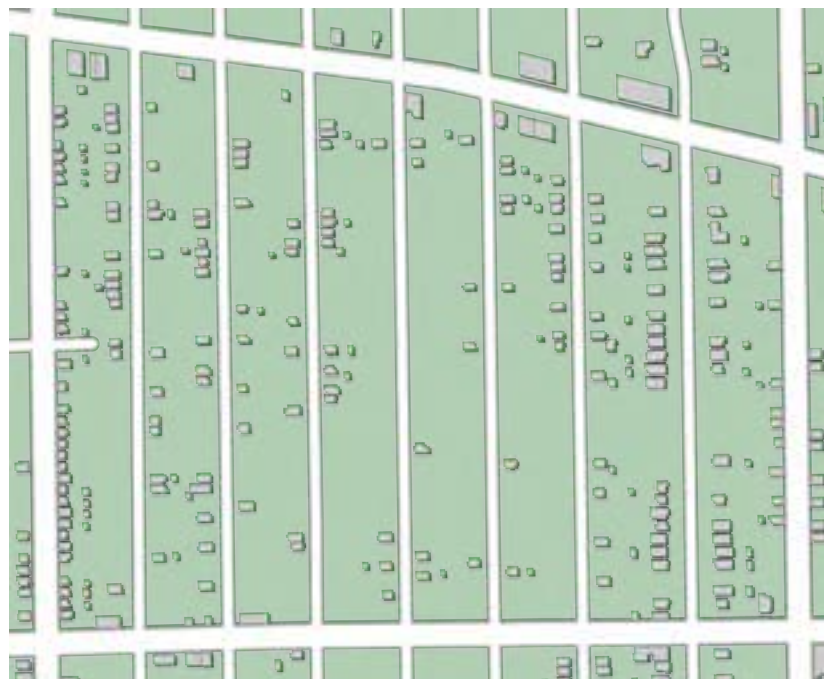




Neighborhood Stabilization Recommendations

Implementation

The implementation of Resilience Zones around Greater Warren/Conner is intended to address the desires residents voiced during the planning process. Residents are concerned about air quality degradation from nearby industrial uses, flooding from increasingly intense storms, the desire to access nature, traffic safety, and the lack of safe pedestrian connections. The combination of land activation strategies with residential development results in both a safe and healthy place to live and a unique development pattern that could serve as an example for other neighborhoods around Detroit.



Currently the areas recommended for Resilience Zones consist of a high concentration of vacant land.



Source: Sanctuary Farms



Sanctuary Farms Resilience Zone

Organizations like Sanctuary Farms have proven to be incredible stewards of vacant land in Greater Warren/Conner, activating spaces with food production, education, and small business incubation. What started as four vacant lots in 2021 is now eight plots used for urban farming and composting food waste, as well as air pollution tracking. Through the resilience zone concept, there is opportunity for Sanctuary Farms to further expand their operations and create community spaces like a playground, nature sanctuary, and outdoor event space.



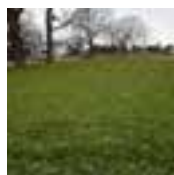
The future vision for the Resilience Zones incorporate a variety of improvements to use vacant land in a way that will benefit the surrounding neighborhood.



Neighborhood Stabilization Recommendations

Many of the Resilience Zone land use strategies have been adopted from the “Living with Land” report from the Gratiot-Seven Mile Neighborhood Framework Plan, [21] as they have already been tested and approved by the City of Detroit and the DLBA. However, there are additional strategies within this plan that go further due to the concentrated and large amounts of publicly-owned vacant land and begin to conceptualize how to use the vacant land to address significant air pollution concerns, provide access to large scale natural areas, improve habitat, reduce flooding, and provide unique infill residential opportunities, in conjunction with the desires of the residents to also support small scale green energy, urban farming, and community pathways.

CLEAN + GREEN



ADOPT-AN-EDGE



LOW MOW



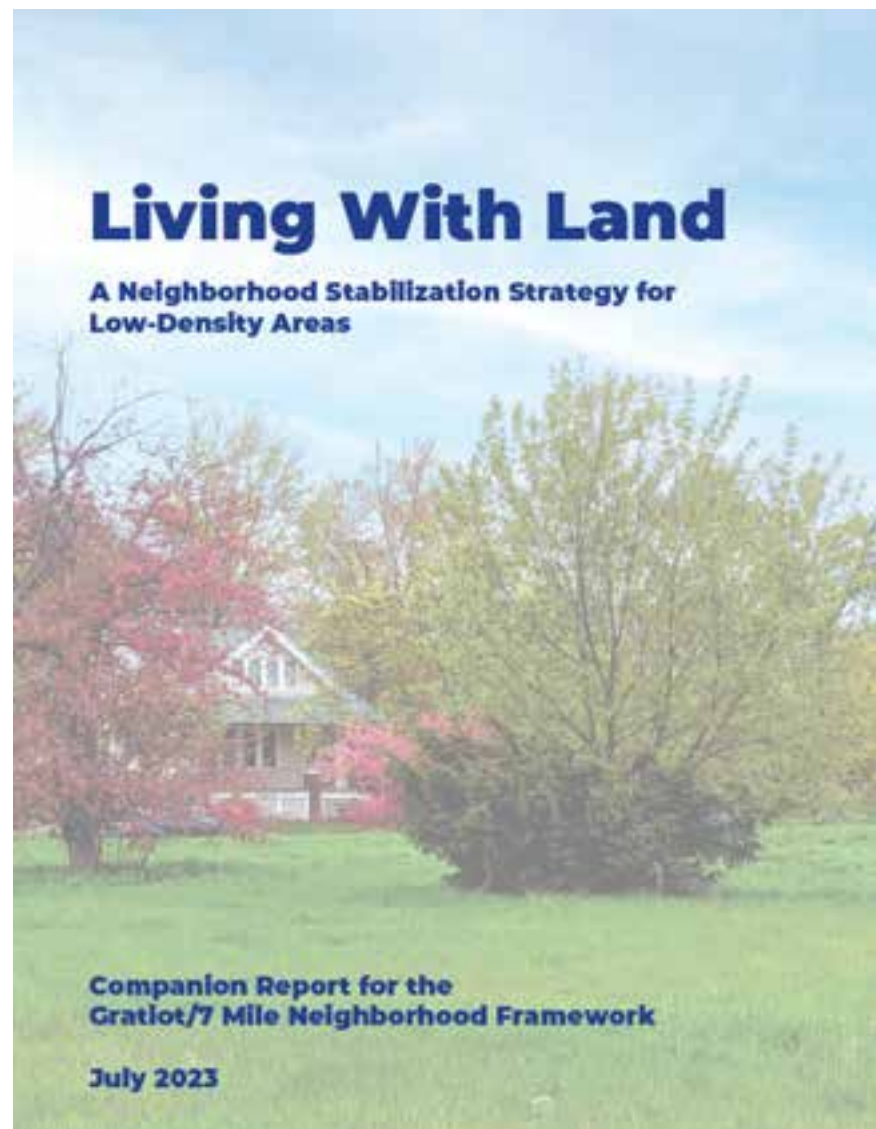
COMMUNITY OPEN SPACE



ECO+ LANDSCAPE

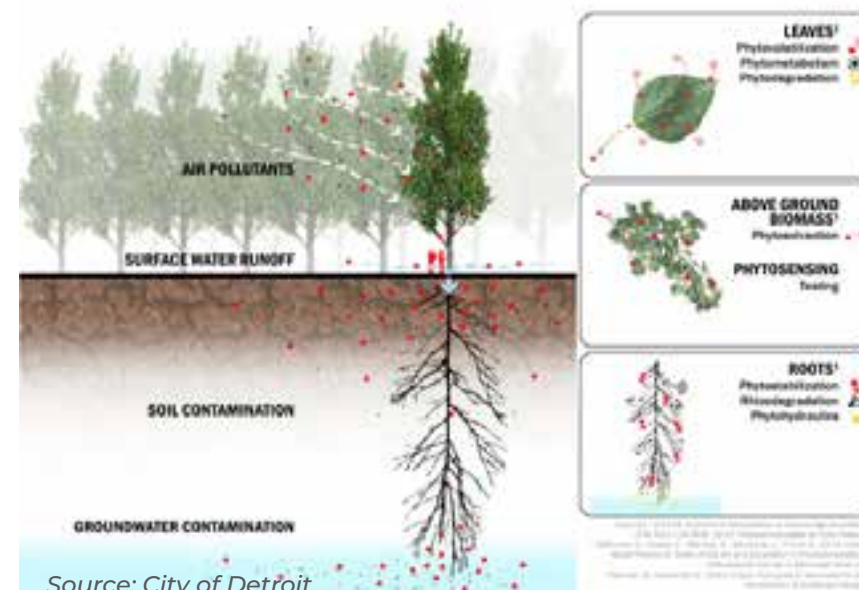


COMMUNITY PATHWAYS



Source: Prospect Park Alliance

PHYTOFOREST MECHANISM



Source: City of Detroit

URBAN FORESTS & CARBON CAPTURE

Open spaces are prime locations for the planting of urban forests. These can take the form of trees planted on private property, in pocket parks, as phytoremediation forests, and along streets, where suitable. Boosting the area’s overall tree canopy cover levels to 30%, aligning with state and SEMCOG recommendations, will help mitigate extreme heat, air pollution, and flooding in the neighborhood.

Carbon “sinks” are natural features that absorb more carbon from the atmosphere than they release. A mature tree can sequester up to 133 pounds of CO2 annually. [22] The numerous efforts around tree planting should be coordinated to create a roadmap of open land activation for tree planting, reforestation, and access to natural areas for eastside residents. This should include the selection of trees that will thrive in Michigan’s projected future climate and species that maximize the reduction of air pollution.

PHYTOREMEDIATION

Phytoremediation uses plants to clean up contaminated environments. In Delray, another industrialized Detroit neighborhood, tall canopy trees (up to 50’ in height) and hedgerow shrubs will be deployed to target the area’s health, pollution, and connectivity issues. The hedgerows act as filters that effectively lift wind up and dissipate it over a factor of 10 times the plant height. Every forest incorporates at least one evergreen species to increase year-round filtration and phytoremediation benefits.



Source: Chandler Park Conservancy

GREEN STORMWATER INFRASTRUCTURE

Landscapes designed to capture stormwater not only help prevent local flooding, but also have the potential to lower drainage fees for property owners. By installing small bioswales on publicly owned vacant land between homes, stormwater runoff from paved surfaces and buildings can be redirected and captured, while also adding a buffer between neighbors. In areas prone to flooding issues, strategically placed larger bioswales or rain gardens within neighborhoods can offer effective stormwater management solutions. While the initial construction cost and maintenance requirements for this open space activation model can be high, they decrease significantly within the first three years.



Source: Sanctuary Farms

URBAN FARMING

Urban farming extends beyond its role in food production and a vacant land activation strategy; it holds the potential to build community relationships, be community-led, acknowledge the expertise of residents, and adapt to the challenges posed by a changing climate. The City of Detroit has demonstrated its commitment to supporting urban farming practices, evident by the adoption of an urban agriculture ordinance in 2013 and the recruitment of a Director of Urban Agriculture in 2023. Progress is also being made on a livestock ordinance allowing Detroit residents to keep ducks, chickens, and honeybees on their property.



Source: Houston Chronicle

SMALL SCALE GREEN ENERGY

The increasing demand for small-scale green energy projects, particularly solar energy, is expected to continue with the declining costs of renewable energy installations. The City of Detroit prepared the Detroit Solar Toolkit [23] in 2021 to streamline the process for Detroit residents interested in adding solar systems to their property. There is a formal permitting process with the City that is separate from the online application with the utility provider, DTE Energy. Various financing options are available, including the Detroit 0% Interest Home Repair Loan, which can cover roof repairs and solar panel installation up to \$25,000.

In 2020, the State of Michigan replaced net metering with the Distributed Generation program for customers generating surplus electricity through renewable energy systems. Under this program, customers pay the regular retail rate for electricity delivered by the utility (DTE Energy) and receive a credit equivalent to the excess power they generate but don't use on-site. As of October 2019, over 5,000 customers in Michigan participated in the program, which sets standards for solar photovoltaic and wind turbines, limiting them to 550 kW and allowing them to produce up to 110% of a customer's annual electricity consumption. Proper interconnection with the utility company (DTE Energy) is a requirement for participating in the distributed generation program.



Source: Detroit Free Press



Source: City of Detroit

ACCESS TO NATURE

Nature is often a destination at the end of a car ride, but there is a valuable opportunity to transform open space in Greater Warren/Conner into beautiful and functional landscapes. As highlighted in SEMCOG's Access to Nature data and maps, [24] the east side of Detroit is lacking access to this amenity. Large-scale reforestation in Greater Warren/Conner as part of Resilience Zones could help provide natural areas to east side residents. Additionally, in collaboration with the Greening of Detroit and Detroit Outdoors, the City of Detroit initiated the smaller scale Nature Pockets Program, [25] with grant support from the National Parks and Recreation Association (NRPA). Designed to enhance access to nature for Detroit residents, this program has introduced Nature Pockets in seven parks across the city. These pockets feature additions such as expanded tree canopy, wildflower meadows, and rain gardens, which can be replicated into an open space activation model in Greater Warren/Conner.

COMMUNITY PATHWAYS

As new improvements are made to transit and through streetscape projects, parallel investment should also be made to transform open space into last-mile pedestrian and bicycle paths. Activating these open spaces with well-designed pathways not only enhances safety for pedestrians but also fosters connectivity, effectively stitching the neighborhood together. Adding small trails and pathways through the urban forests, migratory bird habitat, green stormwater infrastructure, and public spaces can help residents easily access nature near their homes, boosting both mental and physical health outcomes. This approach physically links activation strategies across different Focus Areas, creating a cohesive and integrated network of pedestrian amenities.



Source: Conner Creek Greenway DECC



Source: Safe Harbors Green

COMMUNITY OPEN SPACES

Activating open spaces within neighborhoods extends beyond physical development to create communal areas with social, health-oriented, or recreational purposes. Vacant land activation for community spaces eliminates blight and elevates neighborhood value and livability for residents. Most importantly, it establishes a deep sense of public ownership, where residents and local stakeholders actively engage in the stewardship of land within their communities and contribute to its management and use. To achieve this vision, community partners must enter into multi-year use agreements to maintain and activate vacant City-owned land with projects of their own design. This allows communities to create and manage land-based projects that are meaningful to them, without the commitments of permanently owning the site. Further, it presents an opportunity to expand land-based community open space projects incrementally across multiple phases and larger plots of land over time.

MIGRATORY BIRD HABITAT

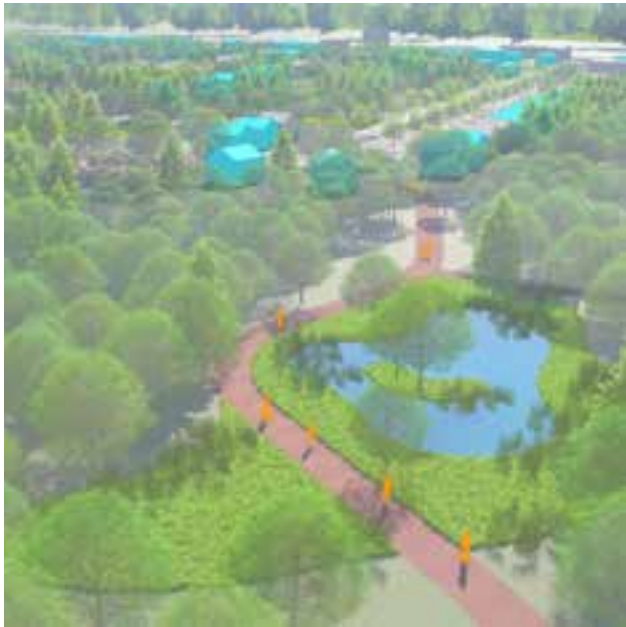
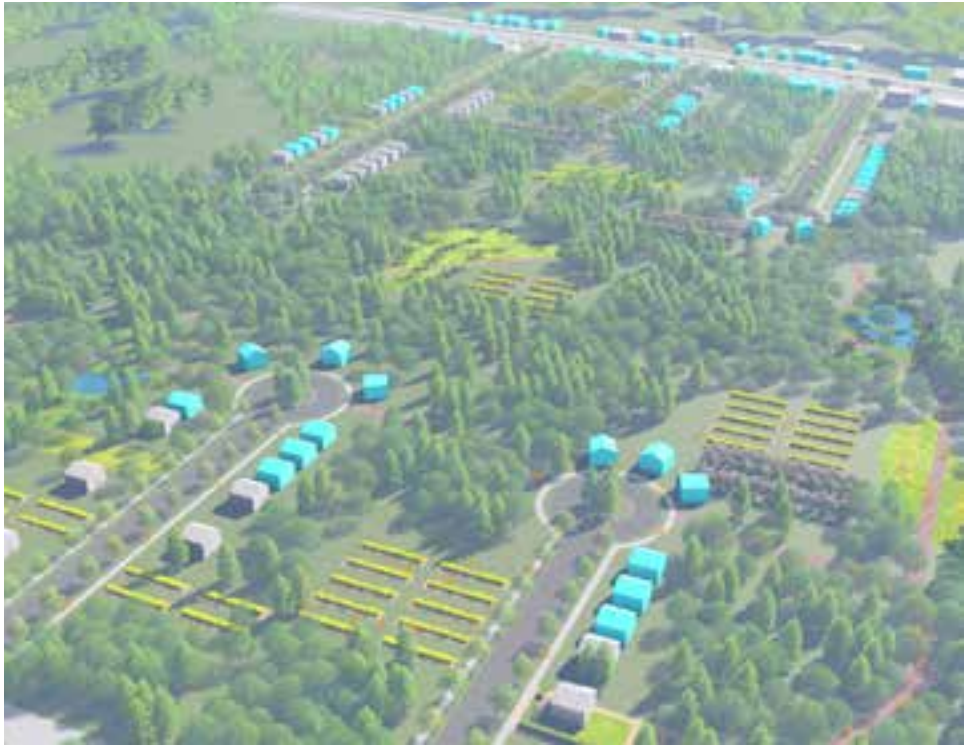
The Detroit River region has been identified as one of the 12 most important wetland regions across the Great Lakes that are most important to conserve or restore for vulnerable migratory birds. [26] After decades of urbanization and industrialization, the region has undergone a historic loss of migratory bird habitat, threatening the species that rely on them for resting, nesting, and feeding their young. Similar to the establishment of milkweed stands for migratory Monarch butterflies, the establishment of shrubs and trees specifically to create bird habitat is another land activation strategy that benefits goals for reforestation and green stormwater infrastructure.



Source: Detroit Bird Alliance



The vision of Resilience Zones in Greater Warren/Conner.





Commercial and Economic Development Recommendations

The story of retail and commercial spaces within the Greater Warren/Conner neighborhood mirrors housing market and population changes in Detroit. Once bustling with multiple commercial corridors meeting local needs, the area has experienced shifts due to population decline, evolving retail trends, online shopping, and economic factors. In Detroit and elsewhere, retail landscapes have transformed significantly since these neighborhoods were established. Instead of replicating extensive retail fronts, the emphasis should be on revitalizing neighborhood-focused commercial nodes, integrating essential services into a vibrant, walkable, and interconnected environment.

Within Greater/Warren Conner, commercial and economic development efforts will need to take a multi-faceted approach to bring new resident-focused services to the neighborhood. Future economic development will also need to rely on grassroots efforts to build activity at specific areas and attract people to support them. The approach for building local commercial and retail development should be coordinated with the following strategies:

Proximity to People – Commercial and retail businesses rely on people for support and should be located where there is a current density of residents, or where new residents may be anticipated.

Activate Publicly Owned Property – Property owners can be difficult to negotiate with, especially those located outside the region or state. Activating publicly owned property near each node can be a quick way to build activity around an area.

Start Small with Low-Cost, Low-Risk Activations – Farmers or artisan markets, food truck rallies, pop-up shopping events, and other quick activation strategies can be used to establish the location as a destination for shopping. Focus first on connecting local entrepreneurs with residents to help build businesses.

Coordinate Development and Investments at Key Nodes – Adjacent streetscape improvements, park enhancements, new residential development, and more should be focused near the commercial nodes so that the spillover effects from each investment are captured.

Focus Investments First in the Targeted Multi-family Housing Areas - The Targeted Multi-family Housing Areas (TMHA) are designated areas in the City where mixed-use and multi-family developments are encouraged. New investments in mixed-use buildings with retail or commercial spaces should be located within the Greater Warren/Conner TMHA.



Commercial and Economic Development Recommendations

The Greater/Warren Conner neighborhood contains a variety of locations that, with the right investment approach, could begin to re-establish local retail and commercial areas. The following locations and strategies should be deployed to attract a variety of customers, services, stores, and entrepreneurial spaces to the neighborhood and enhance the quality of life for residents, while keeping their dollars in the community.

- **Invest in the identified commercial nodes** to establish local retail and commercial areas with services and stores that enhance the quality of life for residents.
- **Implement activation efforts** first in primary nodes with existing business activity, then underutilized property areas, with food truck vendors, local markets, event programming, infrastructure upgrades, and landscaping enhancements.
- **Prepare commercial nodes to be ‘redevelopment ready’** through building demolition and rehabilitation, blight reduction, landscaping, and infrastructure improvements.
- **Re-zone former school properties** slated for demolition to facilitate residential and mixed-use development compatible with surrounding neighborhoods.
- **Support neighborhood retail opportunities**, such as at Jack’s Party Store, that are being pursued by local community groups to build activated nodes throughout the Greater Warren/Conner neighborhood.
- **Encourage mixed-use development** within the Targeted Multi-family Housing Area to build retail nodes and take advantage of support programs offered by the City of Detroit.

Business Support Programs

There are various programs and resources for businesses and developers in the City of Detroit to activate a commercial property. Below are a list of potential programs and tools to reignite commercial development in the Greater Warren/Conner community:

- **Detroit Economic Growth Corporation (DEGC)** is the City’s economic development catalyst that provides innovative programs to attract investment, create jobs, and advance Detroit’s economy. The DEGC has various loan funds for developers in Detroit to finance acquisitions, construction, redevelopment, fixtures and furniture, and equipment. The DEGC also serves as the intermediary for tax abatements developers may seek for a project.
- **Michigan Economic Development Corporation (MEDC)** assists Michigan businesses in their growth strategies to foster vibrant communities across the state. MEDC collaborates with more than 100 economic development partners, such as the DEGC, to provide programs and resources for developers and businesses.
- **The Detroit Land Bank Authority (DLBA)** markets commercial structures, lots, and residential buildings with two or more housing units. Applicants must adhere to the DLBA’s proposal guidelines and submit an offer to the appropriate broker on the listing.
- **Motor City Match** provides businesses with technical assistance and grant funding to launch a brick-and-mortar business in the City of Detroit.
- **Restore Track of Motor City Match** is a grant program through the Motor City Match program that provides facade grants to neighborhood businesses throughout the City. Grants cover 50% of the total project cost up to \$25,000.
- **The Detroit CDFI Coalition** is a collaborative group of community development financial institutions (CDFIs) and Detroit stakeholders that work together to strategically expand CDFI investment, loans, and banking services. The Coalition has created a summary table of resources and a CDFI Grid that is a helpful reference guide for navigating financial assistance for projects.
- **Hatch Detroit by TechTown** supports independent small businesses through funding, education, exposure, and mentoring. Hatch awards entrepreneurs with \$100,000 and in-kind services to open their brick-and-mortar business in Detroit, Highland Park, or Hamtramck.
- **ProsperUs Detroit** advances economic equity in under-resourced communities by supporting entrepreneurs with opportunities and capital needed to build businesses, generational wealth, and vibrant neighborhoods.
- **Ebiara** provides capital and resources to support the growth of Detroit’s Black and Brown-owned development firms as they work to grow and strengthen Detroit neighborhoods. Resources include low-cost loans and customized technical assistance solutions to support development firms.



Commercial and Economic Development Recommendations

Commercial Nodes

Within Greater Warren/Conner, there are hubs of increased commercial activity that are prime locations for future expansion. These areas contain a higher concentration of active businesses, are located along major transportation routes, and are near higher density residential areas, ensuring a customer base. Four intersections have been pinpointed for immediate and long-term attention regarding local economic growth strategies. These intersections are categorized as Primary Commercial Nodes and Secondary Commercial Nodes.



Source: OHM Advisors



Commercial and Economic Development Recommendations

Primary Commercial Nodes

Primary Commercial Nodes

The Primary Commercial Nodes are the areas with an existing density of active businesses, available retail spaces, nearby employment or residential activity, and adjacent future development plans. These areas are the best opportunity for near-term economic development to support quality of life investments in Greater Warren/Conner. It is important to note that given the current market conditions, public or philanthropic assistance will likely be needed to support the introduction of new commercial development at each node.

Conner Street at E. Warren Avenue

Conner Street at E. Warren Avenue represents the greatest opportunity for new neighborhood commercial activity given the current concentration of existing businesses, adjacent transportation routes, and nearby employment centers. Currently, there are several “big box” stores, fast food chains, social services, and local businesses near the intersection which creates a centralized area for neighborhood shopping.

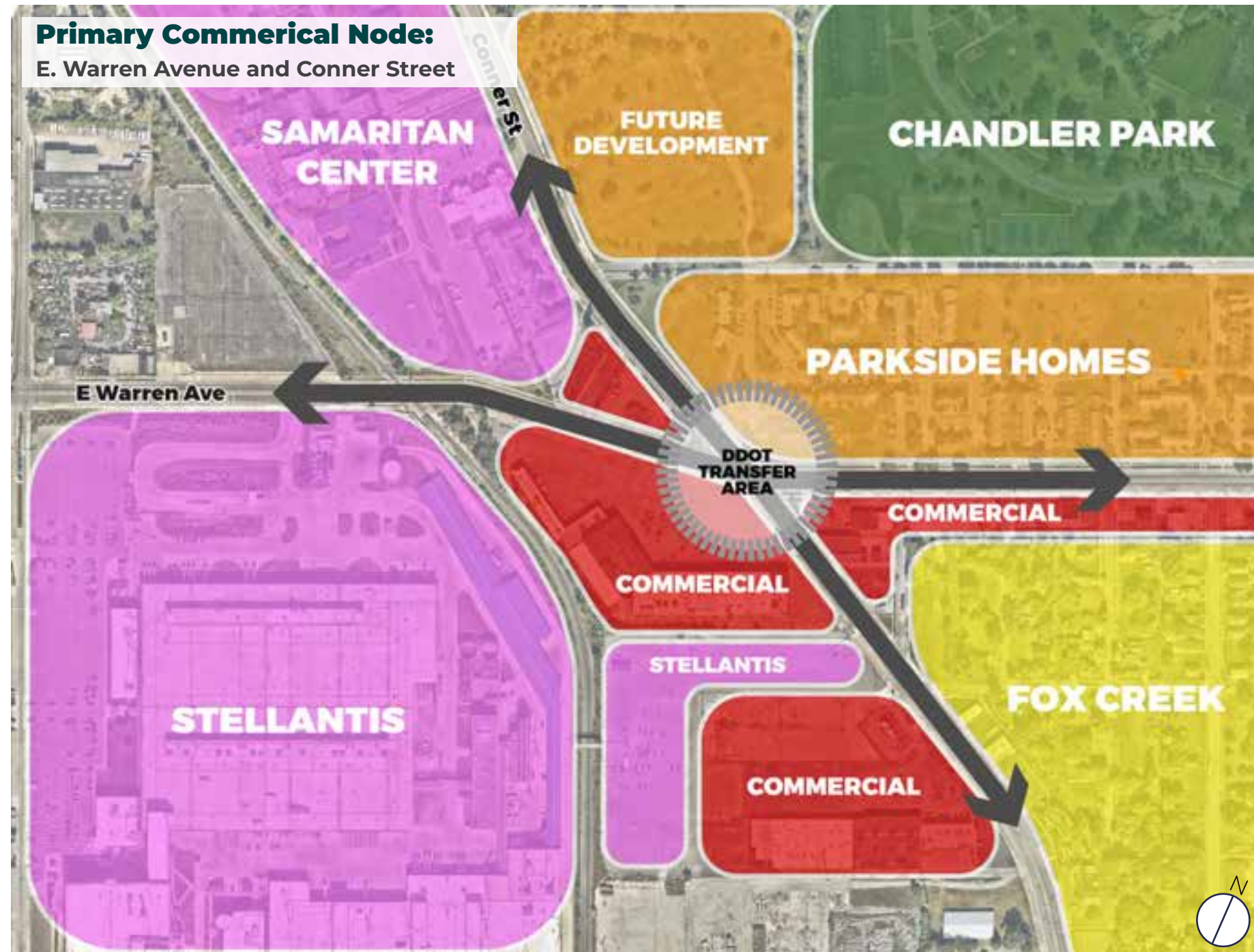
The intersection is located near several large employers, including Stellantis, the Samaritan Center, Wayne County Community College, and Eastside Community Network, whose employees and visitors may benefit from additional retail amenities in the neighborhood. However, the land use is auto-oriented with large building setbacks, overbuilt parking areas, and no landscaping. Many of the buildings are aging and in need of beautification to increase the curb appeal. The underutilized parking areas provide opportunities for community activation, add public spaces to the corridors, introduce landscaping, and improve pedestrian safety and comfort to the streets.

Both E. Warren Avenue and Conner Street are major transportation routes carrying roughly 10,000 and 20,000 vehicles per day, respectively. Additionally, three DDOT bus routes pass through this intersection, including Route 8 - Warren Avenue which is a ConnectTen route operating 24/7. These two heavy transportation routes result in over 30,000 prospective customers passing through the intersection each day.

Activation Strategies

The large lots of the surrounding properties have underutilized space including a shopping center, fast food restaurants, and single-story commercial buildings. For instance, the shopping center building is roughly 115,000 square feet, however the lot size is 360,000 square feet and primarily devoted to parking. Parcel sizes in the area range from 11,000-360,000 square feet, and building sizes range from 1,000-116,000 square feet. Most of the commercial properties in this area are under private ownership, and would require the City of Detroit or other community partners to implement strategies in collaboration with the owners such as:

- Provide incentives for exterior enhancements through facade grants
- Improve infrastructure that allows people to easily access businesses, such as lighting, sidewalks, roads, and paths
- Catalyze commercial nodes using “pop-up” retail with food truck vendors, artist displays, and local markets





Commercial and Economic Development Recommendations

Primary Commercial Nodes

Mack Avenue at Alter Road

Mack Avenue at Alter Road is home to an established commercial hub (the Mack Alter Square Grocery and Shopping Center). There is also existing residential density through the adjacent Morningside neighborhood, active development plans for a boutique hotel and housing, and streetscape plans for Mack Avenue. Building off of the existing commercial activity at Mack Alter Square, efforts to bring additional commercial activity to the surrounding area would be mutually beneficial to both residential development efforts and existing businesses. Most of the structures in the area are either single-story commercial buildings or two-story mixed-use buildings. There is a mixture of private ownership and City-owned parcels.

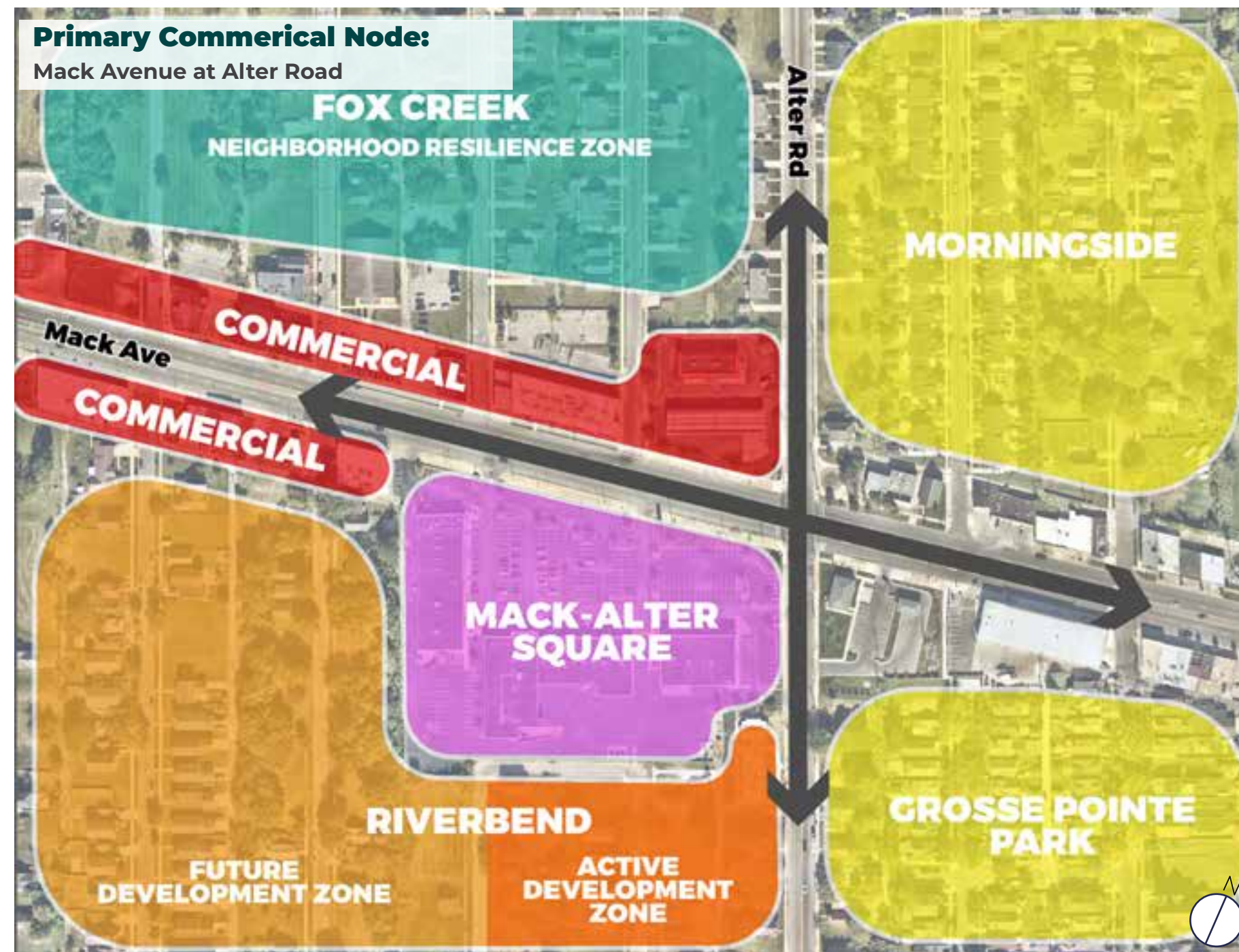
A Targeted Multi-family Housing Area is located in south of Mack Avenue between Alter Road and Lakeview Street. Housing projects in the targeted multi-family housing areas are prioritized to receive support from the Detroit Housing for the Future Fund (DHFF) and other housing development financing and subsidy programs. Additional multi-family housing near the Mack Avenue commercial node supports the development of a walkable urban node while adding both affordable and market rate housing to the neighborhood.

There are five City-owned commercial parcels near or within the node, none of which have structures. A pilot project to activate the publicly-owned lots at 14445 and 14500 Mack Avenue is recommended, given the lot sizes, corner location, and adjacency to an active business. A near-term activation strategy is to set up a pilot neighborhood-centered business that features a market, locally-owned eatery, coffee shop, or other neighborhood service as a proof of concept.

At the same time, active businesses in the area should be targeted for facade improvement grants and connected with developers through programs such as Motor City Match, that provide grant and technical assistance resources to build new construction or rehab the properties. Following a successful activation of a new retail space, the City could then market and sell the remaining publicly-owned lots to local developers or business owners that will further enhance the corridor and meet the needs of the community.

Primary Commerical Node:

Mack Avenue at Alter Road





Commercial and Economic Development Recommendations

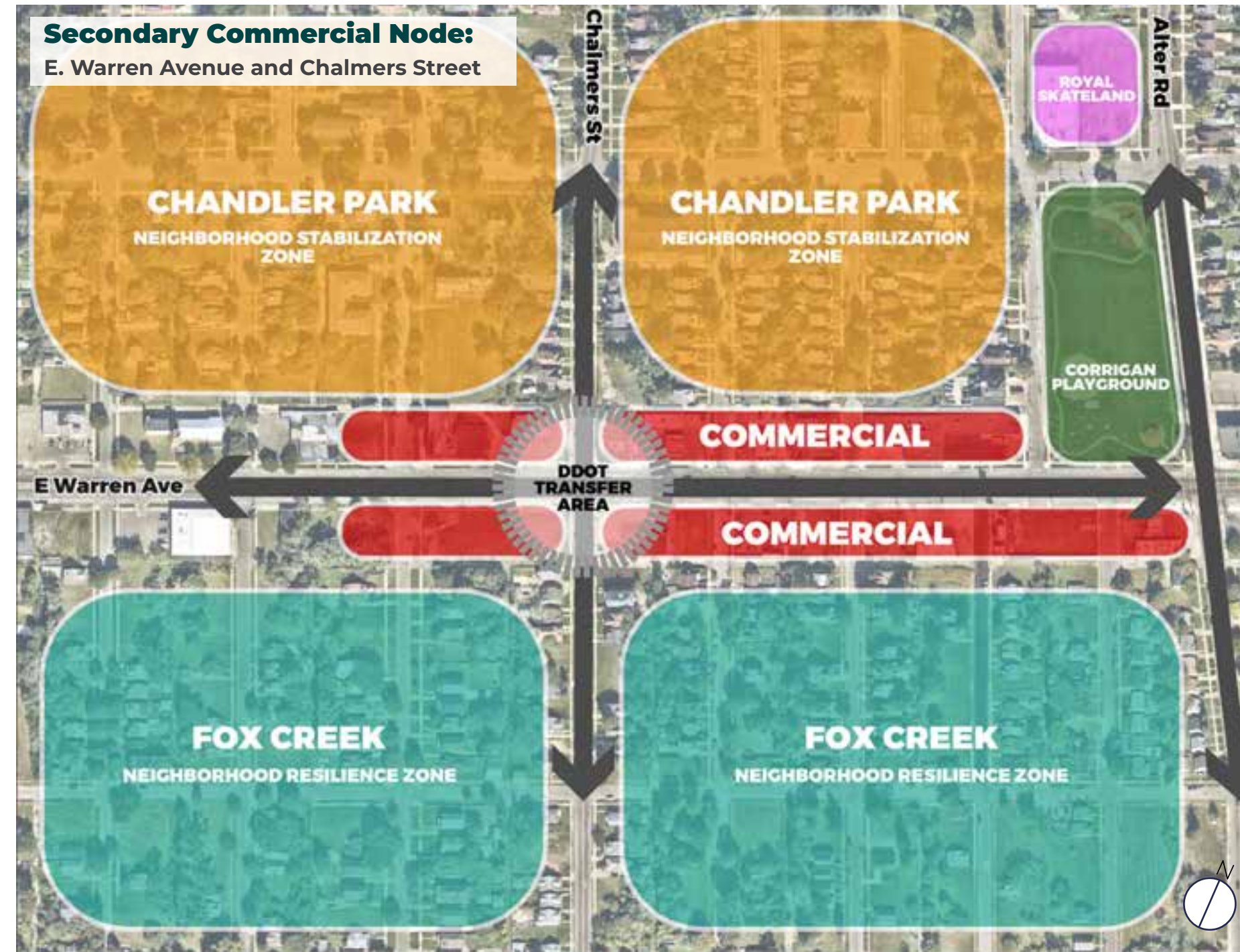
Secondary Commercial Nodes

There are also a few key areas within the Greater Warren/Conner neighborhood that have the long-term potential to serve as smaller neighborhood-focused commercial areas. These secondary commercial nodes would cater more toward the surrounding neighborhood with everyday goods and services. Secondary nodes would include barber shops, hardware stores, coffee shops, restaurants, neighborhood markets, and more. Additionally, these locations have less existing activity, but with future investments in residential development, street infrastructure, and parks, they could serve as a future hub for the neighborhood.

E. Warren Avenue at Chalmers Street

Commercial development at this node should be neighborhood focused to cater to the residents of the Chandler Park-Chalmers neighborhood, which has been identified as a key zone for neighborhood stabilization. Additionally, E. Warren Avenue is a major transportation corridor that has thousands of transit riders and vehicle trips per day.

Six parcels in this area are owned by the City of Detroit (one of which has a structure on it), and 26 are privately owned. The parcel sizes vary from roughly 1,990 square feet to 19,000 square feet. There is opportunity for single-story and two-story mixed-use developments along this commercial node. The city should clean and market the sites it owns, prioritizing developers that will provide amenities to further enhance the quality of life for residents in the community.





Commercial and Economic Development Recommendations

Secondary Commercial Nodes

Shoemaker Street at French Road

Of the four commercial development nodes, the Shoemaker Street and French Road intersection is the least developed and currently surrounded by the fewest residents. However, the Planning and Development Department (PDD) and the Detroit Land Bank Authority (DLBA) own roughly 50 parcels along Shoemaker Street. There is abundant opportunity to stabilize and activate this corridor to provide neighborhood services and retail, increase wages and opportunities for residents, and rebuild the local economy with a focus on equity. Given the economic realities of this portion of the neighborhood, this is likely a long-term development opportunity.

However, the following actions can be taken in the short-term to establish the corridor as redevelopment ready:

- Demolish unsalvageable City-owned structures
- Reduce blight and stabilize salvageable city-owned structures
- Pilot landscape treatments and community activated spaces in open spaces
- Rehabilitate and repurpose city-owned buildings for new uses
- Target infill development at Montclair Street, French Road, Cadillac Avenue, Bewick Street
- Prioritize residential rehabs and infill development near the French Road intersection
- Direct commercial development along Shoemaker to overlap with Safe Streets for All improvements





Commercial and Economic Development Recommendations

Commercial Nodes: Activation Strategies

To enhance commercial development in the Greater Warren/Conner neighborhood, the City should focus on first revitalizing the two primary commercial nodes, building off of the existing commercial activity and development in the area. A longer-term approach should be applied to the secondary commercial nodes, coordinating commercial investment with neighborhood stabilization strategies. Private property owners with active businesses in these corridors should receive facade grants to enhance their exteriors, and there should be widespread updates to lighting, sidewalks, and roads to increase accessibility by foot, bike, transit, or car. The City should clear the parcels it owns of any blight, work to activate large vacant sites with pop up retail, and list and solicit RFPs for development. This comprehensive strategy will help vibrant commercial nodes return to the neighborhood, but only if done in a coordinated way.

Citywide, there are a number of public, private, and philanthropic entities helping Detroiters start and grow their business through the activation of neighborhood commercial spaces.

Farmers Markets

Pop-up markets, such as the East Warren Farmers Market, can provide a recurring dedicated space for local makers and farmers to sell their products. The East Warren Development Corporation (EWDC) owns a former Pizza Hut restaurant and now hosts 30+ local vendors and food trucks in weekly farmers markets and special events. Through this initiative, earnings of \$60,000 went back into the pockets of the 83

predominantly local, minority, and women-owned small businesses who participated in 2021. This was a 240% increase from the initial market launch in 2020. Previously a farmer's market was operated at the Mack Alter Square shopping center from 2011 to 2013.

Recommended Locations:

- Mack Avenue/Alter Road Primary Node
- E. Warren Avenue/Chalmers Street Secondary Node

Micro Retail Pop-up

To support entrepreneurs who may benefit from a small brick and mortar space, micro retail spaces may be an opportunity. Recently, a micro retail space constructed from shipping containers was installed on Dexter Avenue and Tyler Street, south of the Davison Freeway. This shipping container complex will be a temporary venue for pop-up retailers, local art, and food trucks. The project was conceived by the City of Detroit to help revitalize Dexter Avenue, the main thoroughfare between the Russell Woods, Nardin Park, and Dexter-Linwood neighborhoods.

Recommended Locations:

- E. Warren Avenue/Conner Street Primary Node

Food and Retail Truck Space

In addition to the existing regulations outlined in the City of Detroit's Food Truck Ordinance, it's worth noting that changes to zoning policies may be necessary to support the activation of commercial corridors like Greater Warren/Conner area with food truck operations. In Morningside, for example, the East Warren Development Corporation (EWDC) hosts Food Truck Fridays during the summer months, activating the public space with food trucks. While street vendors can operate in public right-of-way with the required licenses and permits, food trucks may face location restrictions and other regulations under the City of Detroit's Food Truck Ordinance. Therefore, exploring potential adjustments to zoning policies could facilitate the integration of food trucks into Greater Warren/Conner's commercial corridors, allowing for similar activation of public spaces and economic opportunities.

Recommended Locations:

- All nodes

Temporary Beautification and Activation Strategy

Better Block and other activation strategies can be used to show the potential for reinvestment in underutilized spaces through low cost, temporary design solutions. A temporary beautification and activation strategy could be successful at any of the key commercial nodes and would focus on bringing pop-up businesses, temporary street changes, planters and landscaping, seating, and activities to build excitement about long term activation. Better Block events from around the county have resulted in the transformation of neighborhood commercial corridors. The Gratiot-7 Mile community recently held a Better Block event to showcase the intersection of Gratiot Avenue and McNichols.

Recommended Locations

- All Nodes



Source: Second Wave Media



Source: City of Detroit



Source: Detroit Metro Times



Commercial and Economic Development Recommendations

Development of Large Publicly-Owned Parcels

Within Greater Warren/Conner, there are five publicly-owned vacant schools scheduled for demolition as part of the City’s Blight to Beauty initiative. These schools are the Andrew Jackson/Ronald McNair School (3970 Marlborough), Carstens Elementary (2550 Coplin Street) Chandler Elementary (9227 Chapin Street), Hosmer Elementary (4365 Newport Street) and Hutchinson Elementary (5220 French Road). There is the opportunity to activate these parcels in the future with uses that are accessible, inclusive, and community-oriented, to rebuild the social infrastructure lost with the closing and removal of the schools. However, a multi-pronged approach will be needed to align site and market conditions to encourage redevelopment, keep development affordable, and incorporate much-needed community spaces.

Development Recommendations

The neighborhood school properties are located in the middle of primarily residential areas and surrounded by single-family home parcels. They are currently zoned R2 (Two-Family Residential District), which allows single-family detached or two-family dwellings by right, and up to eight units of a multi-family dwelling or townhouse with conditional approval, and a number of public, civic, and institutional uses with conditional approval. These school properties are roughly three to five acres each, which is too large for the development allowed within R2 zoning.

In order to allow for development that matches the sizes of these sites, new zoning categories are recommended to align the sites with market conditions, neighborhood context, and developer expectations. Alternatively, the existing parcels could be subdivided to encourage a development pattern that more closely matches the existing

neighborhood context of single or two-family homes, or small multi-family dwellings, however this may limit the response from developers of creative mixed-use developments.

These sites may also be used to add new parks and public space to the community to fill the existing gaps in the park network. The Parks and Recreation Strategic Plan (PRSP) identifies areas of the Fox Creek and Riverbend neighborhoods as locations for new parks within Greater Warren/Conner. Both the Hosmer Elementary and Andrew Jackson/ Ronald McNair School sites could be used to fill that gap.

The following table summarizes the limitations of the current zoning, and potential rezoning changes.



Hutchinson Elementary School (5220 French Street)



Hosmer Elementary School (4365 Newport Street)



Andrew Jackson/Ronald McNair School (3970 Marlborough)



Chandler Elementary School (9227 Chapin Street)



Carstens Elementary School (2550 Coplin Street)

Potential Development Tools

Affordable housing development funding tools can be used to increase and incentivize development in Multi-family Housing Target Areas around the City of Detroit.

Affordable Housing Leverage Fund:

This fund provides affordable housing developers and owners with streamlined access to financial tools that are specifically designed to address housing challenges in Detroit neighborhoods. It encourages the preservation of regulated and naturally occurring affordable housing throughout the City of Detroit and the development of new mixed-income and affordable housing in targeted multi-family housing areas.

Low-income Housing Tax Credits:

This program allows investors in affordable rental housing to claim a credit against their tax liability annually for a period of 10 years. These tax credits have been used in neighborhoods throughout the city, including the Preserves on Ash in North Corktown.

Detroit Housing for the Future Fund:

This is a new fund comprised of private investment that will be invested into affordable housing development and preservation in Detroit. It is designed to deploy private grant and low interest loan capital to complement and leverage public investment through the City of Detroit and Michigan State Housing Development Authority as well as tax credits and other existing affordable housing finance tools. The Detroit Housing for the Future Fund (DHFF) encourages the following:

- The development of new mixed-income and affordable housing in targeted multi-family housing areas
- The preservation of regulated and naturally occurring affordable housing throughout the City of Detroit



Source: Outlier Media

Considering the misalignment of the previous school parcels in size, zoning, and market conditions, change is needed to promote redevelopment, especially needed affordable housing redevelopment. It is recommended that the Housing and Revitalization Department (HRD) release a Request for Proposals for development of the property, with zoning and/or parcel modifications, and use programs or financing tools to encourage affordable housing and mixed-income housing.

School/ Large Structures	Area (Acres)	Surrounding Parcels Zoning	Surrounding Neighborhood Assets	Current Zoning	Potential New Zoning Category
Hutchinson Elementary School at 5220 French Road	3.7	R2	Little Detroit Community Garden, Parousia Healing Ministries	R2 (Two-Family Residential) By-right residential uses: <ul style="list-style-type: none">• Single-family detached• Two-family dwelling	Rezone to R5 (Medium Density Residential) By-right residential uses: <ul style="list-style-type: none">• Assisted living facility• Multiple-family dwelling• Townhouse (suggested use)
Carstens at 2550 Coplin Street	4.9	R1, R2, and B4	John S Vitale Community Center, Wolverine Center, Eastbank Spirits (Distillery), Evangel Church of God & Christ, Lakewood Manor Apartments, St. John Community Center	R2 (Two-Family Residential) By-right residential uses: <ul style="list-style-type: none">• Single-family detached• Two-family dwelling	Rezone to R6 (High Density Residential) By-right residential uses: <ul style="list-style-type: none">• Multiple Family Dwelling• Public, civic, and institutional uses• Library• Neighborhood center
Chandler Elementary School at 9227 Chapin Street	2.8	R2	Brighter Detroit Community Center, Gratiot Woods Co-op Apartments, Nativity of Our Lord Church, Catherine C Blackwell Institute	R2 (Two-Family Residential) By-right residential uses: <ul style="list-style-type: none">• Single-family detached• Two-family dwelling	Rezone to R5 or R6 By-right residential uses: <ul style="list-style-type: none">• Townhouses• Assisted living facility• Multiple-family dwelling
McNair School at 3970 Marlborough Street	3.6	R2	Mack-Alter Square, MASH Small Business Hub, Maison’s Fine Foods, Redeem Church of God in Christ	R2 (Two-Family Residential) By-right residential uses: <ul style="list-style-type: none">• Single-family detached• Two-family dwelling	Retain R2 zoning or rezone to R5 / R6, depending on proposal
Hosmer Elementary School at 4365 Newport Street	3.5	R2	Mack-Alter Square, MASH Small Business Hub, Maison’s Fine Foods, Redeem Church of God in Christ	R2 (Two-Family Residential) By-right residential uses: <ul style="list-style-type: none">• Single-family detached• Two-family dwelling	Retain R2 zoning or rezone to R5 / R6, depending on proposal



Mobility and Streets Recommendations

Streets and roads are critical pieces of infrastructure in any community. They not only provide opportunities for travel and connections, both within and outside of the neighborhood, but are also vast public spaces that provide opportunities for local businesses, community interaction, and beautification. Improvements to the street network within Greater Warren/Conner should be done in conjunction with the recommendations set forth in the Streets For People Design Guide to improve safety and connectivity for all users including transit, pedestrians, bicyclists, vehicles, and trucks.

The amount and scale of streets within Greater Warren/Conner allow ample opportunity to support the improved quality of life of residents. As many of the streets slated for future street improvements are also those with the potential for future commercial development, it is important to coordinate future investments to infrastructure in places that support the development of residential, retail, and open space. The reasoning for this is twofold. First, street renovations signal a commitment to property owners that the area is worth investing in and the improvements are intended to bring more people to the area. Additionally, the recommended improvements focus on improving safety for all roadway users, pedestrians, cyclists, transit users, and motorists, which is an essential aspect of creating active streets in a community. The mobility and streets recommendations intend to address the larger mobility concerns in the Neighborhood to make it easier, safer, and comfortable for residents to walk, bike, take transit, and drive.



Mobility and Streets Recommendations

The mobility and streets recommendations intend to build off of the dedicated funding sources and projects that have been committed to the Greater Warren/Conner neighborhoods. The City's Safe Streets for All implementation grants, I-94 Modernization project, and Iron Belle Trail route all provide opportunities to reframe how residents move through their neighborhood.

- **Support Safe Streets for All (SS4A) initiatives** and pursue funding to fill gaps in improvements.
- **Support pedestrian safety, accessibility, and connectivity** throughout the area.
 - Work with MDOT to **apply safety improvements to I-94's** bridges and service drive.
- **Plant green buffers** of native vegetation for sound and pollution reduction on publicly-owned properties along the highway.
- **Reduce highway through-traffic** on residential streets.
 - Implement a **new Chandler Park Drive design** to create a street that acts as both a connection through the park and as part of the park itself.
- **Explore the potential for a mobility hub** at the intersection of Conner Street and E. Warren Avenue to facilitate transit ridership.
- Coordinate **bus stop improvements with DDOT** to include benches, bus stop signs, and route information.
- Collaborate with city departments, including DPW and DPD, and collaborate with companies operating large vehicles to **reduce the impacts of truck routes** through the community with industrial buffering standards and enforcement along roadways.



Mobility and Streets Recommendations

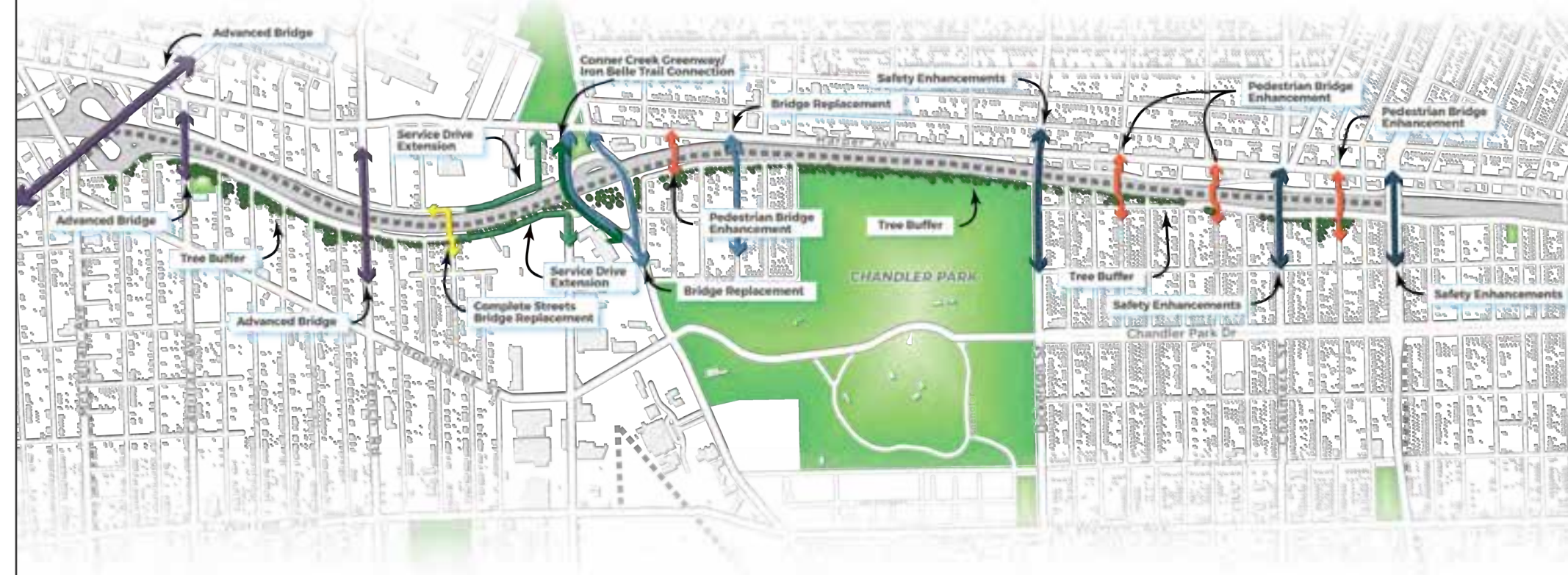
I-94 Safety and Buffering

I-94 is the northern boundary of Greater Warren/Conner. While the interstate provides easy access to other parts of the City and region for residents, it also cuts off the neighborhood from those to the north, contributes to noise and air pollution in the area, and brings speeding vehicles into the neighborhood, particularly at the highway exit ramps. As much of the neighborhood that borders I-94 is residential, it is necessary to introduce ways to improve traffic safety and reduce spillover pollution into the Greater Warren/Conner neighborhood.

Concerns of traffic along residential roads in the Greater Warren/Conner community come from the I-94 exit ramps and service drives. As many residential streets tie into the service drives and are accessible from highway exit ramps, safety improvements to local streets should be added to protect more vulnerable street users. Enhanced safety, beautification, noise, and pollution reduction measures could include:

- High-visibility crosswalk and pedestrian crossing signage at Cadillac Avenue
- Close ramps at French Road*
- Extension of Service Drive from Springfield Street to St. Jean*
- Pedestrian safety improvements along Coplin and Macolm Streets
- High-visibility crosswalk and pedestrian crossing signage at Chalmers Road
- High-visibility crosswalk and pedestrian crossing signage at Outer Drive/Alter Road
- Apply advanced bridge features to Gratiot Avenue, Cadillac Avenue, and French Road crossings*
- Redesign and replace Conner Street overpass*
- Add new Conner Creek Greenway/Iron Belle Trail crossing over I-94*
- Apply advanced bridge features to bridge at Barrett Avenue and Dickerson Road to make crossing to Chandler Park more comfortable
- Add complete street bridge crossing at Lemay Street*
- Enhance pedestrian bridges at Malcom Street, Coplin Street, Newport Street, and Philip Street.
- Plant green buffers consisting of native vegetation for sound and pollution reduction along I-94 and the adjacent service drives using publicly-owned properties

** Included in Plan for I-94 Modernization Project*



Planned Improvements for I-94 through the Greater Warren/Conner study area identified in the I-94 Modernization Project and this Framework Plan

Highway Buffering

Though highways like I-94 made urban expansion possible, their construction was devastating to the neighborhoods they displaced and continue to have lasting effects on the people who live around them. Heavy vehicle traffic is a key contributor to the high particulate matter, low air quality index, and ambient noise in the Greater Warren/Conner neighborhood. There are homes located within 100 feet of I-94, while elevated levels of harmful air pollutants can persist as much as 1,000 feet or more from the road edge. A 100-foot vegetative buffer can reduce ambient noise by five to eight decibels and reduce particulate pollution by 40% (although many factors will influence pollutant removal).

The passage of the federal Inflation Reduction Act (IRA) and the Bipartisan Infrastructure Law (BIL) marks the largest climate investment in history and provides grant funding aimed at offering financial and

technical assistance to communities marginalized by underinvestment and overburdened by pollution. Tree planning and edge treatments should be targeted along the north edge of the Neighborhood, and funded using BIL and IRA grants.

Implementation

- Partner with the Detroit Tree Equity Partnership to plant street trees and additional canopy trees and shrubs suitable for filtering particulate matter and reducing air pollution in DLBA-owned parcels along the Service Drive.
- Work with neighborhood groups and residents to implement “adopt-an edge” or “Eco+ landscape” treatments on DLBA-owned parcels adjacent to I-94. [21]



Mobility and Streets Recommendations

Reducing Traffic on Residential Streets

Many drivers exiting I-94 are directed to use French Road and other residential corridors to access the larger streets in the neighborhood, which can be a safety hazard to residents. MDOT plans to eliminate the French Road exit, which may reduce through traffic, but access from the Edsel E. Ford Service Drive should be discouraged rather than eliminated, utilizing the following techniques that also employ a community centric approach to their design.

Neighborhood Roundabouts



Intersection Narrowing



Chicanes



Additionally, safety along the service drives could be enhanced, particularly at highway crossings with non-motorized connections. Although the Service Drive has a speed limit of 25 mph, the wide lanes encourage high vehicle speeds and lead to greater crash occurrences. NACTO's guidelines recommend a minimum lane width of 12 feet for high-speed and high-volume roadways. The City should work with MDOT to include the following design considerations to the Edsel E. Ford Service Drive for the I-94 Modernization project:

- Lane width reductions
- Clear lane striping
- Signalized left turns
- Crosswalks, pedestrian countdown signals, and raised intersections
- Strategically placed lighting at exit ramps
- Connected sidewalks
- Bicycle infrastructure where appropriate

One way to reduce the traffic burden on neighborhood streets is to connect the existing I-94 Service Drive on the west side of the neighborhood to Conner Street. By directing motorists using the Service Drive to Conner Street with a new connection and appropriate signage, this improvement can reduce the amount of traffic using neighborhood streets to access destinations within the neighborhood. MDOT has already recommended this as part of the I-94 Modernization Project and should be supported in future design stages.



Midblock Crossings



High Visibility Crosswalks



Dedicated Bicycle Infrastructure

Signage



Speed Humps



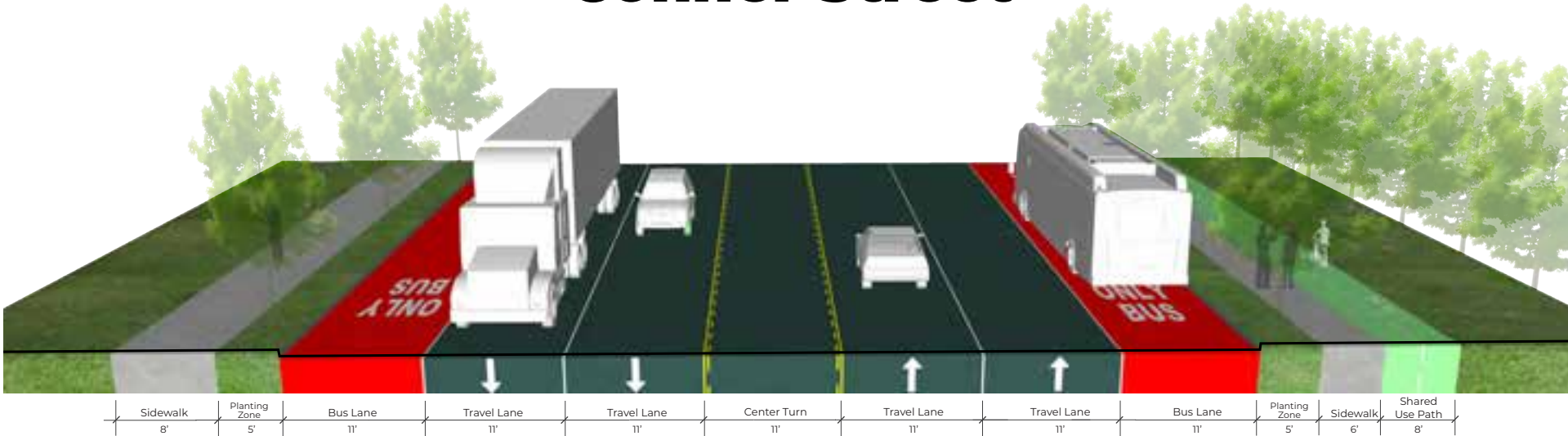
SAFE STREETS FOR ALL (SS4A) Corridors

The City of Detroit has roughly \$50 million to spend on improving safety on streets around the city. Safe Streets for All (SS4A) projects are aimed at reducing pedestrian deaths and making road conditions safer through design elements, lighting, and technology. Three corridors within Greater Warren/Conner are slated for safety improvements funded by the grant. Additional Safe Streets for All implementation grants may also be available in the future and could be used to implement the safety and mobility improvements recommended for the planning area.

Conner Street

Conner Street is a major north-south Arterial Commercial and Industrial Road in the community with considerable truck traffic within a 70’-80’ cross-section. SS4A improvements are slated for I-94 to E. Warren Avenue and are recommended to include the addition of dedicated outside running bus lanes, street trees, and enhanced transit stops. A focus on pedestrian safety is needed including marked and signalized crosswalks and mid-block crossings.

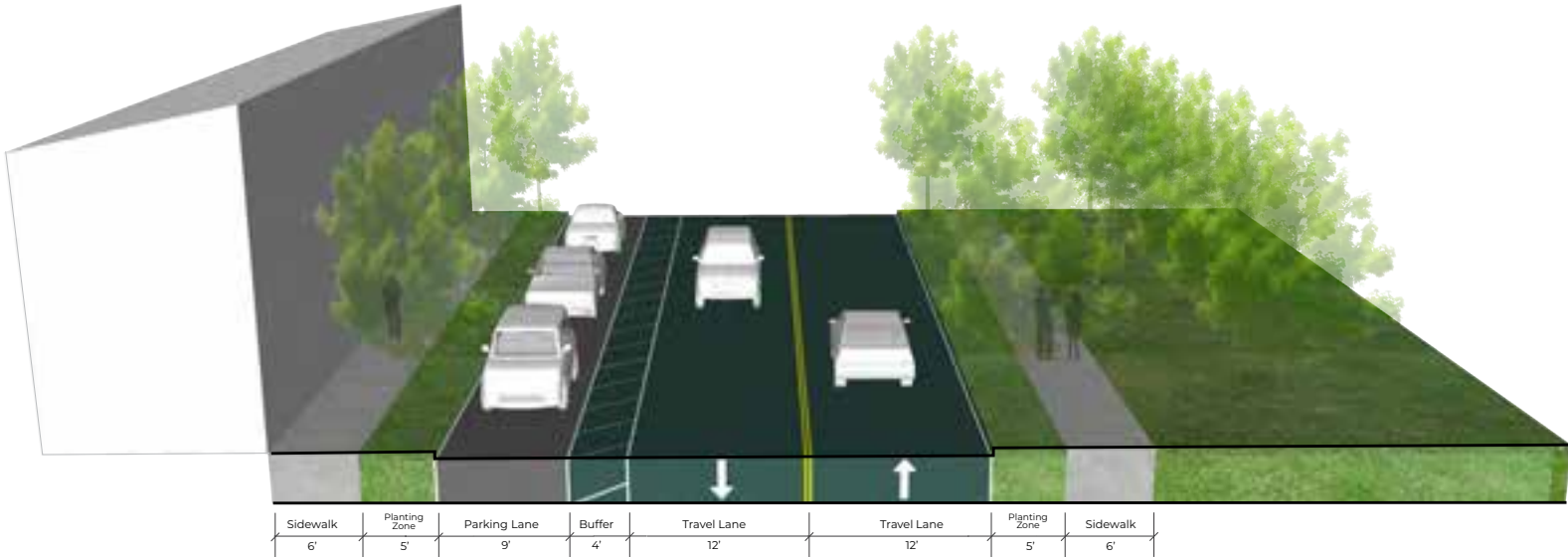
Conner Street



Shoemaker Street

Shoemaker Street connects Gratiot Avenue with Conner Street and the center of the neighborhood, however, few of the former commercial buildings are still left. Given the extra space on the street and the need for slower vehicle speeds, the recommendation for SS4A funding is to add dedicated, striped on-street parking areas, reduce the width of the lanes to 12’, and add street trees throughout.

Shoemaker Street





Mobility and Streets Recommendations

Neighborhood Connectivity

Outside of the SS4A corridors, there are streets within the Greater Warren/Conner area that would benefit from additional safety and non-motorized connectivity improvements to improve resident mobility throughout the neighborhood.

Alter/Outer Drive is an Arterial Residential Street that should support high volumes of people across all modes with a non-motorized focus. Sections of Alter Road have seen sidewalk improvements in recent years. This should continue along with treatments to slow the vehicular traffic, improve intersection crossings, and better accommodate cyclists.

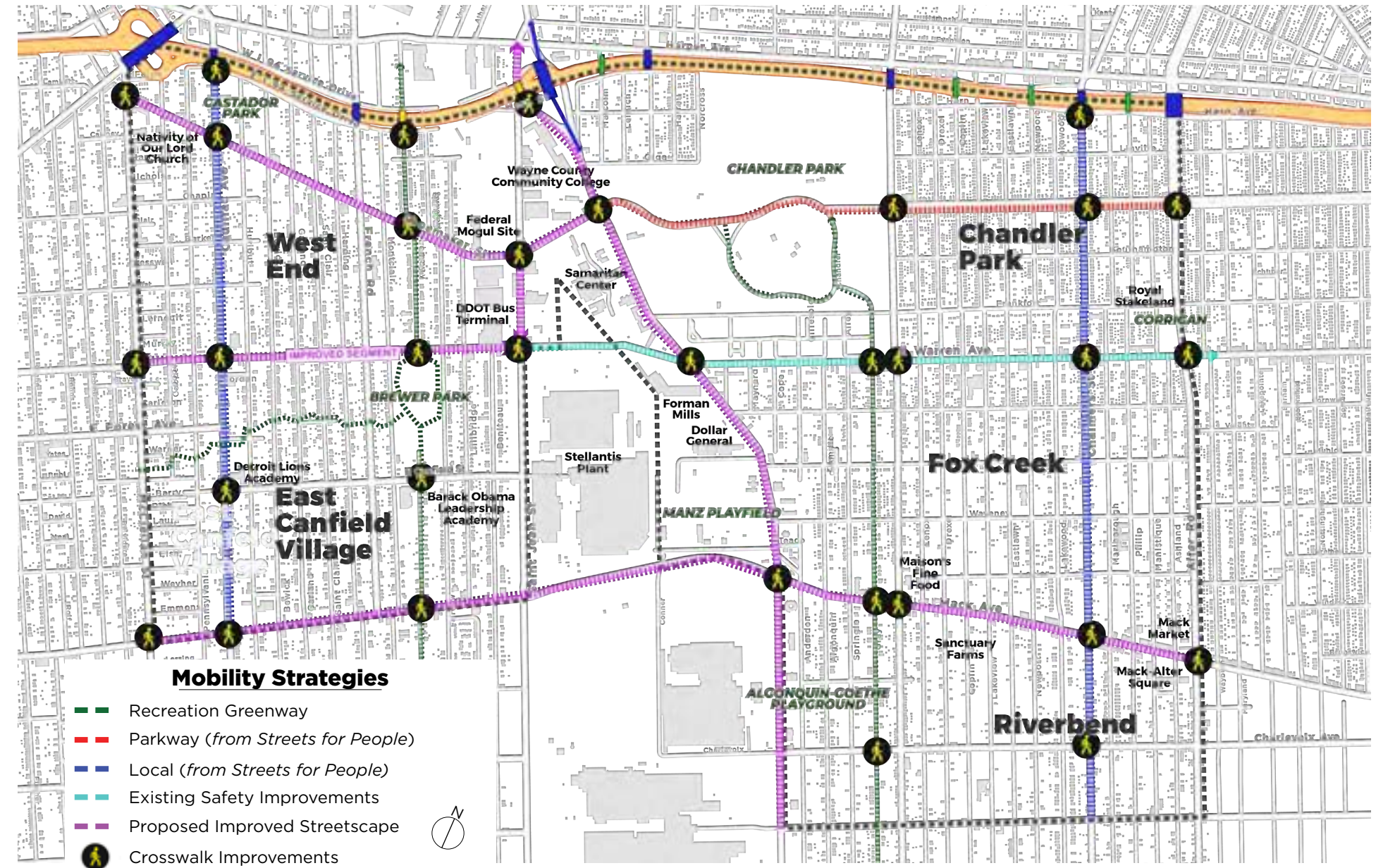
Chalmers Street, McClellan Avenue, Dickerson Avenue, and Cadillac Boulevard are Local Streets that serve to make up a large part of the pedestrian and bicycle network in predominately residential areas. These streets have high property access but should prioritize slow vehicular speeds through traffic calming such as curb extensions and raised and/or marked crosswalks.

Lemay Street is a future priority neighborhood north/south pedestrian corridor through the neighborhood, providing connectivity to the proposed Lemay Street bridge over I-94, Brewer Park, the Iron Belle Trail, Canfield Art Park, and the Barack Obama Leadership Academy. Treatments should be prioritized to slow vehicular speeds, improve sidewalk conditions, plant street trees, and improve crossings.

Gray Street is recommended as a north-south neighborhood greenway and potential alternate route for the Conner Creek Greenway that directly connects Chandler Park and the Detroit Riverfront. The corridor is residential in nature with some blocks having sections of vacant land. In the short-term, there is opportunity for a signed ped/bike route, improved intersection crossings, and pavement and sidewalk repairs. In the long-term, depending on its use, there may be potential to split the street with one half dedicated to one-way vehicle traffic and other a dedicated non-motorized path separated by curbs and bollards.

Greater Warren/Conner suffers from large barriers along the edges and in the middle of the neighborhood that make walking and biking uncomfortable and dangerous. A key aspect of improving the mobility network in the Neighborhood is to establish a hierarchy of streets that connect the various neighborhoods to commercial areas, parks, schools, and other community amenities. The recommendations for non-motorized connectivity build from the solutions identified in the City's Streets for People Plan and specify treatments to each corridor based on its intended use.

Improvements to a network of streets would interact with each other and improve pedestrian and bicyclist connectivity. Throughout the neighborhood, the design of bicycle and pedestrian improvements should be elevated along the Iron Belle Trail route as it travels along E. Warren Avenue, St. Jean Street, Shoemaker Street, and Conner Street.



Chandler Park Drive

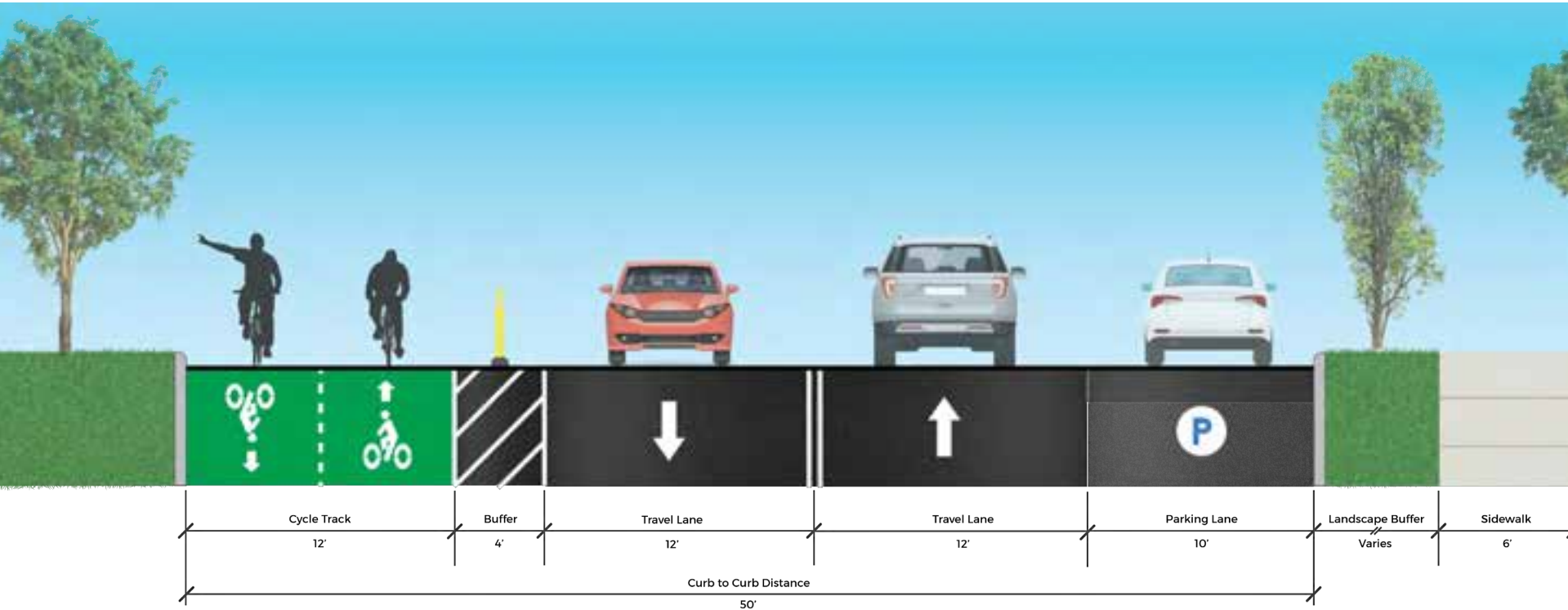
Chandler Park Drive is Chandler Park’s main access road and serves as an east-west connector linking to Balduck Park. It is designated as a “Parkway” in the Streets for People Guidebook, yet it lacks the off-street trails or on-street facilities to support biking and walking and safe travel through the park. The portion through the park has a 50’ cross section and is currently lacking clear delineation between driving lanes, parking areas, and non-motorized lanes.

A new design for Chandler Park Drive from Conner Street to Dickerson Avenue is proposed to create a street that acts both as a connection through the park and as part of the park itself. Chandler Park is a regional destination for visitors from around the city and parking is needed for many users. DDOT buses may be rerouted to serve Chandler Park in the future, requiring space for transit operations. Finally, many users access the park on foot or by bike, requiring the street to also be

part of the recreational infrastructure of the park.

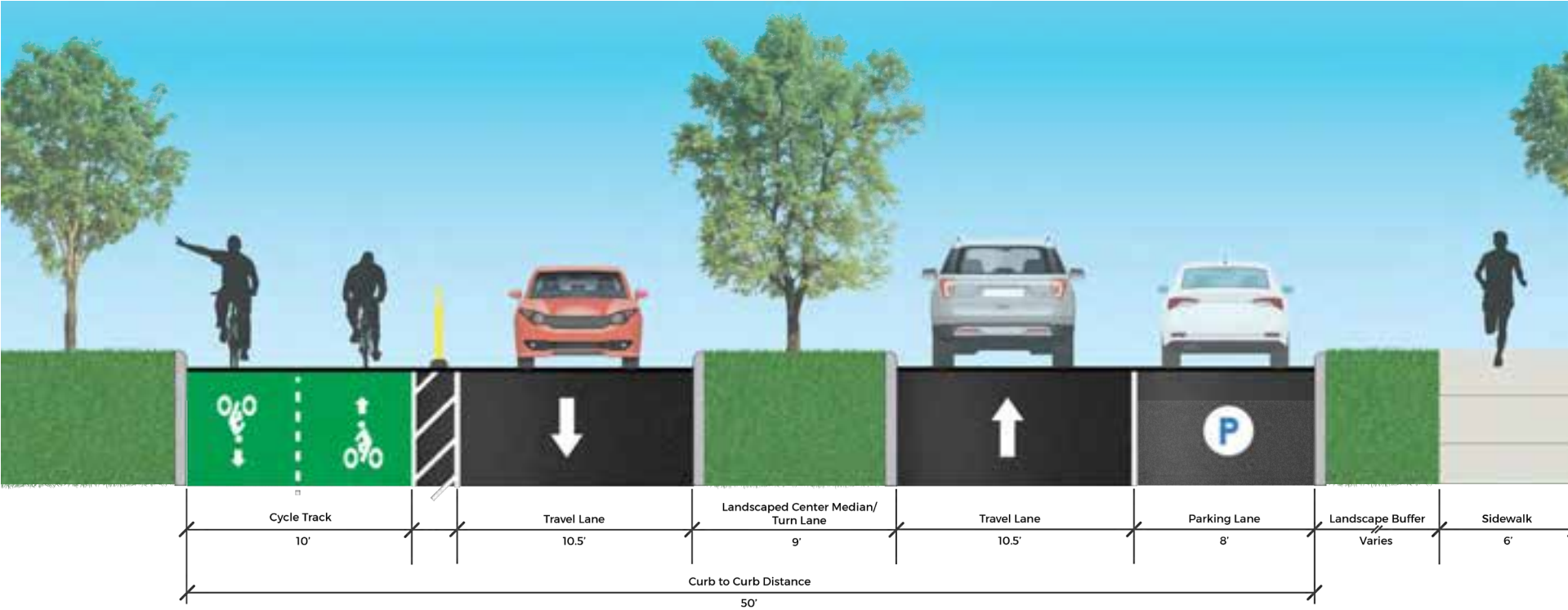
Two potential roadway design options were developed that could be implemented within the current right-of-way available along Chandler Park Drive.

Option 1 uses paint and flexible bollards to better delineate space within the current right-of-way of Chandler Park Drive. Given the roughly 50’ available from curb to curb, there is ample space to maintain vehicle operations, add on-street parking, incorporate transit stops, and add protected on-street bike lanes to Chandler Park Drive. The concept shown is a lower cost option and could be constructed with paint and flexible plastic bollards. The option includes two vehicle lanes, a striped on-street parking lane on the south side of the street, and bike lanes with a four-foot buffer and bollards.



Option 2 is a more intensive option that would add a landscaped center median to Chandler Park Drive and on-street parking to the south side of the street. The center median will serve double duty and help to slow cars to the 25-mph speed limit, while also adding more street trees and reducing the amount of concrete in the park. This is a more costly improvement due to the addition of the median, but may be a more desired long-term solution for the aesthetics of the park.

At the final community meeting, residents preferred the Option 2 design for Chandler Park Drive, however, this is a more costly option to implement and may not be feasible given the available construction budget.





Mobility and Streets Recommendations

DDOT Bus Stop Enhancements

DDOT transit service is an essential service within the City of Detroit, and particularly in the Greater Warren/Conner Neighborhood, as a larger than average proportion of residents do not have access to a private vehicle. Some of the higher ridership routes in the DDOT system provide service to this community: Route 8 - Warren, Route 31 - Mack, and Route 13 - Conner.

The intersection of E. Warren Avenue and Conner Street marks the intersection of three bus routes in the neighborhood; Route 8 - Warren, Route 31 - Mack, and Route 11 - Clairmount. Given the likelihood of rider transfers in this area, a more comfortable, visible, and substantial space for riders to wait is recommended for the intersection. This 'mobility hub' would consist of amenities and infrastructure to make multi-modal trips easier and encourage more riders to use DDOT. Additionally, the intersections of routes in other areas of the neighborhood would benefit from stop improvements to help with transfers between routes. A mobility hub is any DDOT stop in which transit intentionally connects to other modes of transportation. These are intended to improve first and last mile connections and could include bikeshare, scooter parking, Uber/Lyft dropoff points, and commuter parking areas.

Conner/Warren Mobility Hub

The intersection of Conner Street and E. Warren Avenue is the busiest in Greater Warren/Conner, both from the vehicular side and transit side. Warren Avenue is a major crosstown street that connects the east and west sides of the City through the greater downtown area. Route 8 - Warren also travels along this street and is one of DDOT's ConnectTen routes which operate 24/7 on 15-minute or greater frequency. Route 13 - Conner and Route 11 - Clairmount also pass through this intersection and provide connections north and south to other parts of the City.

The mobility hub at Conner Street and Warren Avenue is intended to foster additional transportation connections for the surrounding neighborhood. This should be the main transit stop in the neighborhood and provide space for multi-modal connections for those coming from farther away. The mobility hub should feature safe and comfortable waiting areas with shelters, benches, trash cans, and signage. It should also have secure bike lockers available for riders to use, as well as a space for vehicle pick-ups and drop-offs. Finally, spaces for buses to queue outside of traffic and let passengers safely board should also be considered. All of these amenities are intended to work together to make transit easier, safer, and more comfortable.

Other Stop Improvements

Throughout Greater Warren/Conner, there are six DDOT routes that help carry residents to their daily destinations. The intersections where transfers can occur are recommended to be upgraded to include a higher level of rider amenities (see map on page 50). These locations should include, at a minimum, a bus bench, bus stop sign, and route information for both routes. The highest ridership transfer stops could include a shelter, real time arrival information sign, and a visible designation that the stop is a transfer area.

It is important to note that bus amenities are subject to DDOT service standards and available right of way. Route numbers, destinations, and stop ID numbers are being rolled out gradually across the city. Real-time arrival signs are being piloted along select routes. Additionally, Detroit Public Works is typically responsible for installing bus benches as part of streetscape projects. The two departments should work together to enhance bus stops throughout Greater Warren/Conner.



Example of a mobility hub that co-locates transit, non-motorized, and vehicle infrastructure to facilitate multi-modal trips through the neighborhood.



Truck Routes

Greater Warren/Conner experiences a high level of semi-truck traffic serving the Stellantis manufacturing plants and surrounding industries. Most of this traffic is directed to the truck routes in the area: Conner Street, Mack Avenue, and a small portion of E. Warren Avenue. However, input from the community suggests that trucks are using other roads to access the auto plants, other areas of the city, or avoid congestion on the busier roadways. The increased volume of trucks over the years has adverse impacts on residents through increased noise pollution, air quality impacts, traffic congestion, and roadway safety.

To address the impacts of trucks in the Greater Warren/Conner planning area, the City should adopt a truck route ordinance to further clarify and enforce truck routes in the area. This ordinance will need to be supported with increased enforcement to ensure trucks are utilizing the correct routes.

To further combat the pollution attributed from trucks traveling through the area, the City of Detroit’s industrial buffering treatments should be applied to the segments of Conner Street, Mack Avenue, and E. Warren Avenue where trucks are directed. The industrial buffering standards include recommendations to increase tree planting within roadway buffers, add sustainable landscape designs to strategic areas along the corridors, and ensure that vehicle access is maintained to properties along truck routes. Additional strategies to reduce truck traffic on local streets include “No Thru Truck” signage on residential roads, police enforcement, and traffic calming measures such as chicanes and mini roundabouts. The goal with these strategies is to reduce the noise and air pollution that infiltrates into the adjacent neighborhoods, keep trucks on the main truck routes, and beautify the corridors.



Trucks on Mack Service Road near the Stellantis Plant. Source: OHM Advisors



Parks and Open Space Recommendations

The Parks and Open Space strategies will extend beyond the existence and maintenance of parks, to integrate gathering spaces into the everyday, nearby experience for residents in Greater Warren/Conner.

- **Preserve park land** for recreational purposes and open space through zoning changes or grants
- Implement the short-term, passive improvements to Brewer Park to **activate the park and preserve space for future uses**.
- Implement the long-term **amenity and infrastructure improvements to Brewer Park** as identified in a future park master plan.
- **Enhance neighborhood connectivity and accessibility** through a trail system, including the community-proposed Canfield Connect trail.
- **Ensure safe passage across Warren Avenue** and improve connectivity to the neighborhoods north of Brewer Park.
- **Strategically enhance Iron Belle Trail visibility** on Warren Avenue and **Conner Creek Greenway** on Conner Street through safety features, signage, and recreation amenities.

Preserve Parkland

The parks in the City of Detroit are often not zoned as such, putting them at risk. The Parks and Recreation (PR) District zoning preserves publicly-owned lands more than four acres in size. Due to the size criteria, few parks in the City are zoned as a PR District. The 2019 zoning update recommends that all parks and recreation facilities be rezoned as PR-Parks and Recreation, the standard zoning for Parks and Open Space in the City. None of the parks in the Greater Warren/Conner neighborhood are zoned as PR. Brewer Park, Chandler Park, Castador Park, Corrigan Park, and Algonquin-Goethe Park are zoned R2, Manz Playfield is zoned B4. Residents in Greater Warren/Conner will have a greater sense of security that their parks will remain parks if they are zoned as such.

An alternative solution is to pursue conservation grants that are tied to land preservation, such as the Michigan Natural Resources Trust Fund (MNTRF), or the Land and Water Conservation Fund (LWCF). Fifty parks to date in Detroit have made use of these funding sources. [27]



Algonquin-Goethe Park. Source: Homes.com



Castador Park. Source: OHM Advisors



Parks and Open Space Recommendations

Brewer Park Improvements

Overview

Brewer Park currently sits below the former Joy Middle School site on Fairview Street, between East Warren Avenue and Canfield Street. Today, the 10.7 acre vacant school site is not distinguishable from the existing 8.2 acre park.

The neighborhood surrounding Brewer Park, with leadership from the Canfield Consortium, has advocated for many years for improvements to Brewer Park. Advocacy has included a resident survey and petition submitted to the City in 2020 as well as hosting the Sidewalk Detroit Festival. Those recommendations are also supported by the input and strategies proposed by this Framework Plan. In addition to their work developing the East Canfield Art Park, south of Brewer Park, on E. Canfield Street, the organization is working on a “Remediation Forest” Project which will provide a natural connection between the passive recommendations for the south end of Brewer Park and contribute toward the overall sustainability goals of the plan.

Improvements to Brewer Park are expected as part of Phase 4 of the Parks & Recreation Strategic Plan (2022-2032). This site fills the park gap for the west side of the Greater Warren / Conner study area. To maximize the potential of both the former school and park parcels, further investigation into the development potential of the former Joy Middle School site should be explored for market viability. To enhance Brewer Park as an asset for the community, a portion of the former Joy Middle School site is recommended to be transferred to the General Services Department. Additionally, PDD supports the dedication of DLBA owned land south of Brewer Park to complete the Remediation Forest project.

Additional community engagement must be done to define a longer-term vision for these parcels and to understand any potential, compatible development opportunities for the former Joy Middle School site.



Brewer Park Improvements

Overall Recommendations

- Transfer land from PDD-owned parcel to GSD for expansion of Brewer Park. Amount of land to be determined.
- Engage community and seek partnership opportunities to align a long-term vision for these parcels and to understand any potential compatible development opportunities for the former Joy Middle School site.
- Develop a looping, non-motorized trail system within the park with access to the neighborhood. The trail should connect to the neighborhoods east and west of the park and to the East Canfield Art Park.
- Create links and views to and from the Iron Belle Trail along E. Warren Avenue, including a safe crossing from E. Warren Avenue to the neighborhood north of Brewer Park.

Near Term Initiatives

Near-term objectives for Brewer Park will focus on an initial phase of passive and low-maintenance improvements, based on available funding and resources. These would include:

- Installation of park signage along E. Warren Avenue.
- Removal of remaining debris plus adding topsoil and seeding former school site and related infrastructure to establish an open play and event area.

- Planting native trees in naturalized groupings with bird meadows and bioswales focusing on improving air quality, reducing flooding, and increasing tree canopy and biodiversity.
- Seek opportunities and partnerships to extend the urban remediation forest concept in the southern end of the park, coordinating with the work being initiated by Canfield Consortium/Sidewalk Detroit at the East Canfield Art Park.

Longer Term Initiatives

Future strategies for Brewer Park and the former Joy Middle School site require continued collaboration and engagement with the community. Initiatives within this phase should focus on developing a detailed plan for activated zones in the park and exploring potential development opportunities, including:

- Adding amenities such as garbage cans, drinking water fountains, Wi-Fi, and art installations
- Designating activated spaces in the park for play and gathering, e.g. pavilions and picnic tables, basketball courts, playground & exercise equipment, seating, and swinging benches
- Evaluating potential development opportunities for the former Joy Middle School site, including mixed-used and housing infill.



Parks and Open Space Recommendations

Brewer Park Improvements

- A** Ensure safe crosswalks (bumpouts, rapid flashing beacon, etc.) are installed on E. Warren Avenue during street improvements.
- B** Remove any remaining debris and add topsoil and seed in areas on north side.
- C** Hold space for future activated areas to support neighborhood gathering.
- D** Hold space for future activated area. Orient features to sightline from Canfield Connect, Warren, and Canfield entries.
- E** Add native trees in naturalized groupings and an understory planting of native perennials.
- F** East Canfield Art Park.
- G** Expand upon the urban remediation forest concept in coordination with Canfield Consortium and Sidewalk Detroit at the East Canfield Art Park.
- H** Hold space for future activated areas to support neighborhood gathering.
- I** Hold space for future activated areas including basketball, exercise, play, and other features.
- J** Keep sidewalk adjacent to street and bring new internal pathway into the park.



Brewer Park. Source: OHM Advisors





Climate Resilience and Health Equity Recommendations

The Climate Resilience and Health Equity recommendations support the need for additional community safeguards from air pollution, environmental disasters, and the cooperative use of vacant land.

- **Invest in existing community buildings** to provide resilience hub services.
- Support and **expand urban farming practices**.
- Dedicate staff and funding to support **flood mitigation initiatives**, including the dedication of runoff storage space and green stormwater infrastructure.
- **Pursue federal, state, and local grants** to monitor and reduce indoor and outdoor air pollution.
- **Grow the tree canopy** along streets and on publicly owned property, using species suitable for filtering particulate matter and reducing air pollution.



Flooding on the Eastside of Detroit. Source: Planet Detroit

Resilience Hubs

Resilience Hubs can help neighborhood stability by providing a third place for neighbors to connect, share resources, and build community. Many residents are affected by a lack of broadband access, flooding, air quality concerns, power outages, and other climate related issues. Resilience hubs are intended to connect residents with resources and services in times of emergency and non-emergencies. These spaces will also be equipped with solar and battery storage so residents can shelter at hubs during a crisis like a power outage, flood, or heat wave.

In Jefferson Chalmers, \$5 million from the Strategic Neighborhood Fund (SNF), \$2 million from the American Rescue Plan Act (ARPA), and \$3.9 million from City funds have provided residents with a community center facility where residents can access during times of emergency, such as flooding, power outages, or severe weather. This model can be replicated in other neighborhoods, including Greater Warren/Conner to help enhance the lives of nearby residents.

One way to use existing infrastructure for resilience hubs is to repurpose community-identified buildings that would qualify as adaptive reuse buildings or rehabbing land bank-owned houses. Another consideration would be to rezone or add a use for various areas that could be considered neighborhood non-profit or community centers.



ECN Stoudamire Wellness Hub. Source: Planet Detroit



Community Center at A.B. Ford Park. Source: City of Detroit

Existing Nearby Resilience Hubs	Organization	Neighborhoods
Stoudamire Wellness Hub	Eastside Community Network	Eastside Detroit
Bailey Park	Bailey Park Neighborhood Development Corporation	McDougall-Hunt
AB Ford Community Center	City of Detroit	Jefferson Chalmers
Brilliant Detroit Chandler Park House	Brilliant Detroit	Chandler Park

Resilience Hubs

Collaboration

The non-profit partners managing resilience hubs should also receive training in a cohort model to learn how to successfully develop and run a resilience hub network. Effective communication between hubs and neighborhood residents is also essential for this model to succeed. The following groups can build on their past collaboration and experience by convening to establish a framework for the buildout of a resilience hub network in Greater Warren/Conner.

Features of Resilience Hubs

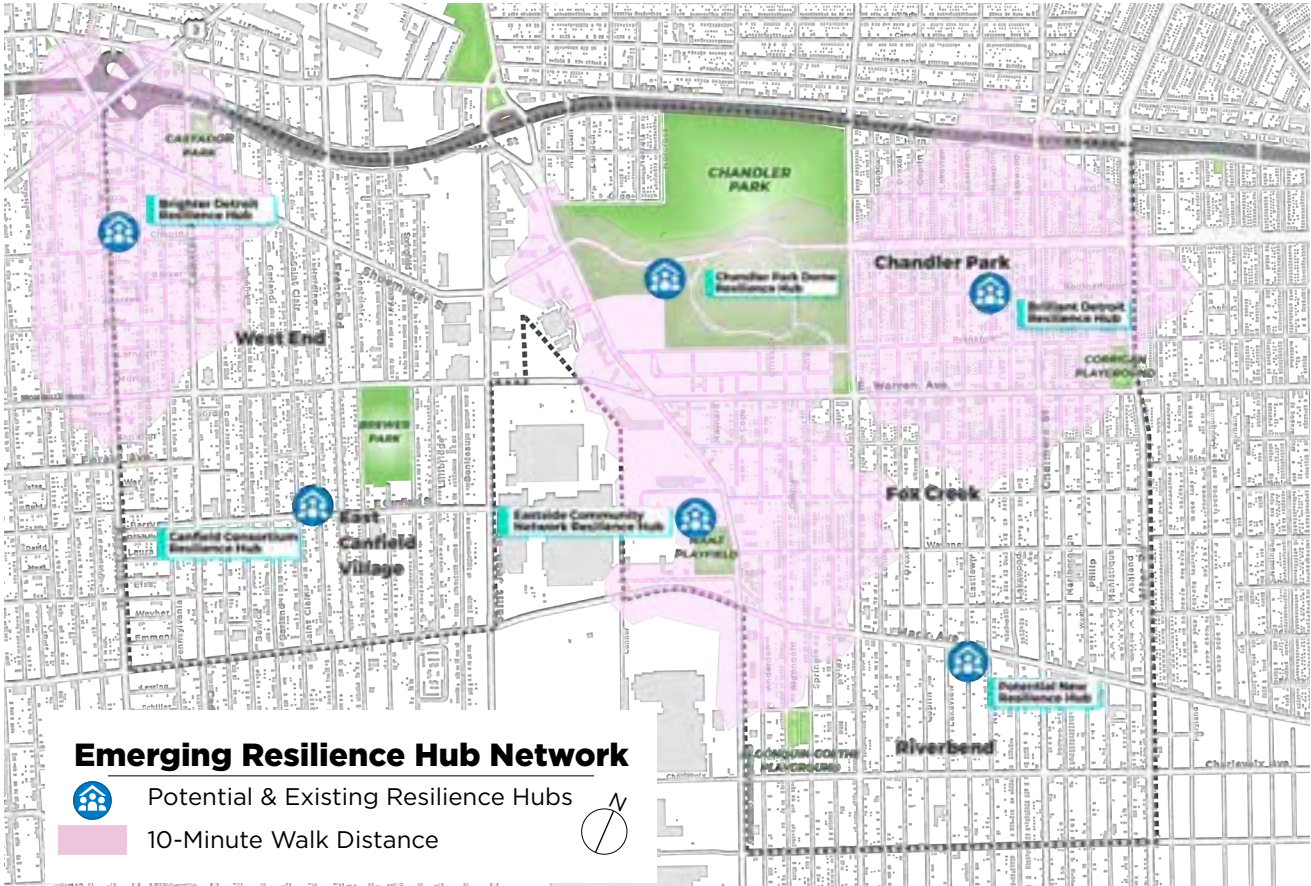
Resilience Hubs are a model of community support that provides a network or resources to residents. The network within Greater Warren/Conner would provide extreme weather relief, resilient sustainable energy systems, workforce development, community education, internet access, medical and social support services, and youth services. The recommended hub network would be accessible to over 4,300 residents within a 10-minute walk.

Local Non-Profit Actors

- Eco-Works
- Brilliant Detroit
- Eastside Community Network
- Elevate Energy
- What About Us (located just west of McClellan in Gratiot Woods)

Municipal Actors

- Wayne County Sustainability Program
- City of Detroit Office of Sustainability
- City of Detroit General Services Department
- City of Detroit and Wayne County Health Departments



Funding Sources to Support Resilience Hubs

Organization	Program	Purpose	Eligibility
FEMA	Hazard Mitigation Grant Program (HMGP)	Develop hazard mitigation plans.	State, local, tribal, and territorial governments. A local community may apply for funding on behalf of homeowners and businesses.
FEMA	Pre-disaster Mitigation Program	Plan for and implement sustainable cost-effective measures design to reduce the risk to individuals and property from future natural hazards.	State, local, tribal, and territorial governments.
FEMA	Building Resilience in Communities (BRIC)	Capability- and capacity-building activities, mitigation projects, management costs.	State, local, tribal, and territorial governments.
HUD	Community Development Block Grant (CDBG)	Property acquisition; rehabilitation of non-residential structures; construction of public facilities and improvements; activities related to energy conservation and renewable energy resources.	Principal cities of Metropolitan Statistical Areas (MSAs); other metropolitan cities with populations of at least 50,000; qualified urban counties with populations of at least 200,000 (excluding the population of entitled cities).
HUD	EDA’s Economic Adjustment Assistance Program	Technical, planning, and public infrastructure assistance; implementation grants to support the execution of site preparation, construction, rehabilitation and equipping of facilities.	Non-governmental organization, state, local, tribe, or territorial government.
Department of Energy	Solar Energy Innovation Network	Peer-to-peer learning and technical assistance to adopt solar energy.	All US-based stakeholders including government entities, community organizations, utilities, and developers.
State of Michigan	Michigan Solar Access Program	Applicants can seek aid to fund and deploy solar energy projects providing access to low-income households.	Cooperative utility, municipal utility, local or tribal government, non-profit organization, or small- to medium-sized business.
State of Michigan	Community Energy Management Incentive Program	Implement energy efficiency and renewable energy, fleet electrification, EV charging infrastructure, sustainable financing solutions, and/or clean energy workforce development.	Local government, tribal government, or other public service entity.
State of Michigan	Renewables Ready Communities	Funding for the construction of a grid-connected wind, solar, or energy storage project.	Local government which hosts or permits any portion of an eligible project.

Urban Farming



Urban farming creatively utilizes space, conserves land, and transforms vacant lots or buildings into productive greenspaces. There is a strong footing for farming in the Greater Warren/Conner neighborhood, with at least five publicly recognized agriculture projects and numerous other small-scale gardens.

- Small Ville Learning Farms (Chandler Park)
- Little Detroit Community Garden (East Canfield)
- Lemay Street Community Garden (West End)
- Sanctuary Farms (Riverbend)
- Sown in Peace Farm (East Canfield)
- Detroit Hives (West End)

Composting

Composting is the process of turning organic waste, such as food scraps and yard debris, into nutrient-rich soil through natural decomposition. It improves soil health, which is crucial for urban agriculture and community gardens to increase access to fresh, healthy produce in food desert areas. Composting also supports green stormwater infrastructure by enhancing soil’s water retention capabilities, helping to mitigate the city’s frequent flooding issues. Additionally, by diverting organic waste from landfills, composting reduces greenhouse gas emissions, while creating a collaborative environment for sustainability and economic initiatives. These benefits are particularly vital for addressing many of the challenges Detroit faces, such as high poverty rates, environmental racism, and food insecurity, especially in neighborhoods like Riverbend, Fox Creek, and East Canfield Villages.

The City’s Urban Agriculture Ordinance currently allows composting as an auxiliary use, supporting community and environmental benefits within urban farming projects. Additionally, the city is exploring the potential for new regulations that could allow composting as a variance. This initiative aims to expand opportunities for composting operations, encouraging broader adoption and making it easier to establish composting projects across the city. Given the support for composting operations in Greater Warren/Conner, pilot projects, funding, and new policies should focus on the neighborhood.

Community Land Trusts

Community land trusts (CLTs) are non-profit organizations that acquire and manage land to ensure it is used for the community’s benefit, often focusing on affordable housing, urban agriculture, and other community-driven projects. This approach benefits cities in multiple ways. CLTs foster cooperative living by providing a framework for residents to share resources, make collective decisions, and support one another, thereby enhancing social cohesion and community resilience. They also promote communal ownership by keeping land and housing permanently affordable, allowing the community to retain control and benefit from local development.

In Detroit, CLTs can address high poverty rates and housing insecurity by ensuring long-term housing stability and preventing displacement due to rising property values. CLTs also support urban agriculture by providing secure land tenure for community gardens and farms, increasing access to fresh, healthy produce in food desert areas. By emphasizing cooperative living and communal ownership, CLTs empower residents to take an active role in shaping their neighborhoods, fostering a sense of ownership and investment in the community’s future.

Momentum and support for Community Land Trusts is growing in the City of Detroit. Specifically there is a growing network of emerging CLTs supported by technical assistance from the Detroit Justice Center [28] and a financial contribution and land donations to start a new CLT was included as part of the Community Benefits Agreement for the new Henry Ford Hospital in New Center/Virginia Park neighborhoods [29].

District 4 Council Member Latisha Johnson is also encouraging the Detroit Land Bank Authority to develop a program for established CLTs to acquire discounted land bank properties for urban farming, commercial development and affordable housing. Community organizations in Detroit and near the Greater Warren/Conner study area are actively working to advance CLT projects. Promising examples include the North Corktown Open Spaces Community Land Trust [30] and the Common Grounds Neighborhood Plan and Community Land Trust [31].

In 2022, it was announced that the first USDA service center in the country dedicated to supporting urban agriculture would be opening in Detroit. While it is possible more programs and support could advance from these initiatives, the following programs are available to support existing and future urban agriculture projects:

Urban Farming Support Programs

Keep Growing Detroit Garden Resource Program (GRP)

Participants receive seeds, transplants, personalized garden assistance, and a connection to the network of gardeners, farmers, and advocates across Detroit, Highland Park, and Hamtramck. Applications open in January every year and resources are distributed the following spring. Keep Growing Detroit has also developed a guide to follow for community garden development.

Detroit Black Farmer Land Fund

The Detroit Black Farmer Land Fund is a coalition of three Detroit urban farming organizations providing financial assistance specifically targeted to support Black farmers in Detroit, Highland Park, and Hamtramck. The program’s funds may be used for land acquisition, infrastructure projects, and/or equipment needs. Applications to request funds open every year in June.

Kresge Foundation

Since 2009, the Kresge Foundation has provided at least \$500,000 in grant funds to support equitable community development and technical assistance that supports expanding farming systems in Detroit. In 2024, \$1.3 million in Implementation grants is available for resident-led community organizations to launch transformative projects that improve the quality of life and activate their neighborhoods. Recipients are eligible for up to \$150,000 each through the 2024 Kresge Innovative Projects Detroit Plus (KIP:D+) program.

City of Detroit Neighborhood Beautification Program

The Neighborhood Beautification Program provides grants to neighborhood groups, non-profit organizations, and faith-based organizations to create community gardens on up to four vacant lots in their community, with support of up to \$15,000. Only parcels zoned R1, R2, and R3 are eligible for funding. Non-profit organizations can partner with a block club or neighborhood association and must submit a partnership letter. Applicants must provide proof of ownership of the lots to be improved in the applicant organization’s name.

USDA Urban Growers Support

The USDA has a wealth of resources available for farms of all sizes within urban environments. Loans and grants are available to provide the necessary capital to buy or lease land, purchase equipment, or help with other operating costs. Technical and financial assistance is also available to meet urban farmers’ specific needs.

Implementation

- Ensure that land development policies and zoning regulations adequately protect the area’s farms and urban gardens. The City’s Urban Agriculture Ordinance allows for these uses and an Animal Husbandry Ordinance is currently proposed that would further support urban agriculture in Greater Warren / Conner.
- Incentivize new developments to include community garden spaces.
- Increase efforts to educate and engage residents about the benefits and processes of forming Community Land Trusts (CLTs).
- Establish a citywide Community Land Trust (CLT) fund to secure additional public and private investments, creating grant programs specifically for CLT development, and offering low-interest loans or financial incentives to encourage the establishment and maintenance of CLTs.



Little Detroit Community Garden. Source: OHM Advisors



Canfield-Lemay Community Flower Garden, established in 2019 by the Canfield Consortium. Source: Detroit Future City.

Flooding Mitigation

Flooding is seen as a challenge that impacts environmental and public health as well as quality of life in the Greater Warren/Conner neighborhood. The City of Detroit is exploring solutions to guide how the city can better manage water and prepare for record precipitation events. To approach the problem of flooding, the City of Detroit has embraced policies encouraging the use of green stormwater infrastructure in landscaping practices and pavements. Green stormwater infrastructure (GSI) installed on public property, adjacent to homes, businesses, and institutions, and in the right-of-way can have many of the same benefits as man-made stormwater management tools for a fraction of the cost as man-made infrastructure.

The U.S. Department of Housing and Urban Development (HUD) announced in March 2022 that the City of Detroit was awarded \$57,591,000 in Community Development Block Grant - Disaster Recovery (CDBG-DR) funding to support long-term recovery efforts following storm events from the 2021 flooding.

From June 25-26, 2021, Detroit experienced a 1 in 1,000-year rainstorm (0.01% probability), resulting in six to eight inches of rain in a 12-hour period in some areas of the city. Between 32,000 - 47,000 households (majority (82%) low-to-moderate income) were directly impacted by the disaster, the majority of those City Council District 4. In Greater Warren/Conner, an estimated 11 - 20% of households experienced water in their basements from these flood events, and are identified as priority neighborhoods for flood mitigation.

Within the Greater Warren/Conner neighborhood, GSI and other flooding mitigation treatments should be targeted in the “High” and “Moderate” flood risk zones in the neighborhood, shown in the map on page 62. These zones also overlap with the recommended locations for the Resilience Zones, which should include GSI as part of their design.

Through the City of Detroit’s CDBG Disaster Recovery Funds, the Housing Recovery & Protection Program was created to help repair or replace damaged sewer service lines and install backwater valves at eligible households (at or below 80% AMI) and replace items such as furnaces that were damaged due to the flooding. The program has a budget of \$43 Million.

Implementation

- Market the available funding through the Housing Recovery & Protection Program to repair or replace household sewer service lines and install backwater valves.
- Market incentives and educational opportunities for residents, businesses, and institutions to increase and maintain green stormwater infrastructure (GSI) on their properties.
- Dedicate staff and resources to support collaboration between the General Services Department, Office of Sustainability, Planning and Development Department, and Land-Based Projects team to oversee the preservation, creation, and maintenance of green stormwater infrastructure (GSI).
- Implement land use and transportation planning strategies in construction and development projects to account for and create space for flood storage and green stormwater infrastructure (GSI), removing significant volumes of stormwater from the combined sewer system. See map on page 60 for the most flood-prone areas of the Greater Warren/Conner neighborhood.
- Re-establish historic marshland habitat on publicly owned land through the creation of a shallow stormwater treatment wetland. Ensure that this habitat serves as an amenity to the surrounding community by making the space universally accessible and reflective of community recreation needs.



Chandler Park Bioswale. Source: OHM Advisors

Tree Planting Zones

In 1950, Detroit claimed to have more trees per capita than any other industrialized city. Between the 1950s and today, the Emerald Ash Borer, Dutch Elm disease, maintenance challenges, and climate change impacts have devastated the city’s tree canopy, with a 2021 study estimating a shortage of 1.2 million trees. Historically redlined communities in Detroit are also more likely to have about half of the tree cover than communities that were not segregated by socioeconomic and racial identity.

As a result, lower-income communities of color are vulnerable to environmental consequences, such as urban heat island effects, air quality issues, and flooding. In addition to improving public and environmental health conditions, trees have the potential to increase property values, reduce heating and cooling costs, slow traffic speeds, and build the green workforce in Detroit. The Urban Heat Island Effect map on page 64 shows the locations in the Greater Warren/Conner neighborhood where temperatures are significantly elevated compared to the City average.

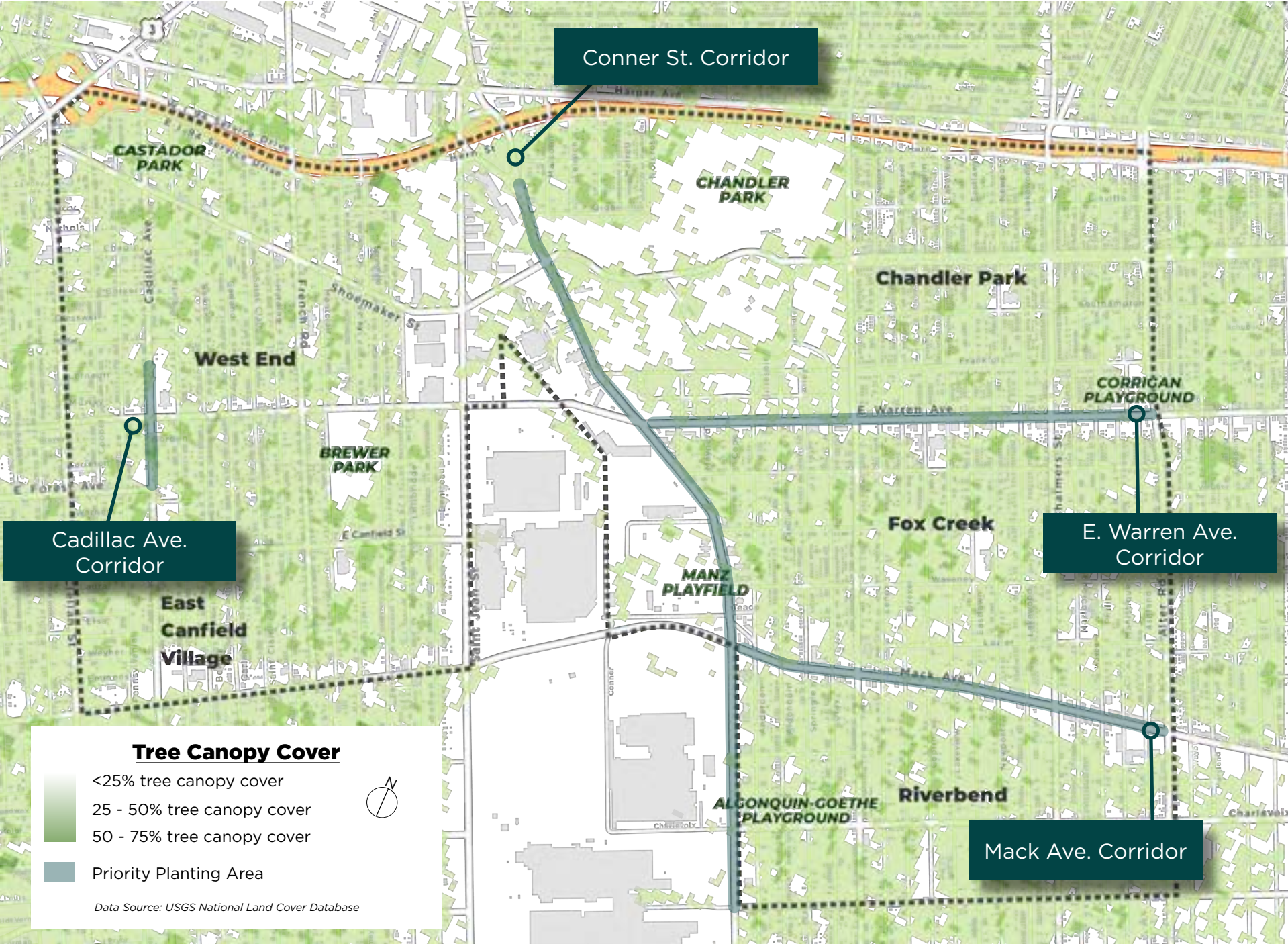
There are a number of organizations in Detroit participating in initiatives to rebuild the urban tree canopy. The City of Detroit Forestry Division is the entity responsible for planting, trimming, and removing trees on public property and in the public right-of-way. In 2023, the City expanded their forestry services to include trees needing maintenance or removal on private property, through the Dead, Dangerous, and Diseased Tree program.

The Greening of Detroit is a non-profit organization that has been active in community outreach and involving residents in tree planting for the past 35 years. In 2022, the Detroit Tree Equity Partnership (DTEP) was formed by American Forests, the City of Detroit, the Greening of Detroit, and other community organizations. Over the next five years, DTEP will plant more than 75,000 trees, employ over 300 Detroit residents in tree care and maintenance jobs, and invest \$30 million in Detroit neighborhoods during the five-year pilot phase.

There is also support at the federal level for tree planting and related activities. Between 2022 and 2026, the USDA will disburse \$1.5 billion to provide grants for tree planting and related activities through the Urban and Community Forestry Assistance Program. Funds will remain available under September 30, 2031, and require a 50% match.

Implementation

- Engage the Detroit Tree Equity Partnership to track and increase tree canopy in the Greater Warren/Conner neighborhood, prioritizing underserved areas or areas with an aging inventory.
- Focus initial tree planting efforts on corridors with a greater urban heat island effect and/or are lacking tree canopy cover, including Conner Street, E. Warren Avenue, Mack Avenue and along Cadillac Avenue.
- Increase marketing materials for the City of Detroit street tree planting program and Dead, Dangerous, and Diseased Tree program in Greater Warren/Conner.
- Develop a tree maintenance plan to trim, water, and care for new trees planted in Greater Warren/Conner and ensure healthy trees.



The Greater Warren/Conner Neighborhood Framework Plan consists of a wide variety of recommendations intended to improve the quality of life for residents. The strategies outlined in the plan for each of the Focus Areas are conceptual options of what will be implemented in Greater Warren/Conner to address the specific needs of residents. However, City Departments and neighborhood entities will need to work together in the future to make the concepts a reality.

It is important to note that this plan is the first step in a years-long effort to improve conditions in the neighborhoods. Stakeholders will be able to build upon the recent projects, upcoming investments, future plans, and hard work of the residents of the Neighborhood. Similar to other Framework Plans, this is intended to be a visionary document that guides both public and private investments so that they spur wider improvements to the area over time.

Ongoing work, additional designs, and deeper study will be required by City of Detroit agencies in coordination with community partners to implement the strategies identified in this Plan. Other incremental changes from private sources, such as business owners and residents, are expected to follow, bringing even more benefit to the community.

Ensuring the participation of residents in future work will be critical to ensuring each project reflects the unique desires and character of the community.

Over the coming years, following the completion of this Plan, several projects will move forward to more detailed planning and design phases, including the implementation of the Safe Streets for All improvements and a future redesign of Brewer Park.

Each project brings opportunities for community members to help refine and select specific aspects of the recommendations in this Plan.

Community Benefits Agreement Funding

Through conversations with the community, the Greater Warren/Conner Framework Plan developed a plan for spending the FCA/Stellantis Community Benefits Agreement funding in the impact area. Residents were most in favor of supporting additional home repairs throughout the community, as well as supporting community investments that align with feedback received from residents during the neighborhood planning process, such as grants for community initiated projects, street safety improvements, and beautification initiatives. The home repair funding will advance the Neighborhood Stabilization goals of the plan by improving the housing stock throughout the neighborhood and keeping residents in the community. Grants to community-based organizations can provide needed resources and capacity to grassroots organizations seeking to implement projects aligned with the framework goals of Neighborhood Stabilization, Economic Development, Parks, and Climate Resiliency. Upgrades to key streets and nodes can enhance safety for pedestrians and bicyclists, encourage walkability, and promote commercial activity, advancing Economic Development and Mobility goals of this framework.

Safe Streets for All Corridor Improvements

The Department of Public Works (DPW) will lead the design, engineering, and construction of low-cost, high-impact street safety improvements along Conner Street and Shoemaker Street as part of the \$50M grant awarded to the City. The detailed design process will include additional opportunities for neighborhood residents to provide more specific input on the improvements to each corridor. Community input meetings should be organized and promoted in consultation with local representatives once the implementation process for design begins.

Future Brewer Park Improvements

This Plan identifies short-term amenity upgrades to Brewer Park that could be implemented prior to a large-scale park redesign in coming years. The recommended short-term projects are intended to build on Sidewalk Detroit's planned improvements to the adjacent E. Canfield Art Park. When resources have been committed to the full Brewer Park renovation, residents will be able to work with the City to determine the park improvements that will transform the park into a vibrant community amenity.

The recommendations of this plan are intended to address both short-term needs in the community and the long-term goals of residents. As this plan is not backed by a specific funding source like the previous Neighborhood Framework Plans, there will be a continued need to pursue federal, state, and local funding opportunities within each City department to advance the recommendations of this plan. The recommendations of this plan should also be incorporated, where appropriate, into other ongoing and future city planning efforts, such as the Citywide Master Plan and Capital Planning process.

Ongoing Programs and Policies

In addition to future processes, existing DLBA programs, coupled with City initiatives, community development group efforts, philanthropic donations, and resident input will guide the implementation of this Plan.

Deploying Stabilization Programs

The DLBA will continue the existing auction and sales programs, as well as the existing side lot and other vacant lot programs. DLBA and PDD representatives should work with neighborhood block clubs and organizations to identify areas where the Nuisance Abatement Program should be initiated and begin enforcement.

In addition, PDD and the Bridging Neighborhoods Program (BNP) can collaborate to rehab selected properties using the Stellantis Community Benefits Agreement (CBA) funding. Homes will be selected through an application process and efforts to concentrate rehabs near each other will be made to catalyze neighborhood stabilization.

Further, the Detroit Demolition Department will continue to demolish homes that are unable to be renovated.

Other efforts include:

- Auctions and Own it Now programs will continue for DLBA properties throughout the Neighborhood;
- There will continue to be potential for residents to take part in vacant lot re-use (including side lot and neighborhood lot programs) throughout the Neighborhood;
- Depending on capacity, the Nuisance Abatement Program may begin at a targeted location, and potentially expand street by street;
- Focused stabilization should start at the targeted node in Chandler Park, and then potentially expand to East Canfield Village utilizing a combination of demolitions, neighborhood lot sales, property marketing, and Rehabbed & Ready to Renovate projects;
- Finally, the City should work with the DLBA to establish a pilot area for introducing the neighborhood Resilience Zone concepts.

Coordination with Detroit Land Use Policies

The City’s Master Plan sets the guiding vision for the future use of Detroit’s land. The City’s Zoning Ordinance is the Master Plan’s companion. It governs real estate development uses and urban form in accordance with the Master Plan. Based on the future vision that emerged for the community through the Greater Warren/Conner Framework Plan, there are areas of the Master Plan that could be updated to reflect and incorporate these recommendations.

As an example, the Plan calls for the introduction of neighborhood Resilience Zones to better utilize areas with high concentrations of vacant land. However, many of these areas are coded as Jobs and Amenity Development Areas (JADAs) which could provide a long-term opportunity for the development of large-scale public or private revitalization projects. Dedicating portions of the JADA designated areas specifically to support sustainability and quality of life improvements in the neighborhood would likely benefit future redevelopment of the neighborhood.

Additionally, the Plan recommends focusing economic development opportunities at nodes along the commercial corridors. Introducing a mix of uses along these corridors and within the nodes is essential to building activity in these areas to support future small business development.

One way to achieve a greater mix of uses along the neighborhood commercial corridors is to establish a Traditional Main Street Overlay (TMSO) district. Recently a TMSO was established along E. Warren from Audubon Street to Radnor Street. The TMSO relaxed zoning restrictions on traditional building types, like the existing commercial buildings along E. Warren Avenue, Mack Avenue, and Shoemaker Street that have upper floor apartments. PDD will continue to determine which elements of the City’s land use policy framework can best incorporate the community-centered vision expressed through the Plan to facilitate the implementation of the overall vision.

Phasing the Strategies of this Plan

The table below summarizes the implementation strategy for the Greater Warren/Conner Neighborhood Framework Plan. Each Plan Focus Area has areas of priority in the short-, medium-, and long-term that will be achieved by the specific recommended actions. The matrix is not intended to be a detailed breakdown of all the recommendations of the Framework Plan, but instead to help guide City departments, neighborhood groups, philanthropic organizations, and other entities in achieving the goals of the community.

The initiatives are sorted by short-term (1-3 year completion), mid-term (4-6 year completion), and long-term (7-10 year completion) recommendations. Short-term projects may have already secured

funding and/or City approval and can be completed in the near future. Mid-term projects will require additional planning, design, and/or funding before implementation. Finally, long-term projects will rely on the success of the short- and mid-term projects to ensure conditions are right for implementation.

Additionally, long-term projects that have not been identified here may be pursued by public or private entities, or neighborhood residents in partnership with City staff, neighborhood stakeholders, and other leaders.

	Short-Term 1-3 Year Initiatives	Mid-Term 4-6 Year Initiatives	Long-Term 7-10 Year Initiatives
Neighborhood Stabilization	Apply focused stabilization strategies to the priority areas in Chandler Park and East Canfield Villages.	Support new residential development through Parkside Homes, a pilot of the Resilience Zone, and coordination with the TMHA.	Establish a stable residential real estate market that can self-sustain home rehabs, as well as market rate and affordable new construction.
Commercial and Economic Development	City to focus on ‘Blight to Beauty’ improvements, including demolitions, tree plantings, murals, corridor clean ups, and safety enhancements.	Begin to stabilize the corridors by building activity at primary nodes through facade improvements, pop-up activation, and gray-boxing commercial spaces.	Begin a full retail strategy at key nodes with gap financing support, through Motor City Match and philanthropic support. Identify opportunities for new development.
Mobility and Streets	Implement street design solutions to slow vehicles down, improve traffic safety, and address air pollution.	Redesign streets, such as Chandler Park Drive, and implement new transit amenities to support resident mobility.	Continue to implement transit, non-motorized, and traffic safety improvements that will benefit the neighborhood.
Parks and Open Space	Implement the passive park improvements to Brewer Park to establish spaces for future amenities.	Implement full redevelopment of Brewer Park alongside GSD.	Support the enhancement of the Iron Belle Trail and Canfield Connect through the neighborhood.
Climate Resilience and Health Equity	Utilize existing programs and identify new funding sources to support sustainability and Resilience Hubs in the Neighborhood.	Support the expansion and continued maintenance of tree planting, green stormwater infrastructure, urban farming, and Resilience Hubs in the Neighborhood. Identify and support new opportunities for Resilience Hubs in areas that are lacking access.	

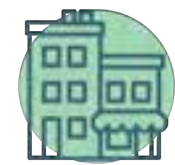


Neighborhood Stabilization Implementation Plan

ACTIONS	TIMELINE	PROJECT PARTNERS
Home rehabs in Chandler Park	Short-Term	Detroit Land Bank Authority (DLBA), Housing & Revitalization Department (HRD), Bridging Neighborhoods
Home stabilization in East Canfield Village	Short-Term	DLBA, HRD, Bridging Neighborhoods
Encourage new build residential developments in the Targeted Multifamily Housing Area near Mack Avenue and Alter Road	Mid-Term	HRD, DLBA, Planning and Development Department (PDD)
Redevelopment of Parkside Homes and Reconnection to Surrounding Neighborhood	Mid-Term	Detroit Housing Commission
Establish a pilot Resilience Zone as a proof of concept and monitor its performance	Mid-Term	DLBA, PDD, HRD
Support home repair and renovations throughout neighborhood	Long-Term	DLBA, Detroit Justice Center, Sanctuary Farms, Private Sources
Explore the establishment of a community land trust	Long-Term	PDD, DLBA, HRD, local advocacy groups
Build out a full Resilience Zone concept with infill housing and select another area to introduce a project	Long-Term	PDD, Office of Sustainability

Funding Options

- Community Development Block Grants (CDBG)
 - Detroit Neighborhood Initiative (DNI)
 - Community Development Financial Institutions (CDFIs)
 - HUD HOME Investment Partnerships Program
 - EPA Environmental Justice Collaborative Problem Solving (EJCPS) Cooperative Agreement Program
 - MSHDA Michigan Housing Opportunities Promoting Energy
- Efficiency Program (MI-HOPE)
 - Michigan Environment, Great Lakes, and Energy (EGLE)
 - Philanthropic and Non-Profit Organizations, such as the Gilbert Family Foundation
 - Neighborhood Impact Fund (from Stellantis Community Benefits Agreement)
 - Financial Institutions



Commercial and Economic Development Implementation Plan

ACTIONS	TIMELINE	PROJECT PARTNERS
Establish a commercial corridor beautification entity responsible for cleaning, maintaining, and upgrading Mack, Conner, Shoemaker, and Warren.	Short-Term	Detroit Econmic Growth Corportation (DEGC), Jobs & Economy Team (JET), Community Partners
Demolish commercial building that are unable to be saved.	Short-Term	Demolition Department, Detroit Building Authority (DBA)
Identify and market City-owned properties for redevelopment at key commercial nodes	Mid-Term	DEGC, JET, HRD, PDD, DBA, Invest Detroit
Implement pop-up and grassroots placemaking efforts to build activity at the recommended primary commercial nodes through food truck vendors, local markets, and event programming.	Mid-Term	PDD, DEGC, Eastside Community Network (ECN), Canfield Connect, Sanctuary Farms, Small Ville Farm
Identify funding opportunities to support small scale property improvements, including facade upgrades and gray boxing commercial spaces.	Mid-Term	DEGC, Invest Detroit
Target landscaping and infrastructure improvements at recommended nodes	Mid-Term	Department of Public Works (DPW), General Services Department (GSD)
Re-zone former school properties slated for demolition to facilitate residential and mixed-use development compatible with surrounding neighborhoods.	Long-Term	DBA, HRD
Develop a retail strategy to bring new businesses to the commercial corridors, utilizing gap financing, Motor City Match, and philanthropic support.	Long-Term	PDD

Funding Options

- Community Development Block Grants (CDBG)
 - Community Development Financial Institutions (CDFIs)
 - New Market Tax Credits
 - Detroit Economic Growth Corporation (DEGC)
 - Michigan Economic Development Corporation (MEDC)
- Invest Detroit
 - Community Foundation of Southeast Michigan
 - Kresge Foundation
 - Neighborhood Impact Fund (from Stellantis Community Benefits Agreement)



Mobility and Streets Implementation Plan

ACTIONS	TIMELINE	PROJECT PARTNERS
Green/tree buffering of I-94 on vacant parcels.	Short-Term	Michigan Department of Transportation (MDOT), DPW
Slow vehicles and reduce through traffic on neighborhood streets by adding signage, speed bumps, chicanes, and other design treatments.	Short-Term	DPW, PDD
Redesign Safe Streets for All corridors with low-cost, high impact improvements.	Short-Term	DPW
Clarify and enforce truck routes through the neighborhood with a truck route ordinance.	Short-Term	DPW, PDD, City Council
Implement safety improvements for all users on I-94 bridge crossings.	Mid-Term	MDOT, DPW
Redesign Chandler Park Drive to support multiple travel modes.	Mid-Term	DPW, GSD, Chandler Park Conservancy
Add bus stops amenities throughout the community to support transit riders.	Mid-Term	DDOT, DPW, Regional Transit Authority (RTA) of Southeast MI
Implement a ‘mobility hub’ at Conner and Warren to support multi-modal travel and connections.	Long-Term	DDOT, DPW, RTA of Southeast MI
Implement the recommendations from Streets for People to support multi-modal travel through the neighborhood.	Long-Term	DPW

Funding Options

- American Forests
 - DTE Foundation
 - SEMCOG TAP grants
 - EPA Environmental Justice Collaborative Problem Solving (EJCPS) Cooperative Agreement Program
- Michigan Department of Transportation (MDOT)



Parks and Open Space Implementation Plan

ACTIONS	TIMELINE	PROJECT PARTNERS
Implement the short-term, passive park improvements to Brewer Park.	Short-Term	GSD
Enhance the pedestrian crossing areas across E Warren Avenue to Brewer Park.	Short-Term	DPW, GSD
Perform a park master plan exercise for Brewer Park to identify future amenity improvements.	Mid-Term	GSD
Implement long-term park improvements to Brewer Park as specified through the master plan process.	Mid-Term	GSD
Support the development of new trails within the neighborhood, including Can-field Connect and the new alternative routing of the Conner Creek Greenway (off of the Stellantis Truck Route).	Long-Term	PDD, DPW, DLBA, GSD, Canfield Connect, Detroit Green-ways Coalition
Strategically enhance Iron Belle Trail visibility on Warren Avenue through safety features, signage, and recreation amenities.	Long-Term	Michigan Department of Natural Resources (DNR), DPW, GSD
Preserve parkland within the neighborhood for recreation and open space.	Long-Term	GSD, PDD

Funding Options

- American Forests
 - DTE Foundation
 - SEMCOG TAP grants
 - EPA Environmental Justice Collaborative Problem Solving (EJCPS) Cooperative Agreement Program
- Michigan Environment, Great Lakes, and Energy (EGLE)
 - Michigan Department of Natural Resources (DNR)
 - Neighborhood Impact Fund (from Stellantis Community Benefits Agreement)

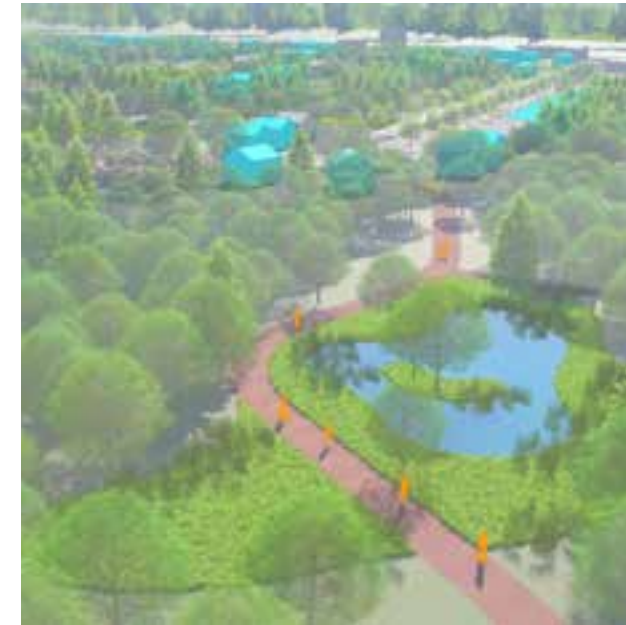
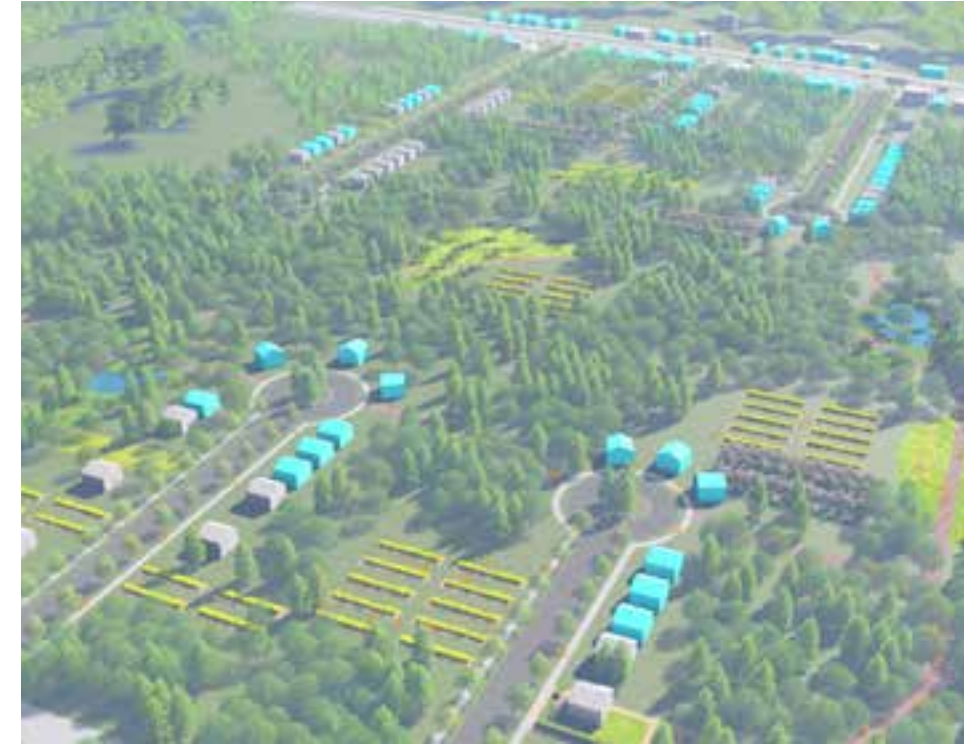


Climate Resilience and Health Implementation Plan

ACTIONS	TIMELINE	PROJECT PARTNERS
Support investments in existing Resilience Hubs within the Neighborhood.	Short-Term	Community Partners, Philanthropic Partners
Pursue federal, state, and local grants to monitor and reduce air indoor and outdoor air pollution.	Short-Term	Buildings, Safety, Engineering, & Environment Depart-ment (BSEED), Wayne County
Grow the tree canopy along streets and on publicly owned property, using species suitable for filtering particulate matter and reducing air pollution.	Short-Term	GSD, DPW, Greening of Detroit
Support the expansion of urban farming in the neighborhood.	Mid-Term	Keep Growing Detroit, Detroit Black Farmer Land Fund, Kresge Foundation, USDA Urban Growers, Neighborhood Beautification Program
Dedicate staff and funding to support flood mitigation initiatives, including the dedication of runoff storage space and green stormwater infrastructure.	Mid-Term	Detroit Water & Sewerage Department (DWSD), GSD
Develop a tree maintenance plan for street trees and others within public rights of way.	Mid-Term	GSD
Invest in future Resilience Hubs in the Neighborhood to support needs in the community.	Long-Term	Community Partners, Philanthropic Partners

Funding Options

- American Forests
 - DTE Foundation
 - SEMCOG
 - EPA Environmental Justice Collaborative Problem Solving (EJCPS) Cooperative Agreement Program
- Michigan Environment, Great Lakes, and Energy (EGLE)
 - University of Michigan Infrastructure Investment and Jobs Act (IIJA) & Inflation Reduction Act (IRA) grants tracker
 - Neighborhood Impact Fund (from Stellantis Community Benefits Agreement)



SOURCES

[1] City of Grosse Pointe. *History of the Pointes*. https://www.grossepointecity.org/how_do_i_learn_about_grosse_pointe/history.php

[2] 2022 ACS 5-Year Estimates, Table B01003.

[3] 2022 ACS 5-Year Estimates, Table DP02.

[4] Jumi Consulting Group, Market Analysis.

[5] EPA. *Environmental Justice Screening and Mapping Tool*. <https://ejscreen.epa.gov/mapper/>

[6] Alvarez, Sarah. *In Detroit’s housing market, rentals may be outpacing owner-occupied properties*. <https://outliermedia.org/detroit-rental-market-properties-increase-data/>

[7] UM Poverty Solutions. *A Decent Home: The Status of Home Repair in Detroit*. <https://poverty.umich.edu/files/2020/10/The-Status-of-Home-Repair-in-Detroit-October-2020.pdf>

[8] Jumi Consulting Group, Market Analysis.

[9] ACS 5-Year Estimates. *Table B25070 | Gross Rent as a Percentage of Household Income in the Past 12 Months*. <https://data.census.gov/table/ACSDT5Y2022.B25070?t=Renter%20Costs&g=160XX00US2622000>

[10] City of Detroit. *Streets for People Design Guide*. https://detroitmi.gov/sites/detroitmi.localhost/files/2021-12/SFP_DesignGuide_20210930%20%283%29.pdf

[11] Detroit Collaborative Design Center. *Planning for Resilience and Healthy Equity*. <https://www.dcdc-udm.org/resilient-healthy-thriving-neighborhoods>

[12] Brooker, Jena. *Detroit’s ‘ghost’ streams can be a threat in the present*. https://www.bridgedetroit.com/detroits-ghost-streams-can-be-a-threat-in-the-present/?utm_source=pocket_reader

[13] Wayne State University. *Household Flooding in Detroit*. https://huw.wayne.edu/learning-center/detroit_flood_report_2021.pdf

[14] Eastside Resident Environmental Health Working Group. *Environmental Health Survey Summary of Findings*. <https://www.eastsideenvironmental.org/2019/08/13/environmental-health-survey-summary-of-findings/>

[15] Detroit Neighborhood Initiative. <https://detroitmi.gov/government/mayors-office/detroit-neighborhood-initiative>

[16] Detroit Land Bank Authority Property Programs. <https://buildingdetroit.org/programs>

[17] Michigan State Housing Development Authority. MI Home Loan Program. <https://www.michigan.gov/mshda/homeownership/mi-home-loan>

[18] Detroit Planning and Development Department. Detroit 0% Interest Home Repair Loans. <http://www.detroithomeloans.org/>

[19] Detroit Land Bank Authority. <https://buildingdetroit.org/programs>

[20] Proposal N Demolitions and Stabilizations Dashboard. <https://www.arcgis.com/apps/dashboards/89219fd4fb334bac8392a0f04667756e>

[21] Detroit Planning and Development Department. Living with Land. <https://url.us.m.mimecastprotect.com/s/PfOKCQWRWNs6v5PNhMiJuGrke9?domain=acrobat.adobe.com>

[22] US Forest Service. *Carbon*. <https://www.fs.usda.gov/managing-land/sustainability-and-climate/carbon>

[23] City of Detroit. *Detroit Solar Toolkit*. [https://detroitmi.gov/sites/detroitmi.localhost/files/2023-11/19_Detroit Solar Toolkit_Solar101_web.pdf](https://detroitmi.gov/sites/detroitmi.localhost/files/2023-11/19_Detroit%20Solar%20Toolkit_Solar101_web.pdf)

[24] SEMCOG. *Access to Parks*. <https://maps.semco.org/accessparks/>

[25] <https://detroitmi.gov/departments/detroit-parks-recreation/parks-and-greenways/current-park-projects/nature-pockets-program#:~:text=In%20an%20effort%20to%20increase,wildflower%20meadow%2C%20or%20a%20raingarden>

[26] Audubon Great Lakes. *Priority Region: Detroit River and the St. Clair Flats*. <https://gl.audubon.org/coastal-wetlands/priority-region-detroit-river-st-clair-flats#:~:text=Comprised%20of%20highly%20valuable%20wetlands,Black%20Tern%20and%20Marsh%20Wren>

[27] Mondry, Aaron. *Why Detroiters have a right to their parks - mostly*. [https://outliermedia.org/selling-detroit-park-land-conversions-rogell-wigle-wick/?utm_medium=email&utm_source=Outlier+Media&utm_campaign=c869b55631-EMAIL_CAMPAIGN_2024_04_08_09_17&utm_term=0_-c869b55631-\[LIST_EMAIL_ID\]](https://outliermedia.org/selling-detroit-park-land-conversions-rogell-wigle-wick/?utm_medium=email&utm_source=Outlier+Media&utm_campaign=c869b55631-EMAIL_CAMPAIGN_2024_04_08_09_17&utm_term=0_-c869b55631-[LIST_EMAIL_ID])

[28] Detroit Justice Center. *Economic Equity Practice*. <https://detroitjustice.org/eep/>

[29] City of Detroit Planning and Development Department. *Community Benefits Provision - The Future of Health*. https://detroitmi.gov/sites/detroitmi.localhost/files/2024-03/Future%20of%20Health%20Community%20Benefits%20PDD%20Report_UPDATED_2.27.2024.pdf

[30] North Corktown Neighborhood Association. *North Corktown Open Space Community Land Trust*. <https://www.northcorktown.org/open-spaces-community-land-trust>

[31] Dunn, Patrick. *Community land trusts make strides toward long-term affordable housing in Detroit*. <https://outliermedia.org/detroit-community-land-trusts/>



HOOD CLOSED
TO
GENTRIFIERS™