

**Environmental Assessment
Determinations and Compliance Findings
for HUD-assisted Projects
24 CFR Part 58**

Project Information

Project Name: 60-Harper

HEROS Number: 900000010330199

Responsible Entity (RE): DETROIT, PLANNING AND DEVELOPMENT DEPARTMENT
DETROIT MI, 48226

RE Preparer: Kim Siegel

State / Local Identifier: Detroit, Michigan

Certifying Officer: Julie Schneider

Grant Recipient (if different than Responsible Entity):

Point of Contact:

Consultant (if applicable): Environmental Consulting Solutions (ECS)

Point of Contact: Julie Pratt

Project Location: 60 Harper Ave, Detroit, MI 48202

Additional Location Information:

The Property will be located at 60 Harper Avenue, Detroit, Michigan 48202, within the Medbury Park Neighborhood of Detroit. The development site will involve the combination of two adjacent sites - 60 Harper Avenue and 71 E. Edsel Ford - into one

property address at 60 Harper. NOTE: the existing building currently located at 71 E. Edsel Ford is not included in the housing development plans; this building will be parceled out.

Direct Comments to: Penny Dwoinen, the City of Detroit Environmental Review Officer
Telephone: 313.224.2933
Email: dwoinenp@detroitmi.gov

Description of the Proposed Project [24 CFR 50.12 & 58.32; 40 CFR 1508.25]:

The proposed project includes new construction of a three-story, mixed-use, affordable apartment community located at 60 Harper Avenue, Detroit, Michigan 48202. The development site will involve the combination of two adjacent sites (60 Harper and 71 E Edsel Ford) into one property address at 60 Harper. The project will involve the acquisition of the site by 60 Harper Limited Dividend Housing Associates, LLC. It shall be noted that one unoccupied, multi-tenant residence structure is located on the 71 E Edsel Ford parcel. This structure and associated land will be split from the parcel and is not included within the proposed development plans. The Project includes construction of one new three-story mixed-use building, ~17,665 sq. feet, with first floor commercial space and second/third floor residential units. The property will feature 49 affordable apartment units (two-bedroom units with ~785 square feet each) managed by MHT's affiliated managing agent MHT Management, LLC. All units will feature energy efficient appliances including washers and dryers, dishwashers, frost-free refrigerators, ovens and ranges with hoods, disposals, microwaves and central air conditioning. The development will feature a combination of private parking with an attached parking lot and street parking. In addition, the property will include roughly 1,469 square feet of commercial space facing Harper Avenue. MHT plans to target a wide range of residents in this development and has worked closely with the City of Detroit on a development that complements the neighborhood and aligns with the City's design requirements. With MHT's 30+ years of affordable housing development experience, MHT Management's 15+ years of affordable management experience, and SAP's 100+ years as a community member in the neighborhood, there will be sufficient strength and expertise to get this development completed. In looking at the landscape for those needing affordable housing options, the team feels that this housing development offers a great opportunity to provide affordable units in an area that is experiencing tremendous growth, creating a great outcome by allowing lower income wage earners to have access to amenities that they otherwise might not have. The project is consistent with the City of Detroit's strategic targeted revitalization areas, and the development team has received support in the form of \$1,500,000 in HOME funding and a 4% Payment in Lieu of Taxes agreement from the City of Detroit (see Letter of Interest from the City of Detroit in Exhibit 8). Further, the development has received an award of 8 units of Project Based Vouchers from the Detroit Housing Commission, allowing housing to be provided to folks at the lowest income levels. This Environmental Review is valid for up to five years. Total HUD funded amount is \$250,000.00 in HOME 2022.

Statement of Purpose and Need for the Proposal [40 CFR 1508.9(b)]:

The project is an opportunity to improve the quality of life for local residents and create a stronger, more connected community. The housing element of the project centers on the development of mixed-income housing, creating new housing opportunities for current and future Detroit residents across a wide range of household incomes. The wide range of incomes targeted in this development will allow for a diverse community and new housing options for families at every income level. Additionally, the development will replace vacant land with high-quality, modern housing built within the existing landscape. The development will provide stability and safety to the area, as well as desired commercial/retail opportunities within the neighborhood. Future residents will also benefit greatly from the availability of new services that will be made available that otherwise may not be if not for the construction of this development. Specifically, ownership has entered into an MOU to provide for the availability of the healthcare services to be provided on-site and has also received a commitment from a local workforce training center that is expanding its operations to allow for inclusion of residents of 60 Harper.

Existing Conditions and Trends [24 CFR 58.40(a)]:

This housing development offers a great opportunity to provide affordable units in an area that is experiencing tremendous growth, creating a great outcome by allowing lower income wage earners to have access to amenities that they otherwise might not have. In addition to all of the amenities and opportunities that exist in this location for prospective low-income residents today, there are many millions of dollars of planned future investment for this area as well, making this not only a great location to live in, but a great location to use Federal, State, and Local affordable housing resources. The site is currently vacant, underutilized land. The site is presently being used as parking for nearby automotive business and church. If the project does not occur, the site would likely remain vacant underutilized land, further limiting the options for affordable housing in a positive growth area with many employment options and community services nearby. Overall characteristics of the immediate neighborhood are diverse, consisting of a mixture of residential and commercial usages. Woodward Avenue, Grand Boulevard, and Warren Avenue are all located within half mile of the site and contain the greatest amount of retail concentrations locally, although opportunities can be found along other secondary streets throughout the area. The site is located near the thriving Midtown and New Center areas. In contrast to most areas throughout Detroit, the Harper Avenue PMA has exhibited modest growth over the past decade. According to U.S. Census data and ESRI forecasts, the PMA has an estimated overall population of 44,858 persons in 2022 representing an increase of one percent from 2010 (nearly 500 persons). Future projections indicate continued small growth, increasing by less than one percent over the next five years (approximately 50 persons) between 2022 and 2027. In comparison, the city of Detroit (with a population of 637,423 in 2022) decreased by 11 percent between 2010 and 2022 and is anticipated to decrease by an additional one percent through 2027.

Overall income levels within the Harper Avenue market area are among the lowest within the city. As such, the median household income for the PMA was estimated at \$25,903 for 2022, which was 29 percent below the city itself (at \$36,239), and 53 percent below Wayne County as a whole (\$55,187). The median household income for the PMA increased by 2.9 percent annually between 2010 and 2022 (as compared to an annual increase of 2.3 percent for the city). Additionally, steady income appreciation is expected to continue over the next five years, with an annual average increase of 3.4 percent for the PMA (as compared to 3.7 percent for the city). Overall conditions for the Harper Avenue rental market appear extremely positive. Based on a recent survey of 25 rental developments within the PMA, the overall occupancy rate was calculated at 97.9 percent (excluding three properties with units currently under rehab). It should be noted that the most recently developed LIHTC facility within the PMA was rapidly absorbed. Milwaukee Junction Apartments consists of 25 one-bedroom units constructed in 2021 and is located roughly three blocks north of the subject property. According to the property manager, the project was fully leased within one week of opening - providing additional evidence of the need for affordable housing locally. Based largely on the subject's affordability relative to overall market averages and rapid lease-up of the PMA's most recent LIHTC property, and further considering that the subject proposal will be a newly constructed property with ample amenities with visibility from Woodward Avenue, the proposed targeting structure and rental rates appear to be properly positioned. Considering the proposed rents are similar to the 60 percent AMI average, the subject's rental rates can be considered as appropriate and achievable for the Harper Avenue market area.

Maps, photographs, and other documentation of project location and description:

[Figure 1 Site Location and Figure 2 Aerial.pdf](#)

[60 Harper - Project Narrative.pdf](#)

[60 Harper Site Plans.pdf](#)

[Phase I Photographs.pdf](#)

Determination:

✓	Finding of No Significant Impact [24 CFR 58.40(g)(1); 40 CFR 1508.13] The project will not result in a significant impact on the quality of human environment
	Finding of Significant Impact

Approval Documents:

[60 Harper Signature Page.pdf](#)

7015.15 certified by Certifying Officer on:

**7015.16 certified by Authorizing Officer
on:**

Funding Information

Grant / Project Identification Number	HUD Program	Program Name
M22MC260202	Community Planning and Development (CPD)	HOME Program
MI1001	Public Housing	Housing Choice Voucher Program

Estimated Total HUD Funded, Assisted or Insured Amount: \$250,000.00

This project anticipates the use of funds or assistance from another federal agency in addition to HUD in the form of:

Estimated Total Project Cost [24 CFR 58.2 (a) (5)]: \$18,968,969.00

Compliance with 24 CFR §50.4, §58.5 and §58.6 Laws and Authorities

Compliance Factors: Statutes, Executive Orders, and Regulations listed at 24 CFR §50.4, §58.5, and §58.6	Are formal compliance steps or mitigation required?	Compliance determination (See Appendix A for source determinations)
STATUTES, EXECUTIVE ORDERS, AND REGULATIONS LISTED AT 24 CFR §50.4 & § 58.6		
Airport Hazards Clear Zones and Accident Potential Zones; 24 CFR Part 51 Subpart D	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project site is not within 15,000 feet of a military airport or 2,500 feet of a civilian airport. The distance to the closest airport (Coleman A. Young CAY) is approximately 4.3 miles northeast. The project is in compliance with Airport Hazards requirements.
Coastal Barrier Resources Act Coastal Barrier Resources Act, as amended by the Coastal Barrier	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is not located in a CBRS Unit. Therefore, this project has no potential to impact a CBRS Unit and is in

Improvement Act of 1990 [16 USC 3501]		compliance with the Coastal Barrier Resources Act.
Flood Insurance Flood Disaster Protection Act of 1973 and National Flood Insurance Reform Act of 1994 [42 USC 4001-4128 and 42 USC 5154a]	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The structure or insurable property is not located in a FEMA-designated Special Flood Hazard Area. The Project is located in Zone X - Area of Minimal Flood Hazard. The project is in compliance with flood insurance requirements. Refer to attached FEMA panel # 26163C0280E, effective date 2/2/2012.
STATUTES, EXECUTIVE ORDERS, AND REGULATIONS LISTED AT 24 CFR §50.4 & § 58.5		
Air Quality Clean Air Act, as amended, particularly section 176(c) & (d); 40 CFR Parts 6, 51, 93	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project's county or air quality management district is in non-attainment status for the following: Ozone. EGLE is currently working to complete the required SIP submittals for this area; therefore, an alternative evaluation was completed to assess conformity. Specifically, EGLE considered the following information from the United States Environmental Protection Agency's (USEPA) general conformity guidance, which states "historical analysis of similar actions can be used in cases where the proposed projects are similar in size and scope to previous projects." EGLE has reviewed the project proposed to be located at 60 Harper in Detroit, Michigan, it was determined that based on the size, scope and duration of the property project, emission levels for the project should not exceed de minimis levels for general conformity. The project is in compliance with the Clean Air Act. As of July 2023, the ozone area in Wayne County is in attainment/maintenance status.
Coastal Zone Management Act Coastal Zone Management Act, sections 307(c) & (d)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is not located in or does not affect a Coastal Zone as defined in the state Coastal Management Plan. The project is in compliance with the Coastal Zone Management Act.

<p>Contamination and Toxic Substances 24 CFR 50.3(i) & 58.5(i)(2)]</p>	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p>	<p>There are no buildings/structures on the Project site; lead and asbestos determination was not applicable. The Project is located in Wayne County, MI: 17% of homes tested equal or greater to 4 pCi/L for radon. The Project is Wayne County, Zone 3 for Radon. Site contamination was evaluated as follows: ASTM Phase I ESA dated 8/18/2023, ASTM Vapor Encroachment Screening (Section 9.9 in Phase I ESA), ASTM Phase II ESA dated 5/30/2023, BEA completed 5/31/2023, and Response Activity Plan for Due Care Compliance dated 7/5/2023. On-site or nearby toxic, hazardous, or radioactive substances were found that could affect the health and safety of project occupants or conflict with the intended use of the property. The adverse environmental impacts will be mitigated through remedial excavation (removal). With mitigation, identified in the mitigation section of this review, the project will be in compliance with contamination and toxic substances requirements. Phase I ESA identified RECs including: 1) known impact and "facility" status from a release related to fill material and 2) potential vapor encroachment from surrounding properties. Phase II ESA was conducted to assess the RECs; soil and groundwater samples were collected for analysis. The assessment results have indicated the presence of VOCs, PNAs and metals at concentrations exceeding their respective Residential GRCC or VIAP SLs. Specifically, soil samples collected from the subject property identified the following: concentrations of benzene, 2-methylnaphthalene, naphthalene, 1,2,4-trimethylbenzene, 1,3,5-trimethylbenzene and/or xylenes exceeded their respective EGLE Residential Soil VIAP SL in soil samples</p>
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		<p>SB-2 (1.0'), SB-5 (1.0'), SB-16 (2'-3') and SB-26 (3'-4'). Benzo(a)pyrene exceeded EGLE GRCC for Direct Contact at SB-2 (1.0'), SB-8 (2.0'), and SB-26 (3'-4'). Exceedances of the EGLE Residential VIAP SL for phenanthrene were detected SB-1 (0.5'), SB-2 (1.0'), SB-2 (2.0'), SB-6 (1.5'), SB-8 (2.0'), and SB-26 (3'-4'). Exceedances of VIAP SL for naphthalene were detected at SB-2 (1.0'), SB-2 (2.0'), and SB-26 (3'-4'). Lead exceeded EGLE GRCC for Direct Contact in one soil sample, SB-11 (5'). Multiple soil samples exceed VIAP SL for mercury at the Subject Property at depths mainly ranging from 0.5 to 4.5 feet bgs. A Response Activity Plan was prepared, proposing remedial excavation to remove impacted soils followed by verification sampling. A copy of the EGLE Notice of Approval of Response Activity letter dated 8/17/2023 is included as an attachment. Mitigation steps summarized in the RespAP include the following: 1) Removal of impacted soil across the subject property, mainly non-native urban fill material. In locations of the proposed building foundation depths of material to be removed are approximately 5 -6 feet. Across the remaining portions of the Subject Property depths are approximately 3 feet. 2) Verification of remediation Sampling will be conducted at bottom of excavation depths across the Subject Property to ensure that adequate impacted soil has been removed and clean soil remains. Soil will be analyzed by laboratory prior to clean backfill material being placed at the Subject Property. 3) The fill material brought to the site will be documented as clean by analytical results from samples collected from the site of origin documenting that the material does not contain volatile organic compounds,</p>
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		polynuclear aromatic hydrocarbons, or Michigan Ten Metals at concentrations above the applicable generic cleanup criteria.
Endangered Species Act Endangered Species Act of 1973, particularly section 7; 50 CFR Part 402	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is located in the highly urbanized and highly redeveloped area of the City of Detroit. There are no designated critical habitats in the action area, the project is urban infill, and the project is redevelopment of previously developed parcels in a heavily developed urban area. This project has been determined to have No Effect on listed species. This project is in compliance with the Endangered Species Act.
Explosive and Flammable Hazards Above-Ground Tanks)[24 CFR Part 51 Subpart C	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	There is a current or planned stationary aboveground storage container of concern within 1 mile of the project site. The Separation Distance from the project is acceptable. The project is in compliance with explosive and flammable hazard requirements. A propane AST was identified on the subject property; this AST will be removed prior to development activities, and does not present a fire or explosion hazard to the proposed development. ASTs within one mile were evaluated using the HUD ASD assessment tool. The Project is located beyond the ASDs calculated.
Farmlands Protection Farmland Protection Policy Act of 1981, particularly sections 1504(b) and 1541; 7 CFR Part 658	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project does not include any activities that could potentially convert agricultural land to a non-agricultural use. The project consists of midtown-urban land . The project is in compliance with the Farmland Protection Policy Act.
Floodplain Management Executive Order 11988, particularly section 2(a); 24 CFR Part 55	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project does not occur in a floodplain. The Project is located in Zone X (minimal flood hazard). Refer to attached FEMA panel # 26163C0280E, effective date 2/2/2012. The project is in compliance with Executive Order 11988.

<p>Historic Preservation National Historic Preservation Act of 1966, particularly sections 106 and 110; 36 CFR Part 800</p>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<p>Based on historic research, The project is directly east of St. Joseph's Episcopal Church (now Holy Rosary Roman Catholic Church) and directly across Harper from a small portion of the Piquette Avenue Industrial Historic District, both of which are listed in the National Register of Historic Places. Therefore, per Stipulation V.B of the PA, the project shall be carried out in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Properties. This project has been given a Conditional No Adverse Effect determination (Federal Regulations 36 CFR Part 800.5(b)) on properties that are listed or eligible for listing in the National Register of Historic Places, as long as the following conditions are met:</p> <ul style="list-style-type: none"> o The scope of work provided on 8/23/22 is followed, and any changes to the scope of work are submitted to the PS for additional review and approval. o In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed. o Photos of the completed work are submitted to the Preservation Specialist."
<p>Noise Abatement and Control Noise Control Act of 1972, as amended by the Quiet Communities Act of 1978; 24 CFR Part 51 Subpart B</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<p>A Noise Assessment was conducted. The noise level was normally unacceptable: 69.0 db. See noise analysis. Since the DNL is in excess of 65 decibels, mitigation is proposed, including the use of building materials as barriers to mitigate noise. There are no outdoor noise sensitive areas. The HUD STraCAT electronic tool was utilized to conduct a site specific noise assessment. The assessment indicated the wall assemblies meet required attenuation. The project is in compliance with HUD's Noise regulation with mitigation. Mitigation measures include incorporating appropriate construction materials in the building to mitigate</p>

		noise levels within the acceptable range. Construction components include 2X4 wood studs, 16"o.c., 5/8" gypsum board, dual pane low E windows, 4" face brick.
Sole Source Aquifers Safe Drinking Water Act of 1974, as amended, particularly section 1424(e); 40 CFR Part 149	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project is not located on a sole source aquifer area. There are no sole source aquifers in Michigan. The project is in compliance with Sole Source Aquifer requirements.
Wetlands Protection Executive Order 11990, particularly sections 2 and 5	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	The project will not impact on- or off-site wetlands. There are no wetlands on the Project property or adjoining properties. The project is in compliance with Executive Order 11990.
Wild and Scenic Rivers Act Wild and Scenic Rivers Act of 1968, particularly section 7(b) and (c)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	This project is not within proximity of a NWSRS river. The project is in compliance with the Wild and Scenic Rivers Act.
HUD HOUSING ENVIRONMENTAL STANDARDS		
ENVIRONMENTAL JUSTICE		
Environmental Justice Executive Order 12898	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Adverse environmental impacts are not disproportionately high for low-income and/or minority communities. The Project does not create adverse environmental or human health impacts. The mitigation measures approved by EGLE in the ResAP will be implemented to mitigate potential human exposures, including excavation (removal) of soiled followed by verification sampling to confirm the contaminants have been removed. Therefore, the project is in compliance with Executive Order 12898.

Environmental Assessment Factors [24 CFR 58.40; Ref. 40 CFR 1508.8 &1508.27]

Impact Codes: An impact code from the following list has been used to make the determination of impact for each factor.

- (1) Minor beneficial impact
- (2) No impact anticipated
- (3) Minor Adverse Impact – May require mitigation

(4) Significant or potentially significant impact requiring avoidance or modification which may require an Environmental Impact Statement.

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
LAND DEVELOPMENT			
Conformance with Plans / Compatible Land Use and Zoning / Scale and Urban Design	1	This project conforms to the revitalization efforts currently being put forth in the City of Detroit. The proposed Project is not known to be in conflict with any land use policy, plan, or regulation. The proposed building will go through the site plan approval processes with the City of Detroit's Planning and Development Commission, ensuring that the building features architectural measures that align with the historical nature of the community as well as the new age green initiatives to lighten the load on the public utility system. The architectural firm completed the design and planning of the development, paying special attention to the City's standards of design, as well as the neighborhood's features. Further, the new building will put an end to a once blighted and now vacant lot in a premier location in the neighborhood. The Project is comprised of approximately 1.5 acres of land zoned as B4 (general business district) and R6 (High Density Residential District). The Proposed project complies with existing zoning regulations. The existing land use is vacant land, currently used for vehicle parking. The adjacent properties include mixed use residential and commercial developments, and is compatible with proposed mixed use. The Project location will not contribute to urban sprawl, rather redevelop a currently vacant area surrounded by urban development. Refer to Project Plans.	
Soil Suitability / Slope/ Erosion / Drainage and Storm Water Runoff	2	The project area is suitable for development. The area has supported residential and commercial development for over 100 years. According to the EGLE GeoWebFace database, in this area of Detroit, quaternary geology consists of lacustrine clay and silt,	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		<p>bedrock geology is composed of Traverse Group. No sloping issues will be caused by this development. Based on a review of the USGS topographic map, the slope at the subject property would be considered optimum for a residential development. Erosion will not be an issue during development. The developer will comply with state and county soil erosion regulations and manage erosive soils during development. The site storm water is serviced by the City of Detroit. Storm sewers in this area of Detroit are combined sewer, with no discharge to surface water. During new construction, appropriate storm water management practices will be implemented as required in accordance with Wayne County and City of Detroit regulations. The attached C2.0 calls out storm sewer sizes (ranging from 6" roof drain connections up to 84" subsurface storage chambers). Refer to previously uploaded site plans and Phase I and II ESA report.</p>	
Hazards and Nuisances including Site Safety and Site-Generated Noise	2	<p>The site is not in an area which is expected to be influenced by natural hazards (i.e. not in an area prone to earthquakes, flooding, hurricanes). Potential man-made site hazards are mainly associated with the uninhabited nature of the site which included inadequate street lighting and vegetation overgrowth. This development will improve this neighborhood by replacing a formerly blighted area with new construction that has adequate lighting for residents. Temporary construction phase noise will be mitigated by standard procedures. Appropriate construction materials will be incorporated in the building to mitigate noise levels within the acceptable range. Construction components include 2X4 wood studs, 16"o.c., 5/8" gypsum board, dual pane low E windows, 4" face brick. The project is not in close proximity to air pollution generators</p>	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		(i.e. heavy industry, cement plants, oil refineries). Refer to previously uploaded Phase I ESA report. Refer to previously uploaded noise assessment and HUD STraCAT documentation.	
SOCIOECONOMIC			
Employment and Income Patterns	1	Based on MHT's previous experience in projects with a comparable size and scope, this project anticipates creating 75 temporary construction jobs along with at least three full-time positions within the property. These full-time positions will include a full-time property manager, a part time leasing agent, a full-time maintenance supervisor, and a part time maintenance technician. The project will increase the City of Detroit tax base with the redevelopment of vacant under-utilized parcels including mixed-income housing. The addition of limited commercial use will also create job opportunities. (Project Narrative).	
Demographic Character Changes / Displacement	1	The subject property is located in the Medbury Park Neighborhood area of the City of Detroit. Although once a well-developed residential area, much of this area has seen a history of vacant or blighted properties. Development provides a safe, usable option. Proposed development activities will have a positive impact on the character of the community. No reduction or significant alternation of racial, ethnic or income attributes will occur. The development will not be a hindrance for access to local services or institutions. The project will not introduce barriers that would isolate a particular neighborhood or population group, nor will it destroy or harm any community institution. Residents will not be displaced as a result of the project. Rather, the project provides housing, and offers limited commercial services. (Field Observations, Google Maps, Project Plans).	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
Environmental Justice EA Factor	1	Adverse environmental impacts are not disproportionately high for low-income and/or minority communities. The Project does not create adverse environmental or human health impacts. The mitigation measures approved by EGLE in the ResAP will be implemented to mitigate potential human exposures, including excavation (removal) of soiled followed by verification sampling to confirm the contaminants have been removed. Removing existing contamination will benefit the site and the area as it removes potential harmful materials from the site.	
COMMUNITY FACILITIES AND SERVICES			
Educational and Cultural Facilities (Access and Capacity)	2	The City of Detroit provides several educational options through the Detroit Public School System. Additional private and charter school options are also available throughout the City. The proposed project will not negatively impact local educational facilities; redevelopment would not tax existing capacities. (Field observations, www.detroitk12.org). The City of Detroit Art Institute, Detroit Public Library, Detroit Symphony Orchestra, Opera House and Detroit Historical Museum are examples of cultural facilities accessible by bus route. (Field observations, Google Maps).	
Commercial Facilities (Access and Proximity)	2	The project site is located near several main corridors containing restaurants, automotive service shops, professional services, and pharmacies. The proposed project will not negatively impact local commercial facilities; development would not tax existing capacities and would provide additional customers to benefit local business. There are several locations of employment within 3 miles of the site. All are readily accessible by bus, car, and other modes of transportation. These include Eastern Market, Detroit Medical Center, Comerica Park, Ford Field, Little Caesars Arena and many small	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		businesses in the surrounding districts. (Field observations, Google Maps).	
Health Care / Social Services (Access and Capacity)	2	Future residents will also benefit greatly from the availability of new services that will be made available that otherwise may not be if not for the construction of this development. Specifically, ownership has entered into an MOU to provide for the availability of the healthcare services to be provided on-site and has also received a commitment from a local workforce training center that is expanding its operations to allow for inclusion of residents of 60 Harper. There are numerous health care facilities in the area-- including the Detroit Medical Center less than one mile from the site, offering hospital and physician services. These facilities provide access to physicians, emergency services, and/or specialized medical clinics. Several public health services are located within 2 miles of the site, including the Wayne County Department of Health, Veterans and Community Wellness, Wayne County Mental Health Services, Public Health and Human Services and Children's Health Services. (Google Maps). Churches, community centers, senior services, and daycare centers are located within two (2) miles of the project. Other social services are accessible via the DDOT bus system (see Transportation and Accessibility below). Proposed redevelopment will not negatively impact social services or unduly tax existing capacities. (Google Maps).	
Solid Waste Disposal and Recycling (Feasibility and Capacity)	2	No organized solid waste disposal activities were identified as the site is vacant. There are no buildings and no demolition debris. During construction, contracted disposal containers will be used for trash collection which will be emptied/removed by a contracted collection service. The City of Detroit is responsible for solid waste disposal activities at the site. Following construction, contracted disposal	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		containers will be used for trash collection which will be emptied/removed by a contracted collection service. (www.detroitmi.gov)	
Waste Water and Sanitary Sewers (Feasibility and Capacity)	2	The City of Detroit provides waste water sanitary sewer services to the site. The proposed residential use will not negatively impact the local wastewater treatment facility; development would not tax existing capacities. (www.dwsd.org). The attached C1.0 calls out the sizes of the sanitary lateral piping (6" san. lat.).	
Water Supply (Feasibility and Capacity)	2	The attached C1.0 calls out the sizes of the water services (4" wat domestic, 6" wat fire protection). The water supply is provided and maintained by the Detroit Water and Sewerage Department (DWSD) and the Great Lakes Water Authority (GLWA). According to the DWSD 2022 Water Quality Report, no water contaminants were detected above the allowable levels and no violations were reported. The proposed project will not negatively impact local water supply; development would not tax existing capacities. The project will not result in alteration of the course of a stream or river in a manner that could potentially result in substantial erosion or siltation on- or off-site, or result in downstream flooding. There are no sole source aquifers in the City of Detroit. (Designated Sole Source Aquifers in EPA Region 5 Map and http://www.detroitmi.gov).	
Public Safety - Police, Fire and Emergency Medical	2	The City of Detroit provides Police services to the area. The Wayne State University Police Department and 3rd Precinct and 8th Precinct are located less than one mile from the Project. (Google Maps). The City of Detroit provides Fire services to the area. There are several fire stations within five (5) miles of the development: Detroit Fire Engine 17 Ladder 7 is located 1/4 mile to the west of the project. (Google Maps). The City of Detroit	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		provides full Emergency Medical services to the area. Ambulance services are provided by 911 assistance. There are several hospitals and emergency facilities located within 2 miles of the development. (Google Maps). Proposed redevelopment will not negatively impact local emergency medical facilities.	
Parks, Open Space and Recreation (Access and Capacity)	2	Recreation centers are located within two (2) miles of the development including the Williams Recreation Center, The Fresh Start Organization / Considine Little Rock Rec Center, and the Mort Harris Recreation/Fitness Center. The proposed development is located within three miles of the Detroit Riverwalk as well as several parks, accessible by walking, car, or bus. The proposed redevelopment will not negatively impact community recreation services. (Google Maps).	
Transportation and Accessibility (Access and Capacity)	2	The project is located in a developed urban area. The proposed redevelopment will only minimally increase the traffic in the area. The development is considered to be located in a "walker's paradise" area. In addition to the great walkability, there are a multitude of public transit opportunities within 1/3 mile of the project location. The City of Detroit's Q-Line has a stop just north on Woodward about 450 feet from the site as well as a bus stop just south on Woodward approx. 350 feet from the site. (Google Maps, DDOT Bus Schedules, Q-Line Detroit).	
NATURAL FEATURES			
Unique Natural Features /Water Resources	2	The subject site and surrounding land have been fully developed as Urban Land since circa late 1800's. The site use is not agricultural and the proposed actions will not impact agricultural land. No unique natural features or areas (lakes, rivers, streams, wetlands) are located on or near the subject site. (Field observations, Figures, and aeriels in Phase I ESA). Municipal water and sewer service are provided at the site. The project	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		will not deplete groundwater supplies or interfere with groundwater recharge and will not result in alteration of the course of a stream or river in a manner that could potentially result in substantial erosion or siltation on or off-site or result in downstream flooding. There are no sole source aquifers or natural water resources located at the subject site. (Previously uploaded Sole Source aquifer and wild-scenic river maps).	
Vegetation / Wildlife (Introduction, Modification, Removal, Disruption, etc.)	2	Based on a review of Wayne County and US Fisheries and Wildlife Services information, two endangered species and four threatened species were identified in Wayne County; no critical habitat was identified on the Site. In addition, proposed plans for the site will have no effect on migratory birds or the bald eagle. (US Fish and Wildlife Services Wayne County Endangered Species list.) The site is currently vacant with some overgrown vegetation which promotes illicit dumping and a safety/security concern. (Visual observations)	
Other Factors 1			
Other Factors 2			
CLIMATE AND ENERGY			
Climate Change	2	The project is not located in an area of potential natural hazards (i.e., hurricanes, flooding, drought, wildfire, etc.). The Project area is not reliant on a sole source aquifer. The project is in an area of potential extreme cold and heat conditions. The Project incorporates shelter from extreme weather conditions, including energy efficient heating and cooling, insulated windows. Sustainability requirements are incorporated into building design to ensure the project will be resilient to future conditions and reduce the projects impact on the environment.	
Energy Efficiency	2	The proposed buildings will go through rigorous site plan approval processes with the City of Detroit's Planning and Development	

Environmental Assessment Factor	Impact Code	Impact Evaluation	Mitigation
		<p>Commission, ensuring that the building features architectural measures that align with the nature of the community as well as the new age green initiatives to lighten the load on the public utility system. All units will feature energy efficient appliances, and building in accordance with Enterprise Green Criteria. Refer to attached Site Plans for Sustainability Requirements Based on the proposed site use as residential with limited commercial use, energy consumption will be consistent with use in the surrounding area. According to the Michigan Public Service Commission (MPSC), DTE is the provider for electricity and natural gas at the subject site. There are no plans that would substantially increase energy consumption for the area. Refer to site plans.</p>	

Supporting documentation

[SHEET C2 GRADING DRAINAGE AND SESC PLAN storm water.pdf](#)

[SHEET C1 SITE PLAN Sanitary and Water.pdf](#)

[60 HARPER - FOR PERMITS - COMBINED PLAN SET \(05-05-23\).pdf](#)

[Figure 1 Site Location and Figure 2 Aerial\(1\).pdf](#)

[60 Harper Site Plans\(1\).pdf](#)

[60 Harper - Project Narrative\(1\).pdf](#)

[bus stop and Q line- Google Maps.pdf](#)

[education - Google Maps.pdf](#)

[cultural facility - Google Maps.pdf](#)

[commercial facilities - google maps.pdf](#)

[recreation center - Google Maps.pdf](#)

[public health - Google Maps.pdf](#)

[police department - Google Maps.pdf](#)

[hospitals - Google Maps.pdf](#)

[health care - Google Maps.pdf](#)

[fire department - Google Maps.pdf](#)

Additional Studies Performed:

Phase I ESA, Phase II ESAs, Baseline Environmental Assessment, Response Activity Plan

Field Inspection [Optional]: Date and completed

by:

Maura Gibbons

3/2/2023 12:00:00 AM

[60 Harper - Project Narrative.pdf](#)[60 Harper Site Plans.pdf](#)[Phase I Photographs.pdf](#)**List of Sources, Agencies and Persons Consulted [40 CFR 1508.9(b)]:**

HUD Exchange, State Historic Preservation Office, Federal Emergency Management Agency (FEMA), Michigan Department of Environment, Great Lakes and Energy (EGLE), Michigan Department of Natural Resources, National Wetlands Inventory (NWI), United States Fisheries and Wildlife (USFWS), United States Environmental Protection Agency Water Management Division, Region V, Client Provided Documentation, City of Detroit, Wayne County, Google Maps

List of Permits Obtained:

Any permits necessary associated with the proposed development activities would be obtained in accordance with state and local requirements.

Public Outreach [24 CFR 58.43]:

Dissemination and/or publication of the findings will be made by the RE as applicable.

Cumulative Impact Analysis [24 CFR 58.32]:

There is no negative cumulative impact on the environment that would result from proposed site redevelopment activities. The Project will provide affordable housing in an area that is experiencing growth and has a need for affordable housing. The Project will allow lower income wage earners to have access to amenities that they otherwise might not have access to. In addition to the current amenities and opportunities that exist in this project location, there are millions of dollars of planned future investment for this area, making this a great location to live in and provide affordable housing resources.

Alternatives [24 CFR 58.40(e); 40 CFR 1508.9]

Variations in site development specifics have been proposed to accommodate demands. The proposed project will have a positive impact with the City of Detroit and is intended to return vacant properties to the tax base with mixed income housing. Various options and locations for development were considered. MHT and the AOD worked closely to close on the Cathedral Arts Apartments which is currently under construction on Woodward. The partnership and process went well and the

AOD was looking to develop more vacant land. The subject property was previously owned by St. Aloysius Church and was sitting as an undeveloped vacant lot. The AOD asked MHT to come in and co-develop the property. The location is near Woodward in the heart of the City and is a fantastic location nestled next to the Church as well as walkable to many amenities. The development team worked closely with various City of Detroit departments like Planning and Development and Building, Safety, Engineering and Environmental (BSEED) to do a design review to ensure that alternative designs were considered and feedback was given in order to put together a development that follows the proper City code and processes and also provides much needed quality affordable housing.

No Action Alternative [24 CFR 58.40(e)]

One alternative is No Action. The No Action alternative would be to allow the subject property to remain vacant underutilized land in the City of Detroit. No distinguishable benefits to the human environment would be gained by not choosing to initiate the project. The potential adverse impacts to the human environment of not implementing the project include ongoing security of vacant overgrown properties, potential for illicit dumping, potential as an attractive nuisance, and potential depreciation of surrounding residential homes and properties.

Summary of Findings and Conclusions:

The proposed project as designed will not result in a significant impact on the quality of the human environment. The redevelopment of underutilized vacant land will offer a great opportunity to provide affordable housing units in an area that is experiencing tremendous growth and has a need for affordable housing. The project is an opportunity to improve the quality of life for local residents and create a stronger, more connected community. The housing element of the project centers on the development of mixed-income housing, creating new housing opportunities for current and future Detroit residents across a wide range of household incomes. The wide range of incomes targeted in this development will allow for a diverse community and new housing options for families at every income level. Additionally, the development will replace vacant land with high-quality, modern housing built within the existing landscape. The development will provide stability and safety to the area, as well as desired commercial/retail opportunities within the neighborhood. Future residents will also benefit greatly from the availability of new services that will be made available that otherwise may not be if not for the construction of this development.

Mitigation Measures and Conditions [CFR 1505.2(c)]:

Summarized below are all mitigation measures adopted by the Responsible Entity to reduce, avoid or eliminate adverse environmental impacts and to avoid non-compliance or non-conformance with the above-listed authorities and factors. These measures/conditions must be incorporated into project contracts, development agreements and other relevant documents.

The staff responsible for implementing and monitoring mitigation measures should be clearly identified in the mitigation plan.

Law, Authority, or Factor	Mitigation Measure or Condition	Comments on Completed Measures	Mitigation Plan	Complete
Historic Preservation	<p>This project has been given a Conditional No Adverse Effect determination (Federal Regulations 36 CFR Part 800.5(b)) on properties that are listed or eligible for listing in the National Register of Historic Places, as long as the following conditions are met:</p> <p>1) The scope of work provided on 8/23/22 is followed, and any changes to the scope of work are submitted to the PS for additional review and approval.</p> <p>2) In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed.</p> <p>3) Photos of the completed work are submitted to the Preservation Specialist."</p>	N/A	<p>The following conditions will be met:</p> <p>1) The final scope of work is submitted to the Preservation Specialist for review and any changes to the scope of work for the project shall be submitted to the Preservation Specialist for review and approval prior to the start of work.</p> <p>2) In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed.</p> <p>3) Photos of the completed work are submitted to</p>	

			the Preservation Specialist.	
Contamination and Toxic Substances	A Response Activity Plan for due car compliance was prepared to ensure that existing contamination on the property does not cause unacceptable exposures and assures the safe use of the property. The response activity to address potential direct contact and volatilization to indoor air exposures is remedial excavation.	N/A	The RespAP summarizes mitigations to prevent potential unacceptable human exposures.	
Noise Abatement and Control	Since the DNL is in excess of 65 decibels, mitigation is proposed, including the use of building materials as barriers to mitigate noise. There are no outdoor noise sensitive areas. The HUD STraCAT electronic tool was utilized to conduct a site specific noise assessment. The assessment indicated the wall assemblies meet required attenuation.	N/A	Building materials will be used to mitigate potential human exposures for noise.	

Project Mitigation Plan

Refer to the attached Mitigation Plan for a summary of the response activity or continuing obligation, required activities, responsible party, timing, costs and required follow-up.

[60 Harper Mitigation Plan From HMA - Revised.docx](#)

Supporting documentation on completed measures

APPENDIX A: Related Federal Laws and Authorities

Airport Hazards

General policy	Legislation	Regulation
It is HUD's policy to apply standards to prevent incompatible development around civil airports and military airfields.		24 CFR Part 51 Subpart D

1. To ensure compatible land use development, you must determine your site's proximity to civil and military airports. Is your project within 15,000 feet of a military airport or 2,500 feet of a civilian airport?

✓ No

Based on the response, the review is in compliance with this section. Document and upload the map showing that the site is not within the applicable distances to a military or civilian airport below

Yes

Screen Summary

Compliance Determination

The project site is not within 15,000 feet of a military airport or 2,500 feet of a civilian airport. The distance to the closest airport (Coleman A. Young CAY) is approximately 4.3 miles northeast. The project is in compliance with Airport Hazards requirements.

Supporting documentation

[Airport Map - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

✓ No

Coastal Barrier Resources

General requirements	Legislation	Regulation
HUD financial assistance may not be used for most activities in units of the Coastal Barrier Resources System (CBRS). See 16 USC 3504 for limitations on federal expenditures affecting the CBRS.	Coastal Barrier Resources Act (CBRA) of 1982, as amended by the Coastal Barrier Improvement Act of 1990 (16 USC 3501)	

1. Is the project located in a CBRS Unit?

No

Document and upload map and documentation below.

Yes

Compliance Determination

This project is not located in a CBRS Unit. Therefore, this project has no potential to impact a CBRS Unit and is in compliance with the Coastal Barrier Resources Act.

Supporting documentation

[coastal barrier resource system map - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Flood Insurance

General requirements	Legislation	Regulation
Certain types of federal financial assistance may not be used in floodplains unless the community participates in National Flood Insurance Program and flood insurance is both obtained and maintained.	Flood Disaster Protection Act of 1973 as amended (42 USC 4001-4128)	24 CFR 50.4(b)(1) and 24 CFR 58.6(a) and (b); 24 CFR 55.1(b).

1. Does this project involve financial assistance for construction, rehabilitation, or acquisition of a mobile home, building, or insurable personal property?

No. This project does not require flood insurance or is excepted from flood insurance.

Yes

2. Upload a FEMA/FIRM map showing the site here:

[FEMA Firmette - 60 Harper.pdf](#)

The Federal Emergency Management Agency (FEMA) designates floodplains. The [FEMA Map Service Center](#) provides this information in the form of FEMA Flood Insurance Rate Maps (FIRMs). For projects in areas not mapped by FEMA, use the best available information to determine floodplain information. Include documentation, including a discussion of why this is the best available information for the site. Provide FEMA/FIRM floodplain zone designation, panel number, and date within your documentation.

Is the structure, part of the structure, or insurable property located in a FEMA-designated Special Flood Hazard Area?

No

Based on the response, the review is in compliance with this section.

Yes

4. While flood insurance is not mandatory for this project, HUD strongly recommends that all insurable structures maintain flood insurance under the National Flood Insurance Program (NFIP). Will flood insurance be required as a mitigation measure or condition?

Yes

✓ No

Screen Summary**Compliance Determination**

The structure or insurable property is not located in a FEMA-designated Special Flood Hazard Area. The Project is located in Zone X - Area of Minimal Flood Hazard. The project is in compliance with flood insurance requirements. Refer to attached FEMA panel # 26163C0280E, effective date 2/2/2012.

Supporting documentation

[FEMA Firmette - 60 Harper\(1\).pdf](#)

Are formal compliance steps or mitigation required?

Yes

✓ No

Air Quality

General requirements	Legislation	Regulation
The Clean Air Act is administered by the U.S. Environmental Protection Agency (EPA), which sets national standards on ambient pollutants. In addition, the Clean Air Act is administered by States, which must develop State Implementation Plans (SIPs) to regulate their state air quality. Projects funded by HUD must demonstrate that they conform to the appropriate SIP.	Clean Air Act (42 USC 7401 et seq.) as amended particularly Section 176(c) and (d) (42 USC 7506(c) and (d))	40 CFR Parts 6, 51 and 93

1. Does your project include new construction or conversion of land use facilitating the development of public, commercial, or industrial facilities OR five or more dwelling units?

Yes

No

Air Quality Attainment Status of Project's County or Air Quality Management District

2. Is your project's air quality management district or county in non-attainment or maintenance status for any criteria pollutants?

No, project's county or air quality management district is in attainment status for all criteria pollutants.

Yes, project's management district or county is in non-attainment or maintenance status for the following criteria pollutants (check all that apply):

Carbon Monoxide

Lead

Nitrogen dioxide

Sulfur dioxide

- ✓ Ozone
- Particulate Matter, <2.5 microns
- Particulate Matter, <10 microns

3. What are the *de minimis* emissions levels (40 CFR 93.153) or screening levels for the non-attainment or maintenance level pollutants indicated above

Ozone 100.00 ppb (parts per million)

Provide your source used to determine levels here:

EPA General Conformity De Minimis Tables <https://www.epa.gov/general-conformity/de-minimis-tables>

4. Determine the estimated emissions levels of your project. Will your project exceed any of the *de minimis* or threshold emissions levels of non-attainment and maintenance level pollutants or exceed the screening levels established by the state or air quality management district?

- ✓ No, the project will not exceed *de minimis* or threshold emissions levels or screening levels.

Enter the estimate emission levels:

Ozone ppb (parts per million)

Based on the response, the review is in compliance with this section.

Yes, the project exceeds *de minimis* emissions levels or screening levels.

Screen Summary

Compliance Determination

The project's county or air quality management district is in non-attainment status for the following: Ozone. EGLE is currently working to complete the required SIP submittals for this area; therefore, an alternative evaluation was completed to assess conformity. Specifically, EGLE considered the following information from the United States Environmental Protection Agency's (USEPA) general conformity guidance, which states "historical analysis of similar actions can be used in cases where the

proposed projects are similar in size and scope to previous projects." EGLE has reviewed the project proposed to be located at 60 Harper in Detroit, Michigan, it was determined that based on the size, scope and duration of the property project, emission levels for the project should not exceed de minimis levels for general conformity. The project is in compliance with the Clean Air Act. As of July 2023, the ozone area in Wayne County is in attainment/maintenance status.

Supporting documentation

[Attachment - Revised Air Quality Map 2023.pdf](#)

[AQD General Conformity - 60 Harper.pdf](#)

[naaqs-ambient-status-map - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Coastal Zone Management Act

General requirements	Legislation	Regulation
Federal assistance to applicant agencies for activities affecting any coastal use or resource is granted only when such activities are consistent with federally approved State Coastal Zone Management Act Plans.	Coastal Zone Management Act (16 USC 1451-1464), particularly section 307(c) and (d) (16 USC 1456(c) and (d))	15 CFR Part 930

1. Is the project located in, or does it affect, a Coastal Zone as defined in your state Coastal Management Plan?

Yes

No

Based on the response, the review is in compliance with this section. Document and upload all documents used to make your determination below.

Screen Summary

Compliance Determination

This project is not located in or does not affect a Coastal Zone as defined in the state Coastal Management Plan. The project is in compliance with the Coastal Zone Management Act.

Supporting documentation

[Coastal Zone Boundary Map - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Contamination and Toxic Substances

General requirements	Legislation	Regulations
It is HUD policy that all properties that are being proposed for use in HUD programs be free of hazardous materials, contamination, toxic chemicals and gases, and radioactive substances, where a hazard could affect the health and safety of the occupants or conflict with the intended utilization of the property.		24 CFR 58.5(i)(2) 24 CFR 50.3(i)

1. How was site contamination evaluated? Select all that apply. Document and upload documentation and reports and evaluation explanation of site contamination below.

- American Society for Testing and Materials (ASTM) Phase I Environmental Site Assessment (ESA)
- ASTM Phase II ESA
- Remediation or clean-up plan
- ASTM Vapor Encroachment Screening
- None of the Above

2. Were any on-site or nearby toxic, hazardous, or radioactive substances found that could affect the health and safety of project occupants or conflict with the intended use of the property? (Were any recognized environmental conditions or RECs identified in a Phase I ESA and confirmed in a Phase II ESA?)

No

- Yes

3. Mitigation

Document and upload the mitigation needed according to the requirements of the appropriate federal, state, tribal, or local oversight agency. If the adverse environmental effects cannot be mitigated, then HUD assistance may not be used for the project at this site.

Can adverse environmental impacts be mitigated?

Adverse environmental impacts cannot feasibly be mitigated.

- ✓ Yes, adverse environmental impacts can be eliminated through mitigation. Document and upload all mitigation requirements below.

4. Describe how compliance was achieved in the text box below. Include any of the following that apply: State Voluntary Clean-up Program, a No Further Action letter, use of engineering controls, or use of institutional controls.

A Response Activity Plan for due care compliance was prepared to ensure that existing contamination on the property does not cause unacceptable exposures and assures the safe use of the property. The response activity to address potential direct contact and volatilization to indoor air exposures is remedial excavation.

If a remediation plan or clean-up program was necessary, which standard does it follow?

Complete removal

- ✓ Risk-based corrective action (RBCA)

Screen Summary

Compliance Determination

There are no buildings/structures on the Project site; lead and asbestos determination was not applicable. The Project is located in Wayne County, MI: 17% of homes tested equal or greater to 4 pCi/L for radon. The Project is Wayne County, Zone 3 for Radon. Site contamination was evaluated as follows: ASTM Phase I ESA dated 8/18/2023, ASTM Vapor Encroachment Screening (Section 9.9 in Phase I ESA), ASTM Phase II ESA dated 5/30/2023, BEA completed 5/31/2023, and Response Activity Plan for Due Care Compliance dated 7/5/2023. On-site or nearby toxic, hazardous, or radioactive substances were found that could affect the health and safety of project occupants or conflict with the intended use of the property. The adverse environmental impacts will be mitigated through remedial excavation (removal). With mitigation, identified in the mitigation section of this review, the project will be in compliance with contamination and toxic substances requirements. Phase I ESA identified RECs including: 1) known impact and "facility" status from a release related to fill material and 2) potential vapor encroachment from surrounding properties. Phase II ESA was conducted to assess the RECs; soil and groundwater samples were collected for analysis. The assessment results have indicated the presence of VOCs, PNAs and metals at concentrations exceeding their respective Residential GRCC or VIAP SLs. Specifically, soil samples collected from

the subject property identified the following: concentrations of benzene, 2-methylnaphthalene, naphthalene, 1,2,4-trimethylbenzene, 1,3,5-trimethylbenzene and/or xylenes exceeded their respective EGLE Residential Soil VIAP SL in soil samples SB-2 (1.0'), SB-5 (1.0'), SB-16 (2'-3') and SB-26 (3'-4'). Benzo(a)pyrene exceeded EGLE GRCC for Direct Contact at SB-2 (1.0'), SB-8 (2.0'), and SB-26 (3'-4'). Exceedances of the EGLE Residential VIAP SL for phenanthrene were detected SB-1 (0.5'), SB-2 (1.0'), SB-2 (2.0'), SB-6 (1.5'), SB-8 (2.0'), and SB-26 (3'-4'). Exceedances of VIAP SL for naphthalene were detected at SB-2 (1.0'), SB-2 (2.0'), and SB-26 (3'-4'). Lead exceeded EGLE GRCC for Direct Contact in one soil sample, SB-11 (5'). Multiple soil samples exceed VIAP SL for mercury at the Subject Property at depths mainly ranging from 0.5 to 4.5 feet bgs. A Response Activity Plan was prepared, proposing remedial excavation to remove impacted soils followed by verification sampling. A copy of the EGLE Notice of Approval of Response Activity letter dated 8/17/2023 is included as an attachment. Mitigation steps summarized in the RespAP include the following: 1) Removal of impacted soil across the subject property, mainly non-native urban fill material. In locations of the proposed building foundation depths of material to be removed are approximately 5 - 6 feet. Across the remaining portions of the Subject Property depths are approximately 3 feet. 2) Verification of remediation Sampling will be conducted at bottom of excavation depths across the Subject Property to ensure that adequate impacted soil has been removed and clean soil remains. Soil will be analyzed by laboratory prior to clean backfill material being placed at the Subject Property. 3) The fill material brought to the site will be documented as clean by analytical results from samples collected from the site of origin documenting that the material does not contain volatile organic compounds, polynuclear aromatic hydrocarbons, or Michigan Ten Metals at concentrations above the applicable generic cleanup criteria.

Supporting documentation

[MSHDA Phase I ESA Harper Apartments Detroit 81823.pdf](#)
[60 Harper Apartments BEA 05312023 w EGLE acknowledgment.pdf](#)
[73-08 Harper ResAP Final July 5-2023.pdf](#)
[60 Harper Avenue RespAP -7a\(1\)\(b\) Approval Letter.pdf](#)
[Map-Of-Michigan-Radon-Levels.pdf](#)
[HMA Phase II ESA 60-Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Endangered Species

General requirements	ESA Legislation	Regulations
Section 7 of the Endangered Species Act (ESA) mandates that federal agencies ensure that actions that they authorize, fund, or carry out shall not jeopardize the continued existence of federally listed plants and animals or result in the adverse modification or destruction of designated critical habitat. Where their actions may affect resources protected by the ESA, agencies must consult with the Fish and Wildlife Service and/or the National Marine Fisheries Service (“FWS” and “NMFS” or “the Services”).	The Endangered Species Act of 1973 (16 U.S.C. 1531 <i>et seq.</i>); particularly section 7 (16 USC 1536).	50 CFR Part 402

1. Does the project involve any activities that have the potential to affect species or habitats?

No, the project will have No Effect due to the nature of the activities involved in the project.

No, the project will have No Effect based on a letter of understanding, memorandum of agreement, programmatic agreement, or checklist provided by local HUD office

- ✓ Yes, the activities involved in the project have the potential to affect species and/or habitats.

2. Are federally listed species or designated critical habitats present in the action area?

No, the project will have No Effect due to the absence of federally listed species and designated critical habitat

- ✓ Yes, there are federally listed species or designated critical habitats present in the action area.

3. What effects, if any, will your project have on federally listed species or designated critical habitat?

- ✓ No Effect: Based on the specifics of both the project and any federally listed species in the action area, you have determined that the project will have absolutely no effect on listed species or critical habitat. in the action area.

Document and upload all documents used to make your determination below. Documentation should include a species list and explanation of your conclusion, and may require maps, photographs, and surveys as appropriate

May Affect, Not Likely to Adversely Affect: Any effects that the project may have on federally listed species or critical habitats would be beneficial, discountable, or insignificant.

Likely to Adversely Affect: The project may have negative effects on one or more listed species or critical habitat.

6. For the project to be brought into compliance with this section, all adverse impacts must be mitigated. Explain in detail the exact measures that must be implemented to mitigate for the impact or effect, including the timeline for implementation. This information will be automatically included in the Mitigation summary for the environmental review. If negative effects cannot be mitigated, cancel the project using the button at the bottom of this screen.

Mitigation as follows will be implemented:

- ✓ No mitigation is necessary.

Explain why mitigation will not be made here:

There is no potential habitat in the project area; the project is urban infill.

Screen Summary
Compliance Determination

This project is located in the highly urbanized and highly redeveloped area of the City of Detroit. There are no designated critical habitats in the action area, the project is urban infill, and the project is redevelopment of previously developed parcels in a heavily developed urban area. This project has been determined to have No Effect on listed species. This project is in compliance with the Endangered Species Act.

Supporting documentation

[Species List_Michigan Ecological Services Field Office.pdf](#)

Are formal compliance steps or mitigation required?

Yes

✓ No

Explosive and Flammable Hazards

General requirements	Legislation	Regulation
HUD-assisted projects must meet Acceptable Separation Distance (ASD) requirements to protect them from explosive and flammable hazards.	N/A	24 CFR Part 51 Subpart C

1. Is the proposed HUD-assisted project itself the development of a hazardous facility (a facility that mainly stores, handles or processes flammable or combustible chemicals such as bulk fuel storage facilities and refineries)?

No

Yes

2. Does this project include any of the following activities: development, construction, rehabilitation that will increase residential densities, or conversion?

No

Yes

3. Within 1 mile of the project site, are there any current or planned stationary aboveground storage containers that are covered by 24 CFR 51C? Containers that are NOT covered under the regulation include:

- Containers 100 gallons or less in capacity, containing common liquid industrial fuels OR
- Containers of liquified petroleum gas (LPG) or propane with a water volume capacity of 1,000 gallons or less that meet the requirements of the 2017 or later version of National Fire Protection Association (NFPA) Code 58.

If all containers within the search area fit the above criteria, answer "No." For any other type of aboveground storage container within the search area that holds one of the flammable or explosive materials listed in Appendix I of 24 CFR part 51 subpart C, answer "Yes."

No

Yes

4. Based on the analysis, is the proposed HUD-assisted project located at or beyond the required separation distance from all covered tanks?

Yes

Based on the response, the review is in compliance with this section.

No

Screen Summary

Compliance Determination

There is a current or planned stationary aboveground storage container of concern within 1 mile of the project site. The Separation Distance from the project is acceptable. The project is in compliance with explosive and flammable hazard requirements. A propane AST was identified on the subject property; this AST will be removed prior to development activities, and does not present a fire or explosion hazard to the proposed development. ASTs within one mile were evaluated using the HUD ASD assessment tool. The Project is located beyond the ASDs calculated.

Supporting documentation

[60 Harper Ave - Google Maps ASD distances.pdf](#)
[ASTs within One Mile - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Farmlands Protection

General requirements	Legislation	Regulation
The Farmland Protection Policy Act (FPPA) discourages federal activities that would convert farmland to nonagricultural purposes.	Farmland Protection Policy Act of 1981 (7 U.S.C. 4201 et seq.)	7 CFR Part 658

1. Does your project include any activities, including new construction, acquisition of undeveloped land or conversion, that could convert agricultural land to a non-agricultural use?

Yes

No

If your project includes new construction, acquisition of undeveloped land or conversion, explain how you determined that agricultural land would not be converted:

According to the USDA Web Soil Survey, the Project is located in Midtown-Urban land complex.

Based on the response, the review is in compliance with this section. Document and upload all documents used to make your determination below.

Screen Summary

Compliance Determination

This project does not include any activities that could potentially convert agricultural land to a non-agricultural use. The project consists of midtown-urban land . The project is in compliance with the Farmland Protection Policy Act.

Supporting documentation

[Soil Survey - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Floodplain Management

General Requirements	Legislation	Regulation
Executive Order 11988, Floodplain Management, requires federal activities to avoid impacts to floodplains and to avoid direct and indirect support of floodplain development to the extent practicable.	Executive Order 11988	24 CFR 55

1. Do any of the following exemptions apply? Select the applicable citation? [only one selection possible]

- 55.12(c)(3)
- 55.12(c)(4)
- 55.12(c)(5)
- 55.12(c)(6)
- 55.12(c)(7)
- 55.12(c)(8)
- 55.12(c)(9)
- 55.12(c)(10)
- 55.12(c)(11)

None of the above

2. Upload a FEMA/FIRM map showing the site here:

[FEMA Firmette - 60 Harper.pdf](#)

The Federal Emergency Management Agency (FEMA) designates floodplains. The FEMA Map Service Center provides this information in the form of FEMA Flood Insurance Rate Maps (FIRMs). For projects in areas not mapped by FEMA, use **the best available information** to determine floodplain information. Include documentation, including a discussion of why this is the best available information for the site.

Does your project occur in a floodplain?

No

Based on the response, the review is in compliance with this section.

Yes

Screen Summary**Compliance Determination**

This project does not occur in a floodplain. The Project is located in Zone X (minimal flood hazard). Refer to attached FEMA panel # 26163C0280E, effective date 2/2/2012. The project is in compliance with Executive Order 11988.

Supporting documentation

[FEMA Firmette - 60 Harper\(2\).pdf](#)

Are formal compliance steps or mitigation required?

Yes

✓ No

Historic Preservation

General requirements	Legislation	Regulation
Regulations under Section 106 of the National Historic Preservation Act (NHPA) require a consultative process to identify historic properties, assess project impacts on them, and avoid, minimize, or mitigate adverse effects	Section 106 of the National Historic Preservation Act (16 U.S.C. 470f)	36 CFR 800 "Protection of Historic Properties" https://www.govinfo.gov/content/pkg/CFR-2012-title36-vol3/pdf/CFR-2012-title36-vol3-part800.pdf

Threshold

Is Section 106 review required for your project?

No, because the project consists solely of activities listed as exempt in a Programmatic Agreement (PA). (See the PA Database to find applicable PAs.)

No, because the project consists solely of activities included in a No Potential to Cause Effects memo or other determination [36 CFR 800.3(a)(1)].

- ✓ Yes, because the project includes activities with potential to cause effects (direct or indirect).

Step 1 – Initiate Consultation

Select all consulting parties below (check all that apply):

- ✓ State Historic Preservation Offer (SHPO) Completed

- ✓ Indian Tribes, including Tribal Historic Preservation Officers (THPOs) or Native Hawaiian Organizations (NHOs)

- ✓ Bay Mills Indian Community Completed
- ✓ Forest County Potawatomi Community of Wisconsin Completed

✓ Grand Traverse Bay Ottawa & Chippewa	Completed
✓ Hannahville Indian Community	Completed
✓ Ketegitigaaning Ojibwe Nation/Lac Vieux Desert	Completed
✓ Keweenaw Bay Indian Community	Completed
✓ Lac du Flambeau Band	Completed
✓ Little River Band of Ottawa Indians	Completed
✓ Little Traverse Bay Bands of Odawa Indians	Completed
✓ Match-E-Be-Nash-She-Wish	Completed
✓ Menominee Indian Tribe of Wisconsin	Completed
✓ Miami Tribe of Oklahoma	Completed
✓ Michigan Anishinaabek Cultural	Completed
✓ Nottawaseppi Huron Band of the Potawatomi	Completed
✓ Pokagon Band of Potawatomi Indians	Completed
✓ Saginaw Chippewa Indian Tribe of Michigan	Completed
✓ Sault Ste. Marie Tribe of Chippewa Indians	Completed
✓ Seneca Cayuga Nation	Completed
✓ Other Consulting Parties	
✓ City of Detroit Preservation Specialist	Completed

Describe the process of selecting consulting parties and initiating consultation here:

Refer to attached HRD letter summarizing Tribes consulted. ACHP consultation is only required in instances of Adverse Effects.

Document and upload all correspondence, notices and notes (including comments and objections received below).

Was the Section 106 Lender Delegation Memo used for Section 106 consultation?

Yes

No

Step 2 – Identify and Evaluate Historic Properties

- 1. **Define the Area of Potential Effect (APE), either by entering the address(es) or uploading a map depicting the APE below:**

The APE for indirect effects is the entire site (both north and south of the alley) proposed for the physical project plus a half block to the north, south, west, and east.

In the chart below, list historic properties identified and evaluated in the APE. Every historic property that may be affected by the project should be included in the chart.

Upload the documentation (survey forms, Register nominations, concurrence(s) and/or objection(s), notes, and photos) that justify your National Register Status determination below.

Address / Location / District	National Register Status	SHPO Concurrence	Sensitive Information
-------------------------------	--------------------------	------------------	-----------------------

Additional Notes:

Refer to the attached reports.

- 2. **Was a survey of historic buildings and/or archeological sites done as part of the project?**

Yes

Document and upload surveys and report(s) below.
 For Archeological surveys, refer to HP Fact Sheet #6, Guidance on Archeological Investigations in HUD Projects.

Additional Notes:

Additional research was conducted on the 71 Edsel Ford building. Findings indicated the building was not eligible for individual listing in the NRHP.

No

Step 3 –Assess Effects of the Project on Historic Properties

Only properties that are listed on or eligible for the National Register of Historic Places receive further consideration under Section 106. Assess the effect(s) of the project by applying the

Criteria of Adverse Effect. (36 CFR 800.5)] Consider direct and indirect effects as applicable as per guidance on direct and indirect effects.

Choose one of the findings below - No Historic Properties Affected, No Adverse Effect, or Adverse Effect; and seek concurrence from consulting parties.

No Historic Properties Affected

No Adverse Effect

Based on the response, the review is in compliance with this section.

Document reason for finding:

The project will have No Adverse Effect on both the St. Joseph Episcopal Church and the Piquette Avenue Industrial Historic District which are listed in the NRHP. While modern in design, the proposed building is compatible in scale, size, and location to the historic district and church. The proposed building will not destroy any historic materials or character defining features of the St. Joseph Episcopal Church or the Piquette Avenue Industrial Historic District, neither of which are significant for their setting or views. The settings of both properties have been altered over the years with demolition and new construction in the area. Detroit HRD Preservation Specialist indicated a Conditional No Adverse Effect determination (see attached).

Does the No Adverse Effect finding contain conditions?

Yes (check all that apply)

Avoidance

Modification of project

Other

Describe conditions here:

This project has been given a Conditional No Adverse Effect determination (Federal Regulations 36 CFR Part 800.5(b)) on properties that are listed or eligible for listing in the National Register of Historic Places, as long as the

following conditions are met:

- 1) The scope of work provided on 8/23/22 is followed, and any changes to the scope of work are submitted to the PS for additional review and approval.
- 2) In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed.
- 3) Photos of the completed work are submitted to the Preservation Specialist."

No

Adverse Effect

Screen Summary

Compliance Determination

Based on historic research, The project is directly east of St. Joseph's Episcopal Church (now Holy Rosary Roman Catholic Church) and directly across Harper from a small portion of the Piquette Avenue Industrial Historic District, both of which are listed in the National Register of Historic Places. Therefore, per Stipulation V.B of the PA, the project shall be carried out in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Properties. This project has been given a Conditional No Adverse Effect determination (Federal Regulations 36 CFR Part 800.5(b)) on properties that are listed or eligible for listing in the National Register of Historic Places, as long at the following conditions are met: o The scope of work provided on 8/23/22 is followed, and any changes to the scope of work are submitted to the PS for additional review and approval. o In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed. o Photos of the completed work are submitted to the Preservation Specialist."

Supporting documentation

[ACCR archaeological summary 60 Harper Detroit 4423.pdf](#)

[SHPO Response Letter ER96 1 23 60 Harper.pdf](#)

[60 Harper CNAE Section 106 Letter 82423.pdf](#)

[60 Harper CNAE Section 106 Letter.pdf](#)

[60 Harper Section 106 report.pdf](#)

[71 Edsel Service Drive SHPO inventory form.pdf](#)

Are formal compliance steps or mitigation required?

✓ Yes

No

Noise Abatement and Control

General requirements	Legislation	Regulation
HUD's noise regulations protect residential properties from excessive noise exposure. HUD encourages mitigation as appropriate.	Noise Control Act of 1972 General Services Administration Federal Management Circular 75-2: "Compatible Land Uses at Federal Airfields"	Title 24 CFR 51 Subpart B

1. What activities does your project involve? Check all that apply:

- New construction for residential use

NOTE: HUD assistance to new construction projects is generally prohibited if they are located in an Unacceptable zone, and HUD discourages assistance for new construction projects in Normally Unacceptable zones. See 24 CFR 51.101(a)(3) for further details.

Rehabilitation of an existing residential property

A research demonstration project which does not result in new construction or reconstruction

An interstate land sales registration

Any timely emergency assistance under disaster assistance provision or appropriations which are provided to save lives, protect property, protect public health and safety, remove debris and wreckage, or assistance that has the effect of restoring facilities substantially as they existed prior to the disaster
None of the above

4. Complete the Preliminary Screening to identify potential noise generators in the vicinity (1000' from a major road, 3000' from a railroad, or 15 miles from an airport).

Indicate the findings of the Preliminary Screening below:

There are no noise generators found within the threshold distances above.

- ✓ Noise generators were found within the threshold distances.

5. **Complete the Preliminary Screening to identify potential noise generators in the**

Acceptable: (65 decibels or less; the ceiling may be shifted to 70 decibels in circumstances described in §24 CFR 51.105(a))

- ✓ Normally Unacceptable: (Above 65 decibels but not exceeding 75 decibels; the floor may be shifted to 70 decibels in circumstances described in §24 CFR 51.105(a))

Is your project in a largely undeveloped area?

- ✓ No

Document and upload noise analysis, including noise level and data used to complete the analysis below.

Yes

Unacceptable: (Above 75 decibels)

HUD strongly encourages conversion of noise-exposed sites to land uses compatible with high noise levels.

Check here to affirm that you have considered converting this property to a non-residential use compatible with high noise levels.

Document and upload noise analysis, including noise level and data used to complete the analysis below.

6. **HUD strongly encourages mitigation be used to eliminate adverse noise impacts. Explain in detail the exact measures that must be implemented to mitigate for the impact or effect, including the timeline for implementation. This information will be automatically included in the Mitigation summary for the environmental review.**

- ✓ Mitigation as follows will be implemented:

Since the DNL is in excess of 65 decibels, mitigation is proposed, including the use of building materials as barriers to mitigate noise. There are no outdoor noise sensitive areas. The HUD STraCAT electronic tool was utilized to conduct a site specific noise assessment. The assessment indicated the wall assemblies meet required attenuation.

Based on the response, the review is in compliance with this section. Document and upload drawings, specifications, and other materials as needed to describe the project's noise mitigation measures below.

No mitigation is necessary.

Screen Summary

Compliance Determination

A Noise Assessment was conducted. The noise level was normally unacceptable: 69.0 db. See noise analysis. Since the DNL is in excess of 65 decibels, mitigation is proposed, including the use of building materials as barriers to mitigate noise. There are no outdoor noise sensitive areas. The HUD STraCAT electronic tool was utilized to conduct a site specific noise assessment. The assessment indicated the wall assemblies meet required attenuation. The project is in compliance with HUD's Noise regulation with mitigation. Mitigation measures include incorporating appropriate construction materials in the building to mitigate noise levels within the acceptable range. Construction components include 2X4 wood studs, 16"o.c., 5/8" gypsum board, dual pane low E windows, 4" face brick.

Supporting documentation

[STraCAT - 60 Harper.pdf](#)

[March 2023 Noise Assessment - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

- ✓ No

Sole Source Aquifers

General requirements	Legislation	Regulation
The Safe Drinking Water Act of 1974 protects drinking water systems which are the sole or principal drinking water source for an area and which, if contaminated, would create a significant hazard to public health.	Safe Drinking Water Act of 1974 (42 U.S.C. 201, 300f et seq., and 21 U.S.C. 349)	40 CFR Part 149

1. Does the project consist solely of acquisition, leasing, or rehabilitation of an existing building(s)?

Yes

No

2. Is the project located on a sole source aquifer (SSA)?

A sole source aquifer is defined as an aquifer that supplies at least 50 percent of the drinking water consumed in the area overlying the aquifer. This includes streamflow source areas, which are upstream areas of losing streams that flow into the recharge area.

No

Based on the response, the review is in compliance with this section. Document and upload documentation used to make your determination, such as a map of your project (or jurisdiction, if appropriate) in relation to the nearest SSA and its source area, below.

Yes

Screen Summary

Compliance Determination

The project is not located on a sole source aquifer area. There are no sole source aquifers in Michigan. The project is in compliance with Sole Source Aquifer requirements.

Supporting documentation

[No Sole Source Aquifer in MI - 60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Wetlands Protection

General requirements	Legislation	Regulation
Executive Order 11990 discourages direct or indirect support of new construction impacting wetlands wherever there is a practicable alternative. The Fish and Wildlife Service's National Wetlands Inventory can be used as a primary screening tool, but observed or known wetlands not indicated on NWI maps must also be processed. Off-site impacts that result in draining, impounding, or destroying wetlands must also be processed.	Executive Order 11990	24 CFR 55.20 can be used for general guidance regarding the 8 Step Process.

1. Does this project involve new construction as defined in Executive Order 11990, expansion of a building's footprint, or ground disturbance? The term "new construction" shall include draining, dredging, channelizing, filling, diking, impounding, and related activities and any structures or facilities begun or authorized after the effective date of the Order

No

Yes

2. Will the new construction or other ground disturbance impact an on- or off-site wetland? The term "wetlands" means those areas that are inundated by surface or ground water with a frequency sufficient to support, and under normal circumstances does or would support, a prevalence of vegetative or aquatic life that requires saturated or seasonally saturated soil conditions for growth and reproduction. Wetlands generally include swamps, marshes, bogs, and similar areas such as sloughs, potholes, wet meadows, river overflows, mud flats, and natural ponds.

"Wetlands under E.O. 11990 include isolated and non-jurisdictional wetlands."

No, a wetland will not be impacted in terms of E.O. 11990's definition of new construction.

Based on the response, the review is in compliance with this section. Document and upload a map or any other relevant documentation below which explains your determination

Yes, there is a wetland that be impacted in terms of E.O. 11990's definition of new construction.

Screen Summary

Compliance Determination

The project will not impact on- or off-site wetlands. There are no wetlands on the Project property or adjoining properties. The project is in compliance with Executive Order 11990.

Supporting documentation

[Wetland Mapper-60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Wild and Scenic Rivers Act

General requirements	Legislation	Regulation
The Wild and Scenic Rivers Act provides federal protection for certain free-flowing, wild, scenic and recreational rivers designated as components or potential components of the National Wild and Scenic Rivers System (NWSRS) from the effects of construction or development.	The Wild and Scenic Rivers Act (16 U.S.C. 1271-1287), particularly section 7(b) and (c) (16 U.S.C. 1278(b) and (c))	36 CFR Part 297

1. Is your project within proximity of a NWSRS river?

No

Yes, the project is in proximity of a Designated Wild and Scenic River or Study Wild and Scenic River.

Yes, the project is in proximity of a Nationwide Rivers Inventory (NRI) River.

Screen Summary

Compliance Determination

This project is not within proximity of a NWSRS river. The project is in compliance with the Wild and Scenic Rivers Act.

Supporting documentation

[wild and scenic rivers-60 Harper.pdf](#)

Are formal compliance steps or mitigation required?

Yes

No

Environmental Justice

General requirements	Legislation	Regulation
Determine if the project creates adverse environmental impacts upon a low-income or minority community. If it does, engage the community in meaningful participation about mitigating the impacts or move the project.	Executive Order 12898	

HUD strongly encourages starting the Environmental Justice analysis only after all other laws and authorities, including Environmental Assessment factors if necessary, have been completed.

1. Were any adverse environmental impacts identified in any other compliance review portion of this project's total environmental review?

Yes

No

2. Were these adverse environmental impacts disproportionately high for low-income and/or minority communities?

Yes

No

Explain:

The RECs identified in the Phase I ESA are not disproportionately high for low income and/or minority communities.

Based on the response, the review is in compliance with this section. Document and upload any supporting documentation below.

Screen Summary

Compliance Determination

Adverse environmental impacts are not disproportionately high for low-income and/or minority communities. The Project does not create adverse environmental or human health impacts. The mitigation measures approved by EGLE in the ResAP will be implemented to mitigate potential human exposures, including excavation

(removal) of soiled followed by verification sampling to confirm the contaminants have been removed. Therefore, the project is in compliance with Executive Order 12898.

Supporting documentation

[EJScreen Report.pdf](#)

Are formal compliance steps or mitigation required?

Yes

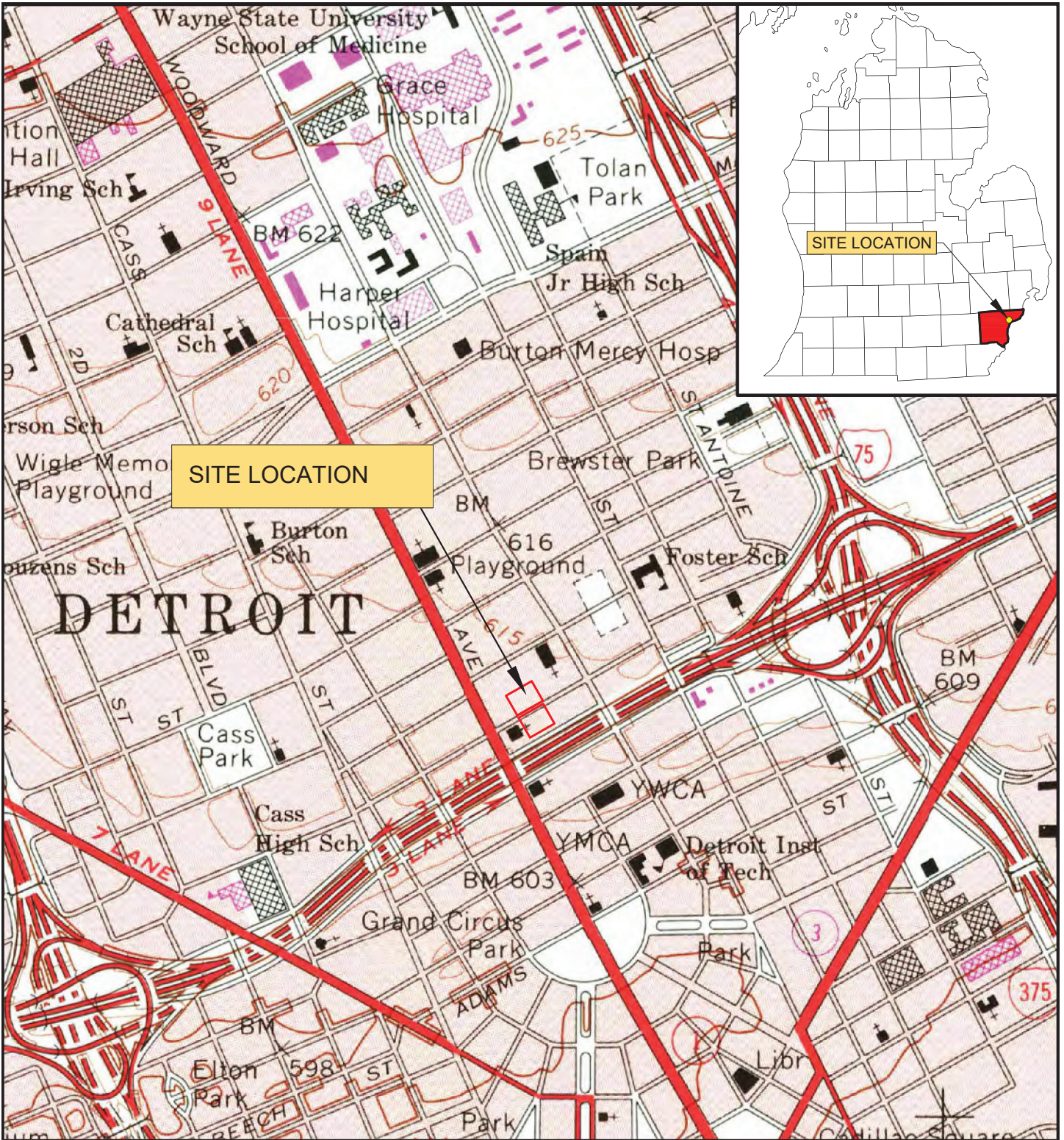
No

**60 Harper Apartments
HMA/ECS
August 2023**

Response Activity or Continuing Obligation	Required Activities	Party Responsible for Completing Activity	Timing of Activity	Cost	Required Follow-up or Reporting
ResAP – Impacted Soil	Remove Impact Soil Across the Subject Property, mainly non-native urban fill material. In locations of the proposed building foundation depths of material to be removed are approximately 5 -6 feet. Across the remaining portions of the Subject Property depths are approximately 3 feet.	Contractor/ Environmental Consultant	During Construction	\$854,500	Include results in DDCC report.
ResAP – Verification of Remediation Sampling	Verification of Remediation Sampling will be conducted at bottom of excavation depths across the Subject Property to ensure that adequate impacted soil has been removed and clean soil remains. Soil will be analyzed by laboratory prior to clean backfill material being placed at the Subject Property.	Environmental Consultant	After Impacted Soil Removal/Prior to Clean Fill Installation	\$48,100	Include results in DDCC report.
ResAP – Clean Fill	The fill material brought to the site will be documented as clean by analytical results from samples collected from the site of origin documenting that the material does not contain volatile organic compounds, polynuclear aromatic hydrocarbons, or Michigan Ten Metals at concentrations above the applicable generic cleanup criteria. With final grade topographic survey.	Contractor/ Environmental Consultant	During Construction	\$503,700	Include results in DDCC report.
Documentation of Due Care Compliance	<p>A. Complete a DDCC report and submit to the City of Detroit Environmental Review Officer for review prior to submitting to EGLE. Engineering controls will require an Operations and Maintenance plan.</p> <p>B. Additional requirements such as a Restrictive Covenants and/or a recorded Notice to Title may be requested depending on site conditions.</p>	Environmental Consultant	During Construction	\$6,000	Provide report to HRD’s ER Team
ResAP – Vapor Mitigation	No vapor mitigation necessary based on proposed response actions.	Environmental Consultant	N/A	N/A	N/A

**60 Harper Apartments
HMA/ECS
August 2023**

Section 106 – Conditional No Adverse Effect Requirements	A. Prior to the start of any work, building plans, specifications and photos must be submitted to the Preservation Specialist for review and Conditional Approval	General Contractor	Prior to Construction	N/A - Completed	Submit work to Preservation Specialist.
	B. If there is a change in the scope of work, those changes will be required to undergo additional Section 106 Review prior to the execution of any work.	General Contractor	At any time		Notify Preservation Specialist
Section 106 – Unanticipated Discoveries Plan	Once construction has started, the SHPO approved Unanticipated Discoveries Plan shall be followed for the duration of the project.	Construction Crew, Foremen, Developer	During Construction	N/A	Unanticipated Discoveries Plan with SHPO approval
Noise Analysis – Unacceptable Noise	Appropriate construction materials will be incorporated in the building to mitigate noise levels within the acceptable range.	Architect, Construction, Crew, Foremen, Developer,	During Construction	N/A – Part of Construction	Building specs



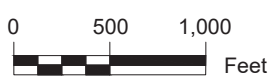
SITE LOCATION

SITE LOCATION

DETROIT

LEGEND

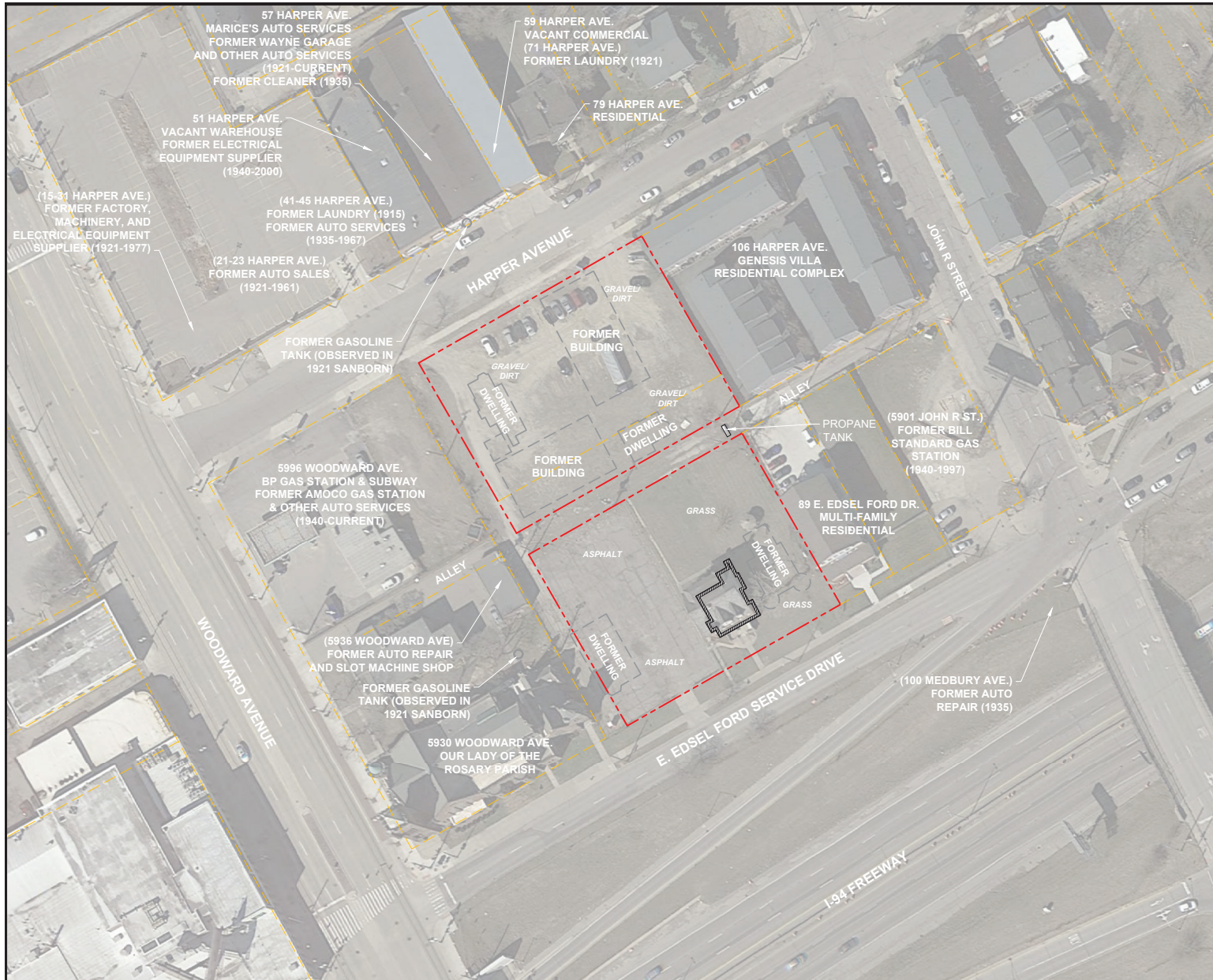
 SITE LOCATION



HAMP
MATHES &
ASSOCIATES, INC.

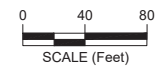
60 HARPER LDHA, LLC
60 HARPER AVENUE AND
71 E. EDEL FORD SERVICE DRIVE
DETROIT, MICHIGAN 48202

FIGURE 1
SITE LOCATION



LEGEND

- PROPERTY BOUNDARY
- SITE PROPERTY BOUNDARY
- FORMER SITE FEATURES
- (#) HISTORICAL ADDRESS AND HISTORICAL OPERATIONS



60 HARPER LDHA, LLC
60 HARPER AVENUE AND
71 E. EDEL FORD SERVICE DRIVE
DETROIT, MICHIGAN 48202

FIGURE 2
SITE AND SURROUNDING PROPERTIES MAP



Photo 1. Southwest corner of the Subject Property, facing northeast across the paved parking area.



Photo 2. Southern edge of the Subject Property, facing east.



Photo 3. Western edge of the Subject Property, facing north.



Photo 4. Northeastern corner of the Subject Property, facing west.



Photo 5. Typical view of the gravel parking area at the northern portion of the Subject Property.



Photo 6. Typical view of the grass cover on the northern portion of the Subject Property.



Photo 7. Typical view of the unoccupied, multi-tenant residential structure located at the southeastern portion of the Subject Property.



Photo 8. Stormwater drain for roof drainage at the unoccupied structure.



Photo 9. View of a solid waste dumpster located along the western edge of the Subject Property. Also evidence of a soil boring in the front of the dumpster.



Photo 10. View of mulch pile and solid waste trash bags near the church on the west adjoining property.



Photo 11. Typical view of stormwater drain located near the southeast corner of the Subject Property.



Photo 12. View of pole mounted transformer within the electrical transmission utility easement that transects the central portion of the Subject Property.



Photo 13. View of two solid waste dumpsters located on the northeast corner of the Subject Property.



Photo 14. View of AST situated near the eastern edge of the Subject Property within the electrical utility easement.



Photo 15. View of adjacent property to the north.



Photo 16. View of multi-tenant residential complex on the adjoining property to the east along Harper Avenue.



Photo 17. View of multi-tenet residential structure located on the east adjoining property along Edsel Ford Service Drive.



Photo 18. View of the church located on the adjoining property to the west.

NEW MIXED USE BUILDING FOR

60 HARPER APARTMENTS

60 HARPER AVENUE, DETROIT, MI 48202

ARCHITECT'S PROJECT # 1307-23

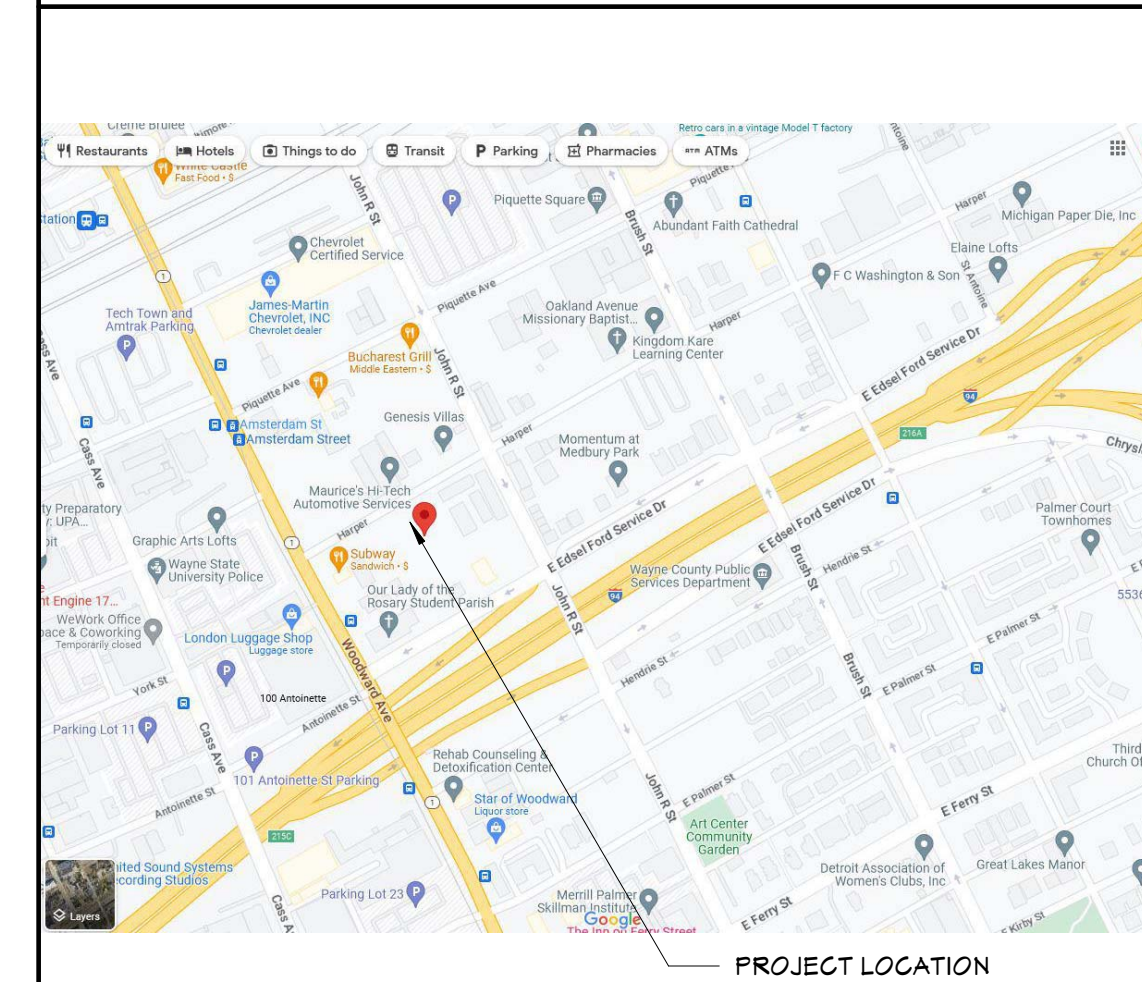
OWNER REVIEW SET

02.10.2023

ABBREVIATIONS

ABOVE FINISH FLOOR	AFF	GALVANIZED	GALV	QUARRY TILE	QT
ACOUSTICAL TILE	ACT	GAUGE	GA	RADIUS	R
ADJACENT	ADJ	GLASS	GL	REFRIGERATOR	REFRIG
AIR ADMITTANCE VALVE	AAV	GRADE	GR	REINFORCEMENT	REINF
ALUMINUM	AL	GYPSPUM BOARD	GYP BD	REQUIRED	REQD
AMERICAN DISABILITIES ACT	ADA	GLAZED MASONRY UNIT	GMU	ROOF DRAIN	RD
ANODIZED	AND	HAND DRYER	HD	ROOF VENT	RV
APPROXIMATELY	APPROX	HANDICAP	HDGP	ROOM	RM
AREA DRAIN	AD	HARDWOOD	HDWD	ROUGH OPENING	RO
AT	@	HEATING	HTG	RUBBER FLOOR TILE	RFT
		HEATING VENTILATING AND			
		AIR CONDITION	HVAC	SANITARY	SAN
BASEMENT	BSMT	HEIGHT	HT	SECTION	SECT
BEARING	BRG	HIGH POINT	HP	SHEET	SHT
BENCH MARK	BM	HIGHWAY	HPY	SHEET METAL	SHT MTL
BETWEEN	BTWN	HOLLOW METAL	HM	SIMILAR	SIM
BARRIER FREE	B.F.	HORIZONTAL	HORIZ	SIMILAR TOO	SIM TO
BLOCK	BLK	INCLUDE	INCL	SMART BOARD	SB
BOARD	BD	INFORMATION	INFO	SPECIFICATION	SPEC
BOTTOM OF	BO	INSIDE DIMENSION	INSUL	SQUARE	SQ
BOTTOM OF STEEL	BOS	INSULATION	INSUL	STAINLESS STEEL	SS
BUILDING	BLDG	JANITOR	JAN	STANDARD	STD
		JANITOR CLOSET	JC	START POINT	SP
CABINET	CAB	JOINT	JT	STEEL	STL
CARPET	CFT	JOIST	JST	STORAGE	STOR
CATCH BASIN	CB	LAMINATE	LAM	STRUCTURAL	STRUCT
CEILING	CLG	LINEAR	LIN	SUSPENDED	SUSP
CENTER LINE	CL	LONG LEG VERTICAL	LLV	TACK BOARD	TB
CLEAN OUT	CO	LOW POINT	LP	TEMPERED	TEMP
COLUMN	COL	LOWER LEVEL	LL	TONGUE & GROOVE	T & G
CONCRETE	CONC	MAN HOLE	MH	TOP OF FOOTING	T O FTG
CONCRETE MASONRY UNIT	CMU	MANUFACTURER	MFR	TOP OF FOUNDATION	T O FTG
CONTINUOUS	CONT	MARKER BOARD	MB	TOP OF STRUCTURAL	T O S
CONTROL JOINT	CJ	MASONRY OPENING	MO	TOP OF WALL	T O W
CUBIC	CU	MAXIMUM	MAX	THICK	THK
		MECHANICAL	MECH	THROUGH	THRU
DEMOLITION	DEMO	METAL	MTL	TYPICAL	TYP.
DETAIL	DET	MISCELLANEOUS	MISC	UNDERWRITER'S LABORATORY	UL
DIAMETER	DIA	MOUNTED	MTD	UNLESS NOTED OTHERWISE	UNO
DIMENSION	DIM	NORMAL	NOM	VENT THROUGH ROOF	VTR
DOWN	DN	NOT IN CONTRACT	NIC	VENTILATION	VENT
DOWNSPOUT	DS	NUMBER	NUM	VERIFY IN FIELD	V I F
DRAWING	DNWG	ON CENTER	OC	VERTICAL	VERT
		OPENING	OPN	VINYL COMPOSITE TILE	VCT
		OPPOSITE	OPP	VINYL WALL COVERING	VWC
		OPPOSITE HAND	OH	VOLUME	VOL
		OUTSIDE DIAMETER	OD	WALL CLEAN OUT	WCO
				WELDED WIRE FABRIC	WWF
				WELDED WIRE MESH	WWM
				WITH	W
				WITHOUT	WO
				WOOD	WD
				WOMANIZED	WOLM
				WORK POINT	WP
				YARD CLEAN OUT	YCO
FEET	FT	FAIR	FR		
FINISHED FLOOR	FIN	FLASTER CEMENT	FLSC		
FIRE EXTINGUISHER	FE	FLASTER GYPSUM	FLSG		
FIRE EXTINGUISHER	FEC	FLATE	FLT		
CABINET	FL	FLYWOOD	FLYND		
FLOOR	FD	PORCELAIN CERAMIC TILE	PCT		
FLOOR DRAIN	FTG	PRESSURE TREATED	PT		
FOOTING	FND				
FOUNDATION	FFF				
FROM FINISHED FLOOR					

SITE LOCATION MAP



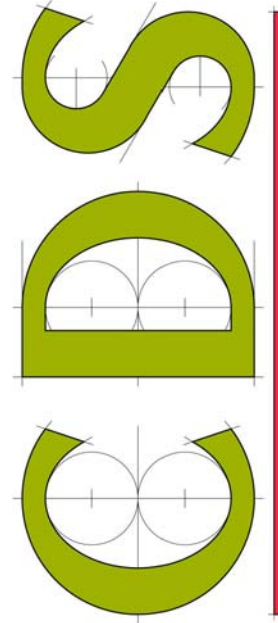
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E1.01 XXX

PROJECT DIRECTORY

STRUCTURAL ENGINEER	ARCHITECT
JDH ENGINEERING KEITH RITSEMA, P.E. 3000 IVANREST SQ, SUITE B GRANDVILLE, MI 49418 P.616.531.6020	CONCEPT DESIGN STUDIO KYLE OSTERHART, AIA, LEED AP BD+C 800 E. ELLIS ROAD, SUITE 508 NORTON SHORES, MI 49441 P.231.799.4838
CIVIL ENGINEERING	MEP ENGINEERING
CALLEN ENGINEERING, LLC BRUCE CALLEN, P.E. 108 EAST SAVIDGE STREET SPRING LAKE, MI 49456 P.616.414.5260	MATRIX ENGINEERING NATHAN SMITH, P.E. 1601 E. CESAR E. CHAVEZ AVENUE LANSING, MI 48906 P.517.487.2511
OWNER	CONSTRUCTION MANAGER
60 HARPER, LLC T. VAN FOX 32500 TELEGRAPH ROAD, SUITE 100 BINGHAM FARMS, MI 48025	MHT CONSTRUCTION PAUL STODDARD, AIA (DIRECTOR OF CONSTRUCTION) 32500 TELEGRAPH ROAD, SUITE 100 BINGHAM FARMS, MI 48025

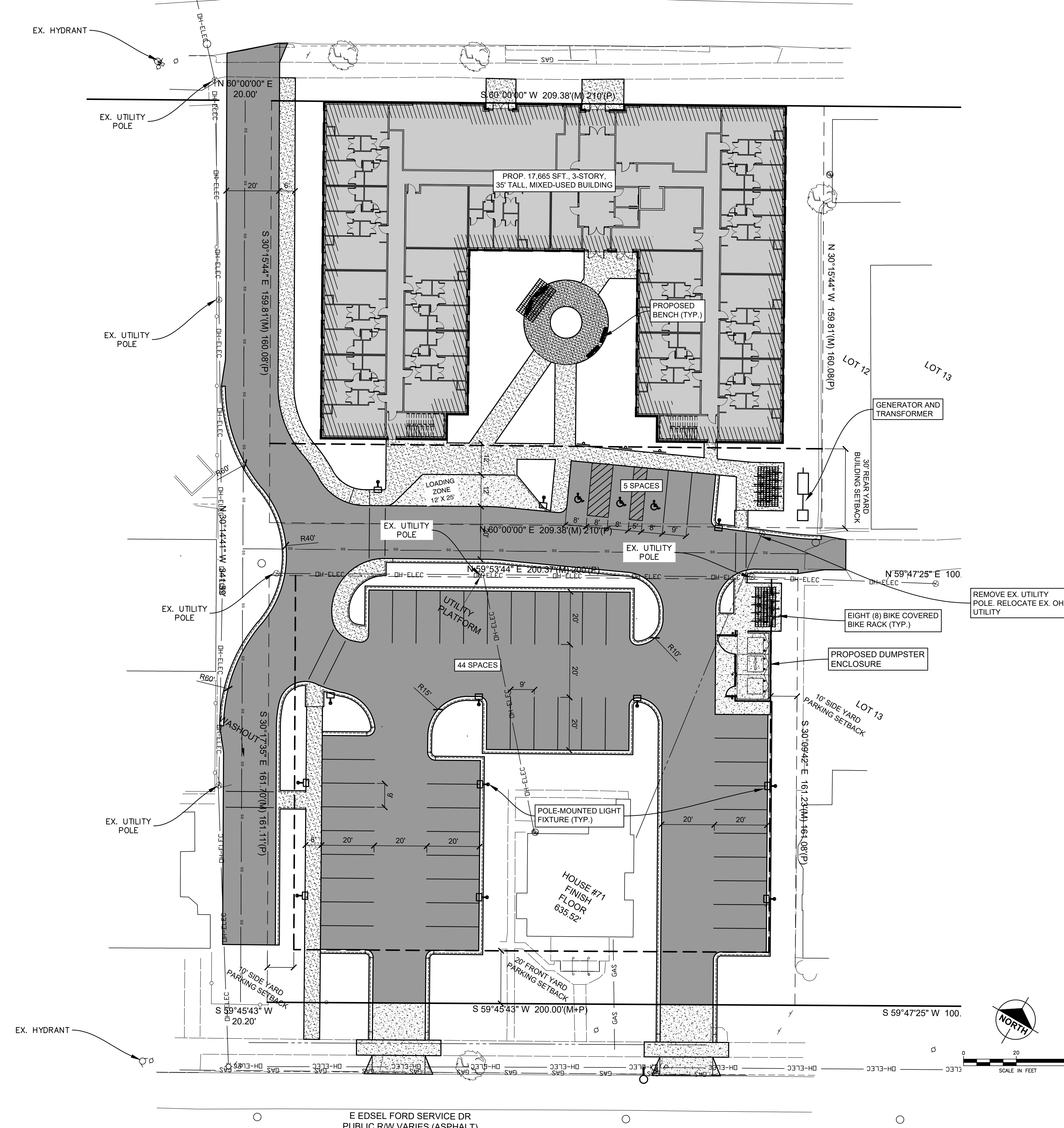
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NEW MIXED USE BUILDING FOR
60 HARPER APARTMENTS
 60 HARPER AVENUE, DETROIT, MI 48202

PROJECT #	1307-23
ISSUANCES	
OWNER REVIEW SET	02.10.2023
DRAWN BY	KO
SHEET TITLE	TITLE SHEET
SHEET NO.	T1.01



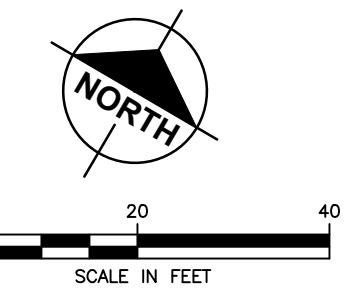
- PROPOSED FEATURES LEGEND**
- NEW HMA PAVEMENT AREA
 - NEW LIGHT DUTY CONCRETE SIDEWALK AREA
 - NEW HEAVY DUTY CONCRETE SIDEWALK AREA

callen
civil engineers


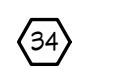
Callen Engineering, Inc.
108 East Savidge Street
Spring Lake, MI 49456
T.616.414.5260
www.callenengineering.com

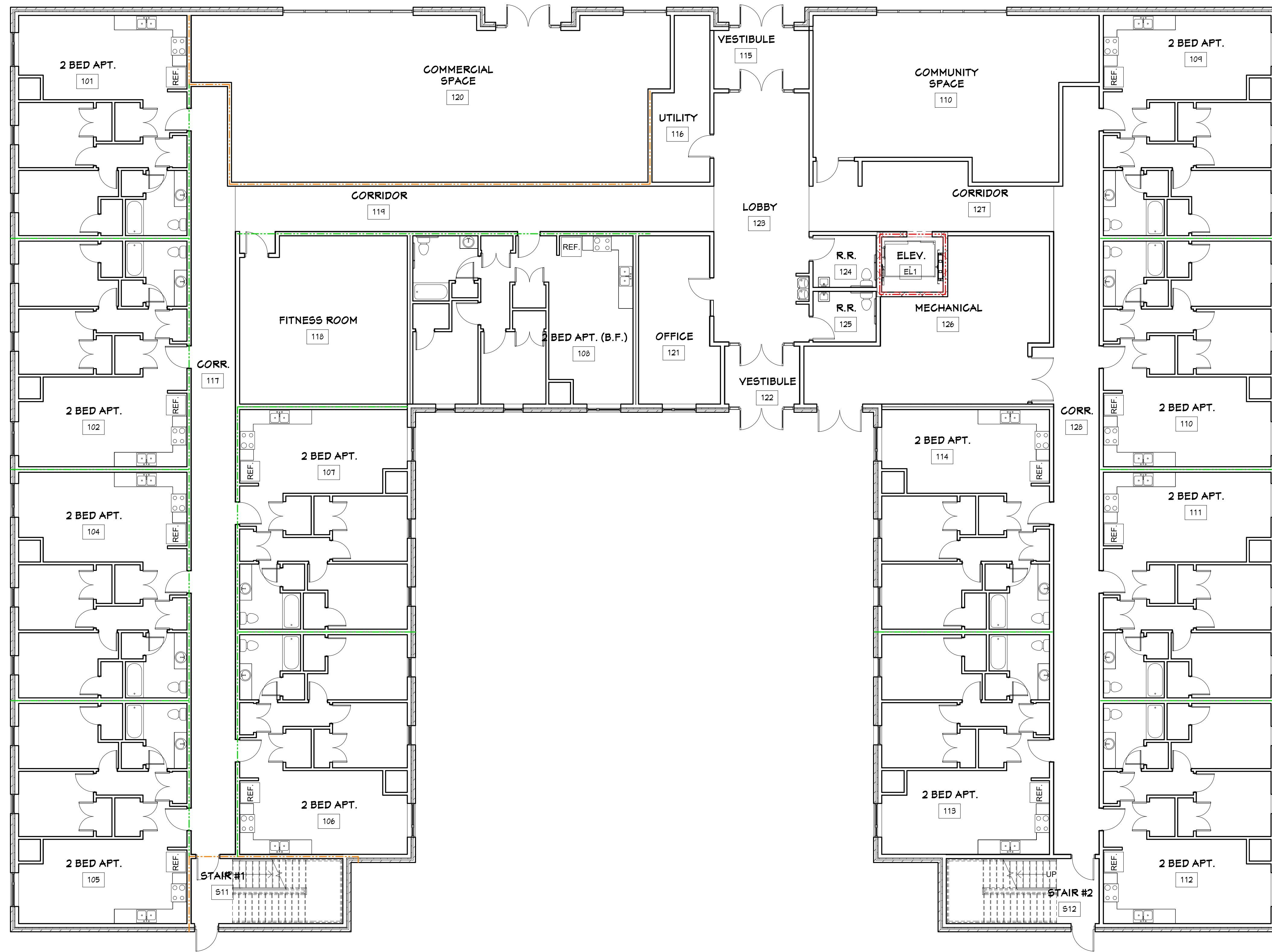
**WOODWARD / HARPER
APARTMENTS
SCHEMATIC PLAN**

60 Harper Avenue & 71 E. Edsel Ford Service Drive
City of Detroit
Wayne County, Michigan



CODE COMPLIANCE LEGEND

- FIRE RATED WALL
- PATH OF TRAVEL LINE w/ DISTANCE TO EXIT
-  EXIT 34"
- F.E.C. FIRE EXTINGUISHER CABINET
- F.E. FIRE EXTINGUISHER
-  ROOM OCCUPANCY



FIRST FLOOR CODE COMPLIANCE PLAN
1/8" = 1'-0"

CODE REVIEW NOTES

BUILDING: 2015 MICHIGAN BUILDING CODE
 MECHANICAL: 2015 MICHIGAN MECHANICAL CODE
 PLUMBING: 2018 MICHIGAN PLUMBING CODE
 ELECTRICAL: 2017 NATIONAL ELECTRICAL CODE
 BARRIER FREE: ICC/ANSI A117.1-2009
 FIRE CODE: INTERNATIONAL FIRE CODE 2018
 ENERGY CODE: ASHRAE 90.1-2013 W/ AMENDMENTS

SUSTAINABILITY REQUIREMENTS

- ENTERPRISE GREEN COMMUNITIES PLUS (EGC+)
- DEPARTMENT OF ENERGY'S ZERO ENERGY READY HOME
- ENERGY STAR
 - ENERGY STAR RATER DESIGN
 - ENERGY STAR MFNC HVAC DESIGN
 - NATIONAL HVAC DESIGN SUPPLEMENT FOR DWELLING UNITS
 - ENERGY STAR MFNC HVAC DESIGN SUPPLEMENT FOR COMMON SPACES AND CENTRAL SYSTEMS
 - ENERGY STAR MFNC WATER MANAGEMENT SYSTEM REQUIREMENTS

BUILDING DATA:

NEW CONSTRUCTION

BUILDING USE GROUP:
 - NON-SEPARATED MIXED USE GROUP
 - R-2 MULTI-FAMILY (SECTION 310.4) & BUSINESS (SECTION 304.1) OR ASSEMBLY (SECTION 303)

CONSTRUCTION TYPE = V-B (TABLE 601)

ALLOWABLE AREA = 21,000 S.F., (3) STORY

ACTUAL AREA = 17,673 S.F.

OCCUPANT LOAD MAX. = ??

NUMBER OF EXITS = (2) REQUIRED, (2) PROVIDED

FIRE SUPPRESSION = NFPA-13 REQUIRED

EXIT ACCESS TRAVEL DISTANCE: 250 FT (TABLE 1017.2)

REQUIRED EXITS: 2 MIN. (TABLE 1006.3.1)

DEAD END CORRIDOR = 50' MAX. W/ AUTO. SPRINKLER SYSTEM (SECTION 1020.4)

BUILDING ELEMENTS (TABLE 601)

PRIMARY STRUCTURAL FRAME: 0 HR.

BEARING WALLS
 EXTERIOR: 0 HR.
 INTERIOR: 0 HR.

NON-BEARING WALLS & PARTITIONS
 EXTERIOR: 0 HR.
 INTERIOR: 0 HR.

FLOOR CONSTRUCTION: 0 HR.

ROOF CONSTRUCTION: 0 HR.

REQUIRED FIRE SEPARATIONS

SHAFT: 1 HR. - (SECTION 713.4)

TRASH CHUTE: 1 HR. - (SECTION 713.13)

EXIT ENCLOSURE: 2 HR. (SECTION 1020.1)

ELEVATOR HOISTWAYS: 1 HR. (SECTION 713)
 - ENCLOSED ELEVATOR LOBBY NOT REQUIRED PER SECTION 713.14

CORRIDORS: 1/2 HR. (TABLE 1018.1)

CORRIDORS: SMOKE & FIRE DAMPERS REQUIRED PER (SECTION 709)

USE GROUP SEPARATIONS: UNIT / UNIT = 1/2 HR. (SECTION 708)

USE GROUP SEPARATION R2/B OR R2/A = 1 HR. (TABLE 508.4)

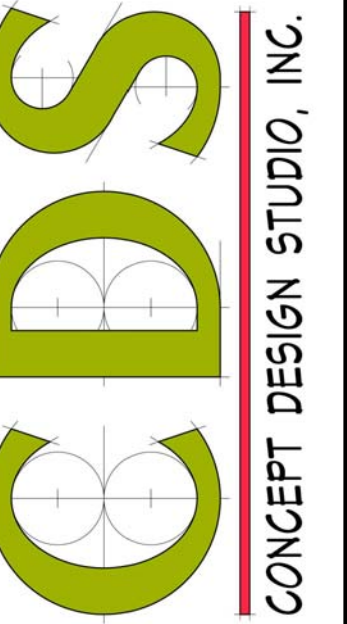
ACCESSIBLE DWELLING UNITS

(5) FULLY ACCESSIBLE TYPE 'A' & ALL OTHERS TYPE 'B'

SITE & UNIT DATA

SITE PARKING SPACES = 49 SPACES
 UNIT MIX = 49 (2 BEDROOM) UNITS

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NEW MIXED USE BUILDING FOR
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PROJECT # 1307-23

ISSUANCES

OWNER REVIEW SET 02.10.2023

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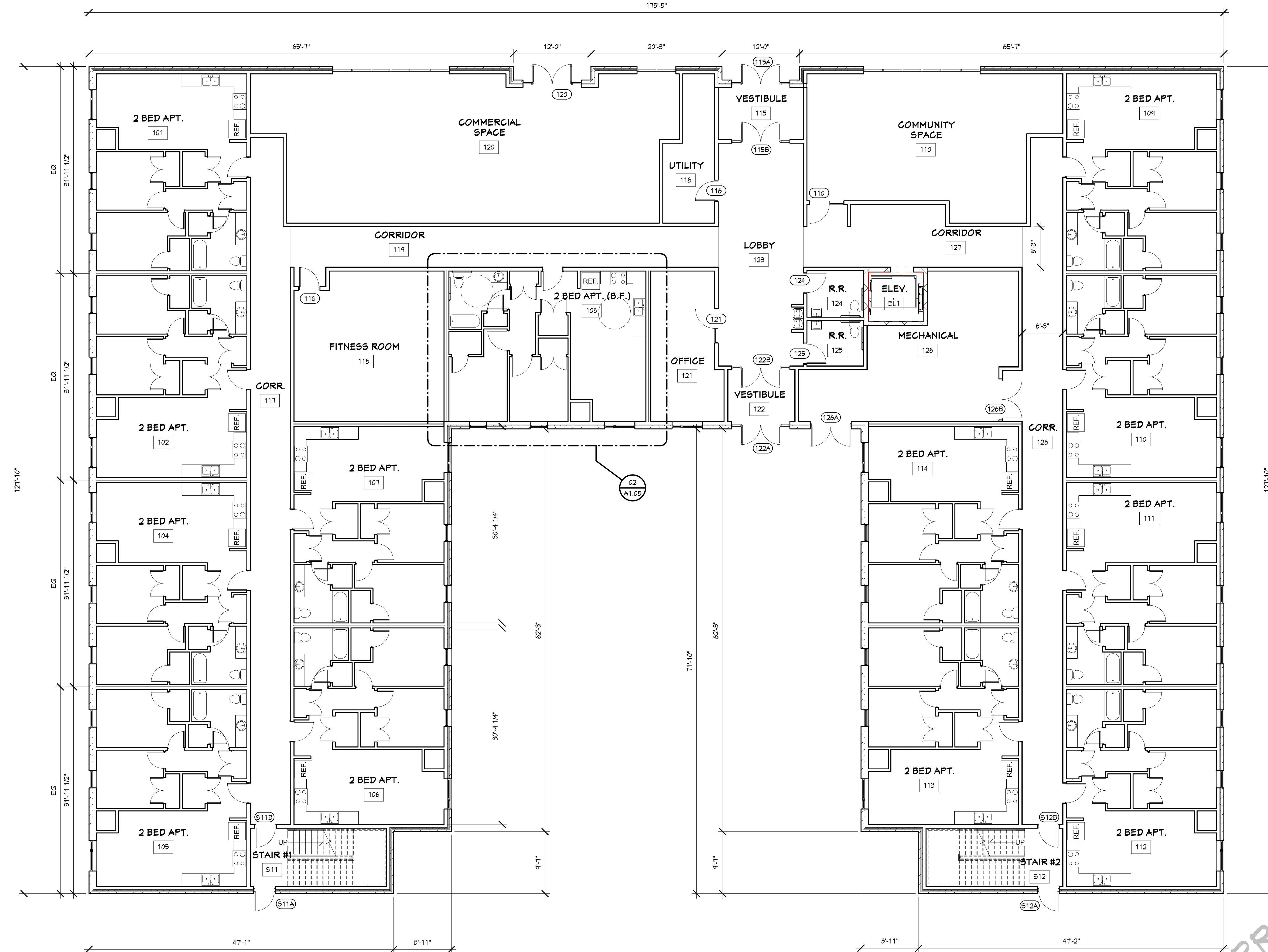
SHEET TITLE

FIRST FLOOR
 CODE
 COMPLIANCE
 PLAN

SHEET NO.

T2.01

NOT FOR CONSTRUCTION

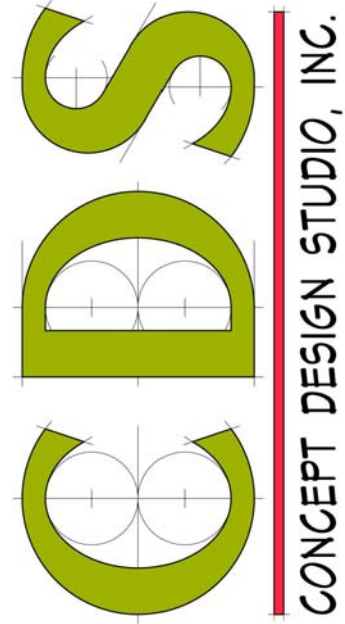


FIRST FLOOR PLAN
1/8" = 1'-0"

GENERAL NOTES

1. ALL TRADES TO COORDINATE TO AVOID INTERFERENCES.
2. CONTRACTOR TO FIELD VERIFY FIELD CONDITIONS AND DIMENSIONS PRIOR TO PROCEEDING WITH WORK. REPORT SIGNIFICANT DISCREPANCIES BETWEEN DRAWINGS AND ACTUAL CONDITIONS.
3. PATCH & REPAIR, & PREP ALL EXISTING WALLS, FLOORS & CEILINGS FOR NEW FINISHES.
4. PROPER FIRE ASSEMBLIES REQUIRED FOR ALL PENETRATIONS THROUGH FIRE RATED WALLS, FLOOR AND CEILINGS. INCLUDE FIRE CAULKING, FIRE COLLARS AND FIRE SLEEVES.
5. INSTALLATION SHALL MEET THE REQUIREMENTS OF ALL APPLICABLE CODES AND REGULATIONS.
6. INSTALL 1/2" FEMOLDED EXPANSION MATERIAL AT JOINTS BETWEEN FLOOR SLAB & FOUNDATION WALL UNLESS NOTED OTHERWISE.

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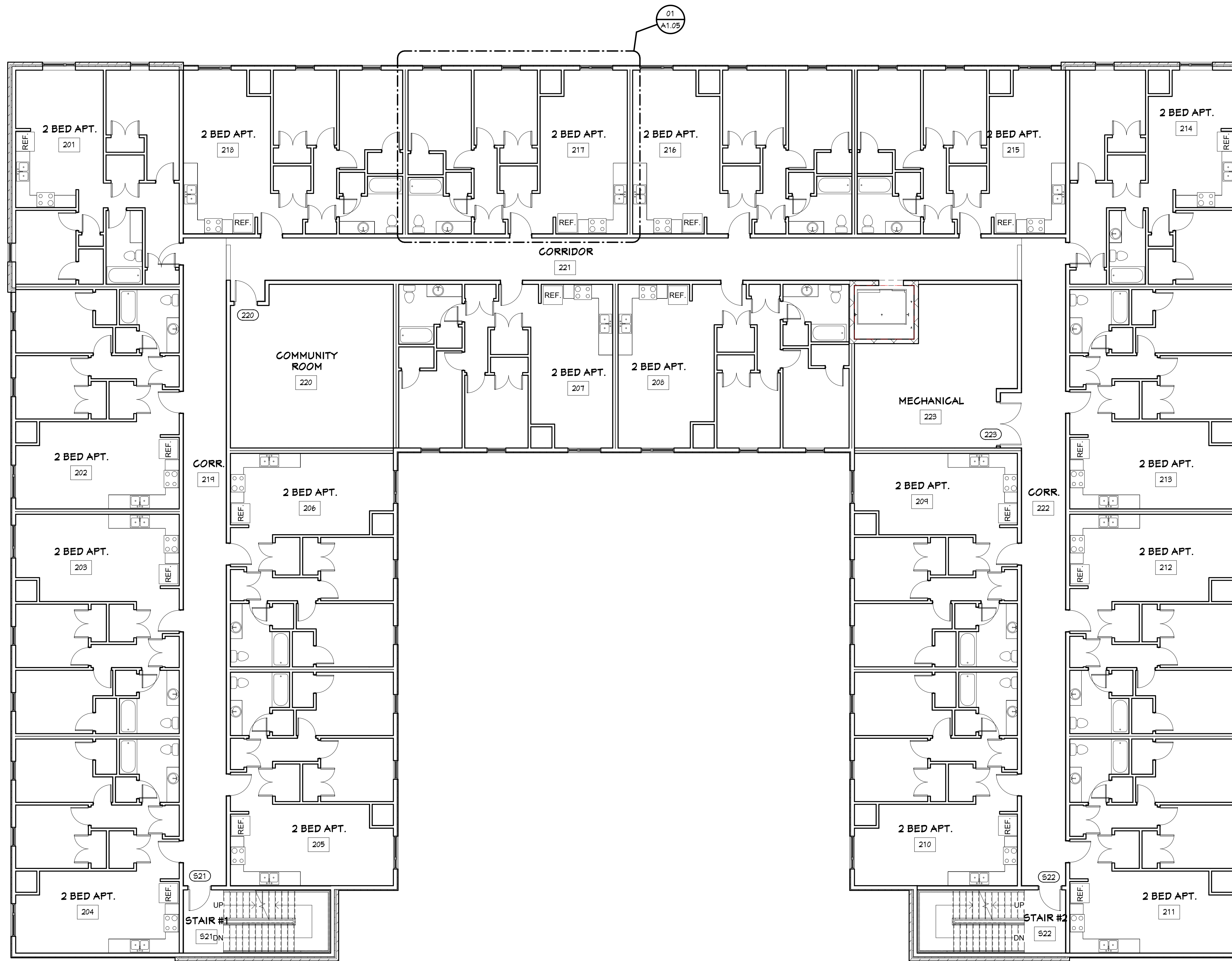
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SHEET TITLE
FIRST FLOOR PLAN

SHEET NO.

A1.01

PRELIMINARY
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SECOND FLOOR PLAN
1/8" = 1'-0"

NEW MIXED USE BUILDING FOR
60 HARPER APARTMENTS
60 HARPER AVENUE, DETROIT, MI 48202

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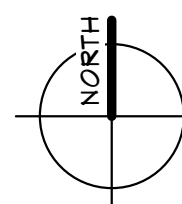
SHEET TITLE

SECOND FLOOR PLAN

SHEET NO.

A1.02

PRELIMINARY
NOT FOR CONSTRUCTION

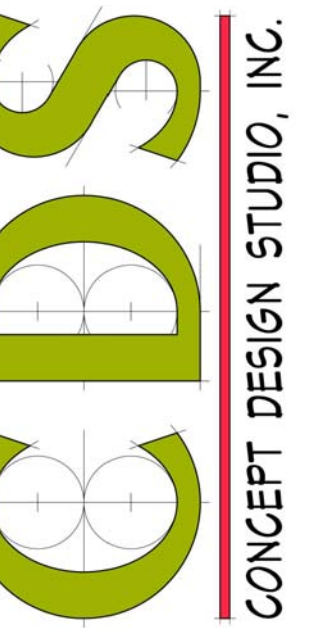


THIRD FLOOR PLAN

1/8" = 1'-0"

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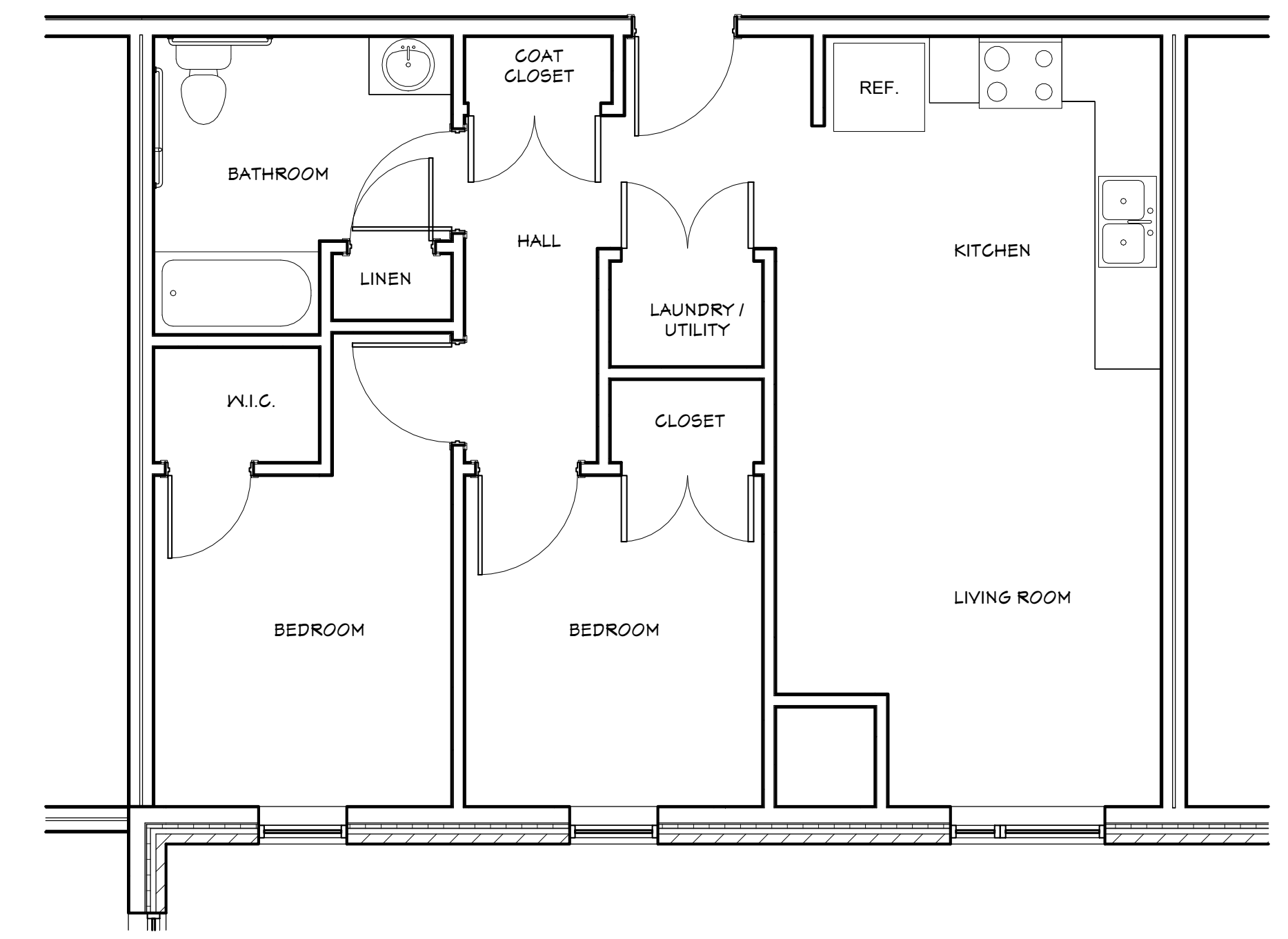
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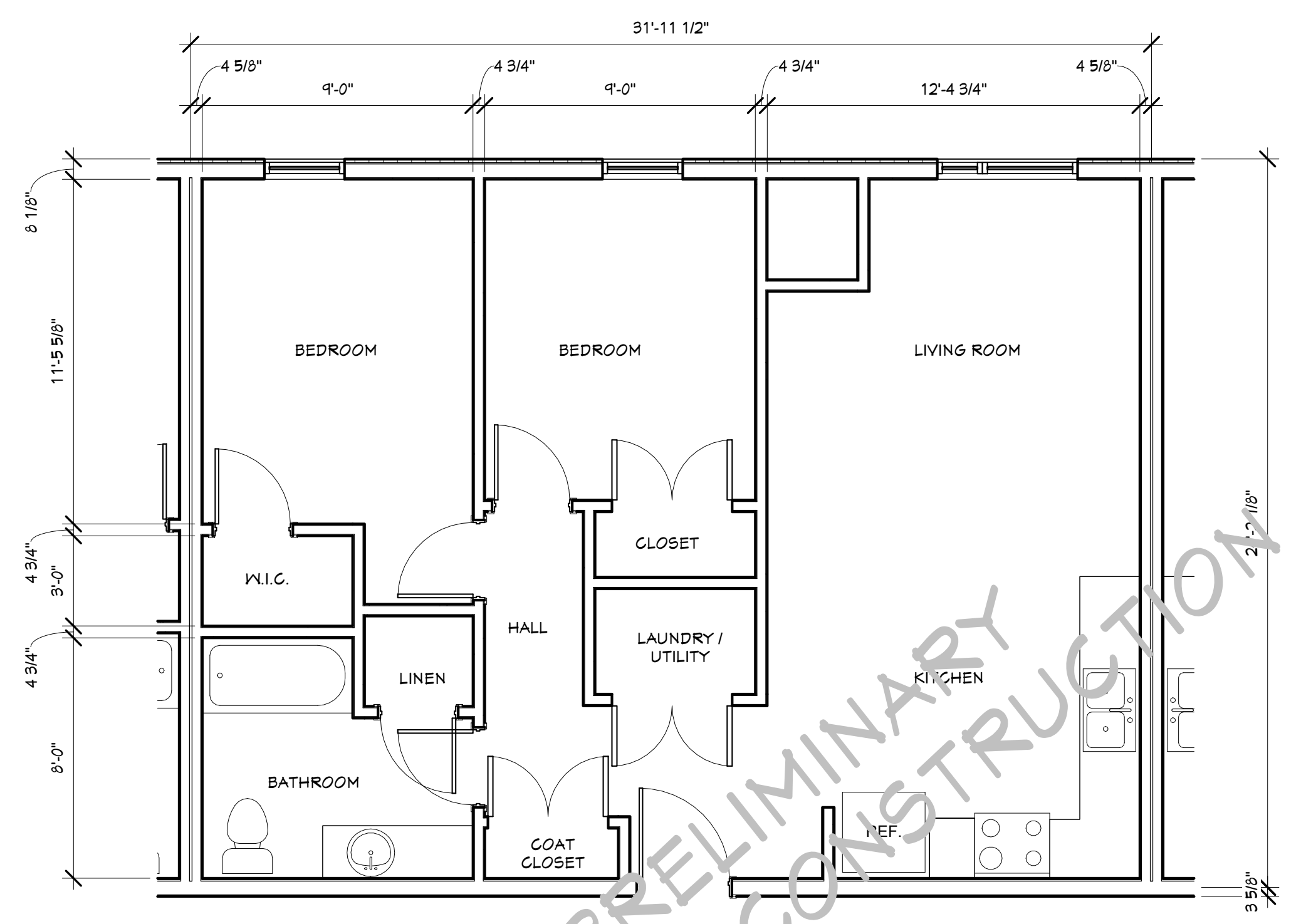
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THIRD FLOOR PLAN

SHEET NO.

A1.03



TYPICAL B.F. UNIT FLOOR PLAN



TYPICAL UNIT FLOOR PLAN

NOT FOR CONSTRUCTION

FINISH KEY

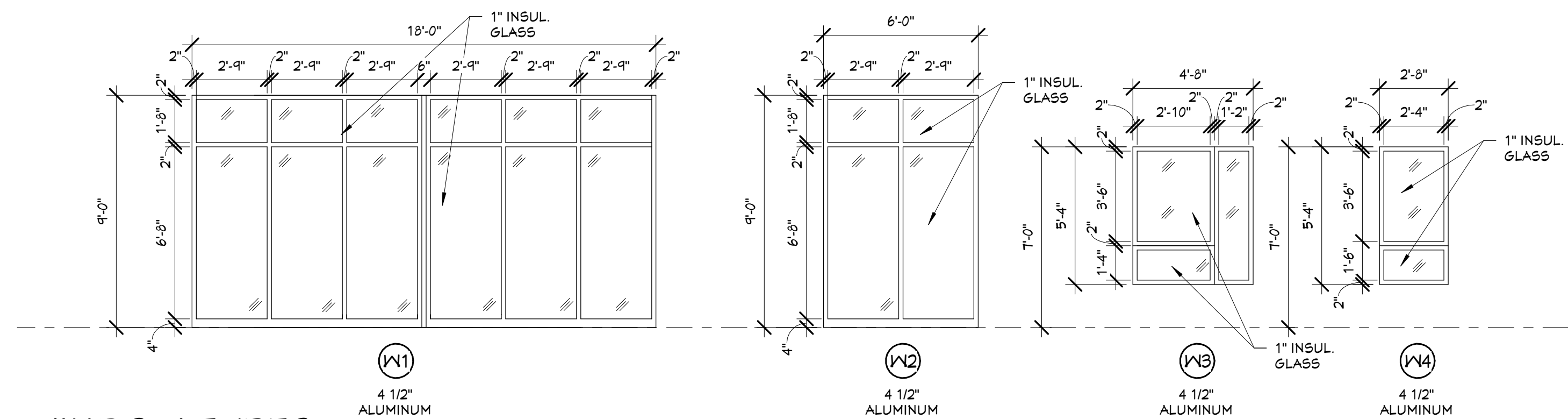
FLOORING:	WALL FINISH:
F-1 CARPET TILE	WF-1 PAINT
F-2 WALK-OFF CARPET	WF-2 CERAMIC TILE
F-3 SEALED CONCRETE	CEILING MATERIAL:
F-4 PORCELAIN TILE	C-1 2X2 SUSPENDED CEILING TILE
F-5 RUBBER FLOORING, TREADS, & NOSINGS	C-2 NONE
	C-3 GYPSUM BOARD (PAINTED)
BASE:	
B-1 4" VINYL	
B-2 NONE	
B-3 PORCELAIN TILE	

ROOM FINISH SCHEDULE

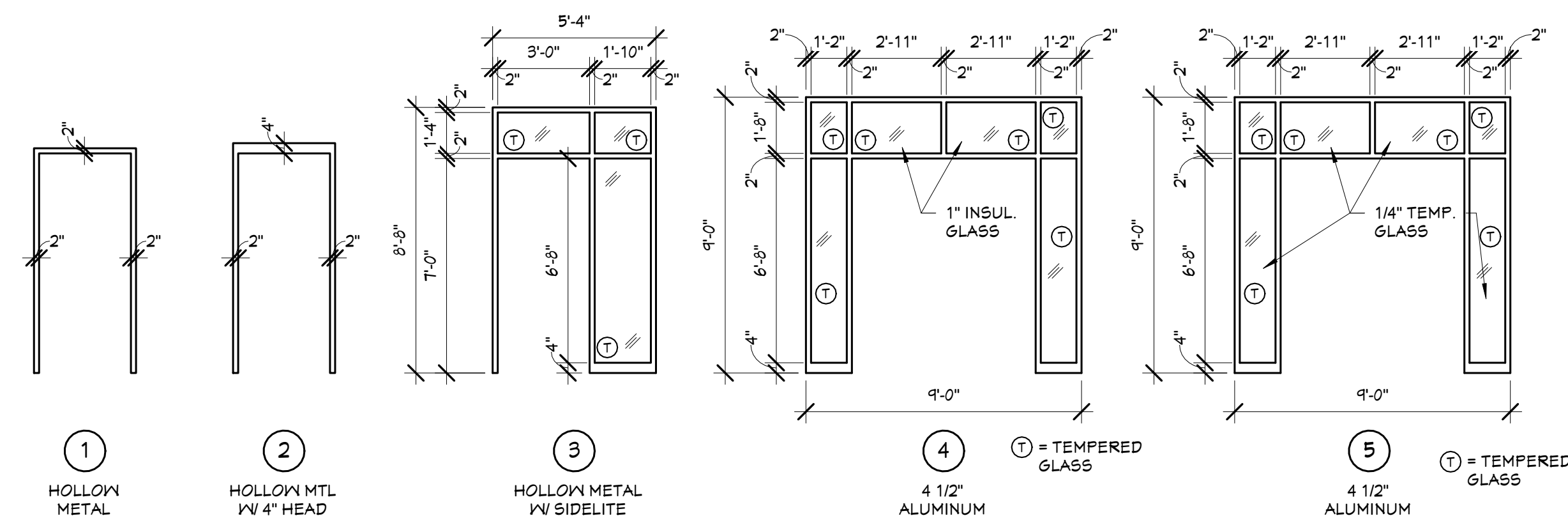
No.	Name	Floor	Base	Wall Fin.	Ceiling	Clg. Ht.	Comments
110	COMMUNITY SPACE	F-1	B-1	WF-1	C-1	9'-0"	
115	VESTIBULE	F-2	B-1	WF-1	C-1	9'-0"	
116	UTILITY	F-3	B-1	WF-1	C-2		
117	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
118	FITNESS ROOM	F-1	B-1	WF-1	C-1	9'-0"	
119	CORRIDOR	F-1	B-1	WF-1	C-1	9'-0"	
120	COMMERCIAL SPACE	F-2	B-2	WF-1	C-2		
121	OFFICE	F-1	B-1	WF-1	C-1	9'-0"	
122	VESTIBULE	F-2	B-1	WF-1	C-1	9'-0"	
123	LOBBY	F-1	B-1	WF-1	C-1	9'-0"	
124	R.R.	F-4	B-3	WF-2	C-1	8'-0"	
125	R.R.	F-4	B-3	WF-2	C-1	8'-0"	
126	MECHANICAL	F-3	B-1	WF-1	C-2		
127	CORRIDOR	F-1	B-1	WF-1	C-1	9'-0"	
128	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
214	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
220	COMMUNITY ROOM	F-1	B-1	WF-1	C-1	9'-0"	
221	CORRIDOR	F-1	B-1	WF-1	C-1	9'-0"	
222	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
223	MECHANICAL	F-3	B-1	WF-1	C-2		
314	COMMUNITY ROOM	F-1	B-1	WF-1	C-1	9'-0"	
320	CORR.	F-1	B-1	WF-1	C-2	9'-0"	
321	MECHANICAL	F-3	B-1	WF-1	C-2	9'-0"	
322	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
323	CORR.	F-1	B-1	WF-1	C-1	9'-0"	
EL1	ELEV.						
511	STAIR #1	F-5	B-1	WF-1			
512	STAIR #2	F-5	B-1	WF-1			
521	STAIR #1	F-5	B-1	WF-1			
522	STAIR #2	F-5	B-1	WF-1			
531	STAIR #1	F-5	B-1	WF-1			
532	STAIR #2	F-5	B-1	WF-1			

DOOR SCHEDULE

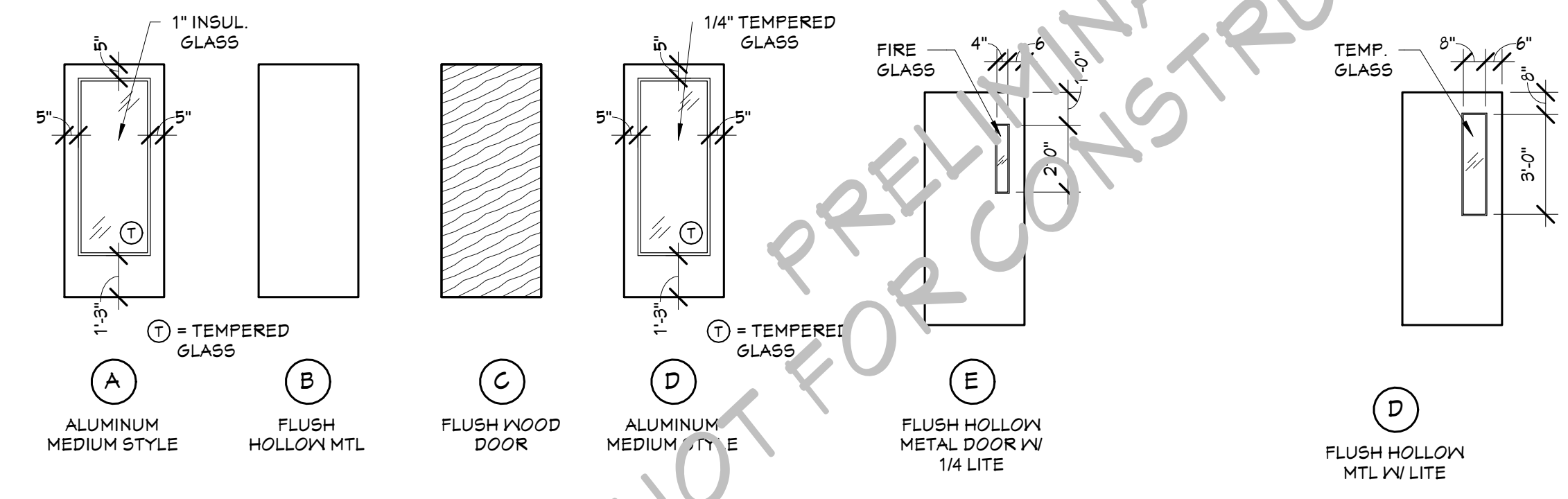
No.	Door			Door Type	Frame Type	Fire Rating	Hdur.	Comments
	Width	Height	Thickness					
110	3'-0"	7'-11"	1 3/4"	C	3			
115A	3'-0"	7'-0"	1 3/4"	A	4			
115B	3'-0"	7'-0"	1 3/4"	D	5			
116	3'-0"	7'-0"	1 3/4"	C	1			
118	3'-0"	7'-0"	1 3/4"	C	1			
120	3'-0"	7'-0"	1 3/4"	A	4			
121	3'-0"	7'-11"	1 3/4"	C	3			
122A	3'-0"	7'-0"	1 3/4"	A	4			
122B	3'-0"	7'-0"	1 3/4"	D	5			
124	3'-0"	7'-0"	1 3/4"	C	1			
125	3'-0"	7'-0"	1 3/4"	C	1			
126A	3'-0"	7'-0"	1 3/4"	B	2			
126B	3'-0"	7'-0"	1 3/4"	E	1			
220	3'-0"	7'-0"	1 3/4"	C	1			
223	3'-0"	7'-0"	1 3/4"	C	1			
314	3'-0"	7'-0"	1 3/4"	C	1			
321	3'-0"	7'-0"	1 3/4"	B	1			
511A	3'-0"	7'-0"	1 3/4"	F	1			
511B	3'-0"	7'-0"	1 3/4"	E	1			
512A	3'-0"	7'-0"	1 3/4"	F	1			
512B	3'-0"	7'-0"	1 3/4"	E	1			
521	3'-0"	7'-0"	1 3/4"	E	1			
522	3'-0"	7'-0"	1 3/4"	E	1			
531	3'-0"	7'-0"	1 3/4"	E	1			
532	3'-0"	7'-0"	1 3/4"	E	1			



WINDOW TYPES
1/4" = 1'-0"

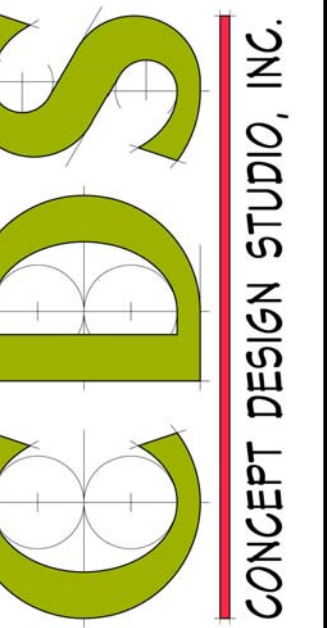


FRAME TYPES
1/4" = 1'-0"



DOOR TYPES
1/4" = 1'-0"

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PROJECT # 1307-23

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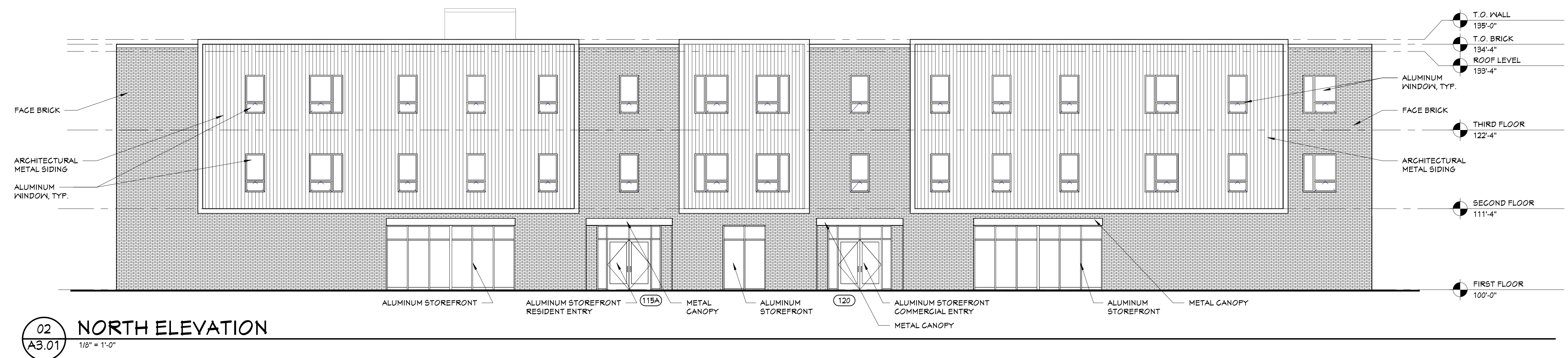
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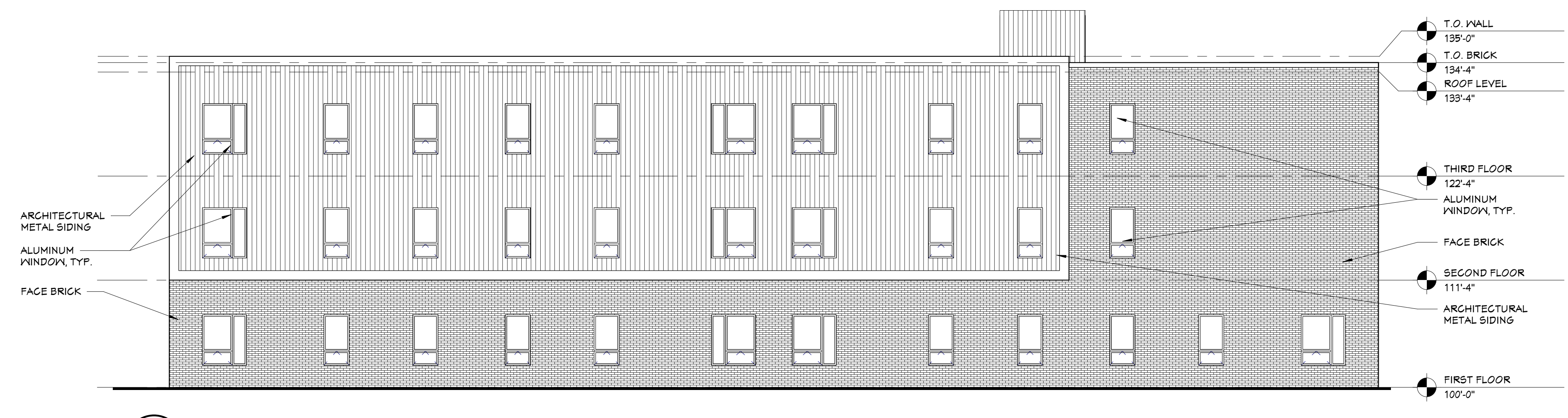
SHEET TITLE
DOOR & FRAME SCHEDULE

SHEET NO.

A2.01



02 NORTH ELEVATION
 A3.01 1/8" = 1'-0"



01 EAST ELEVATION
 A3.01 1/8" = 1'-0"

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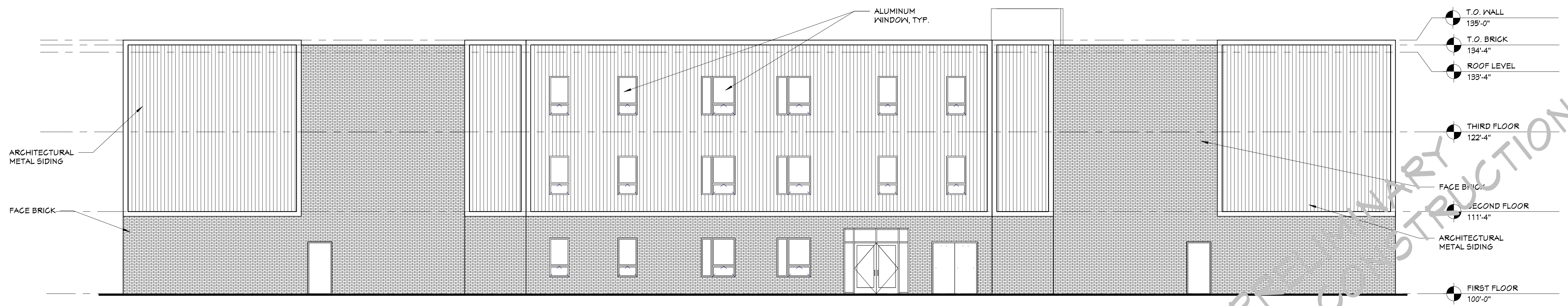
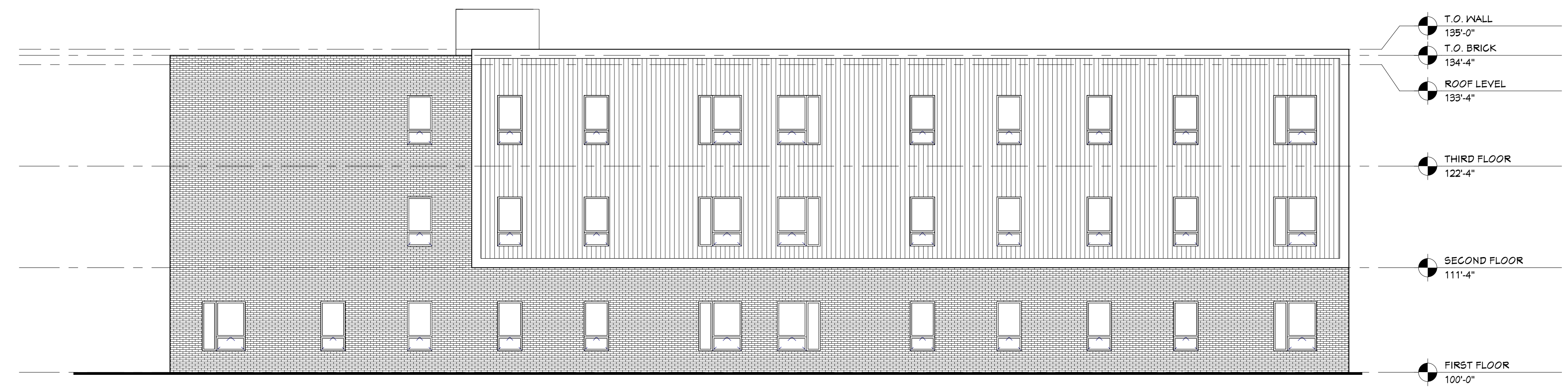
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SHEET TITLE
 WEST EXTERIOR ELEVATION & SOUTH EXTERIOR ELEVATION







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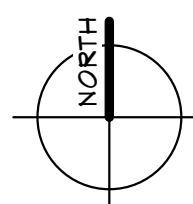
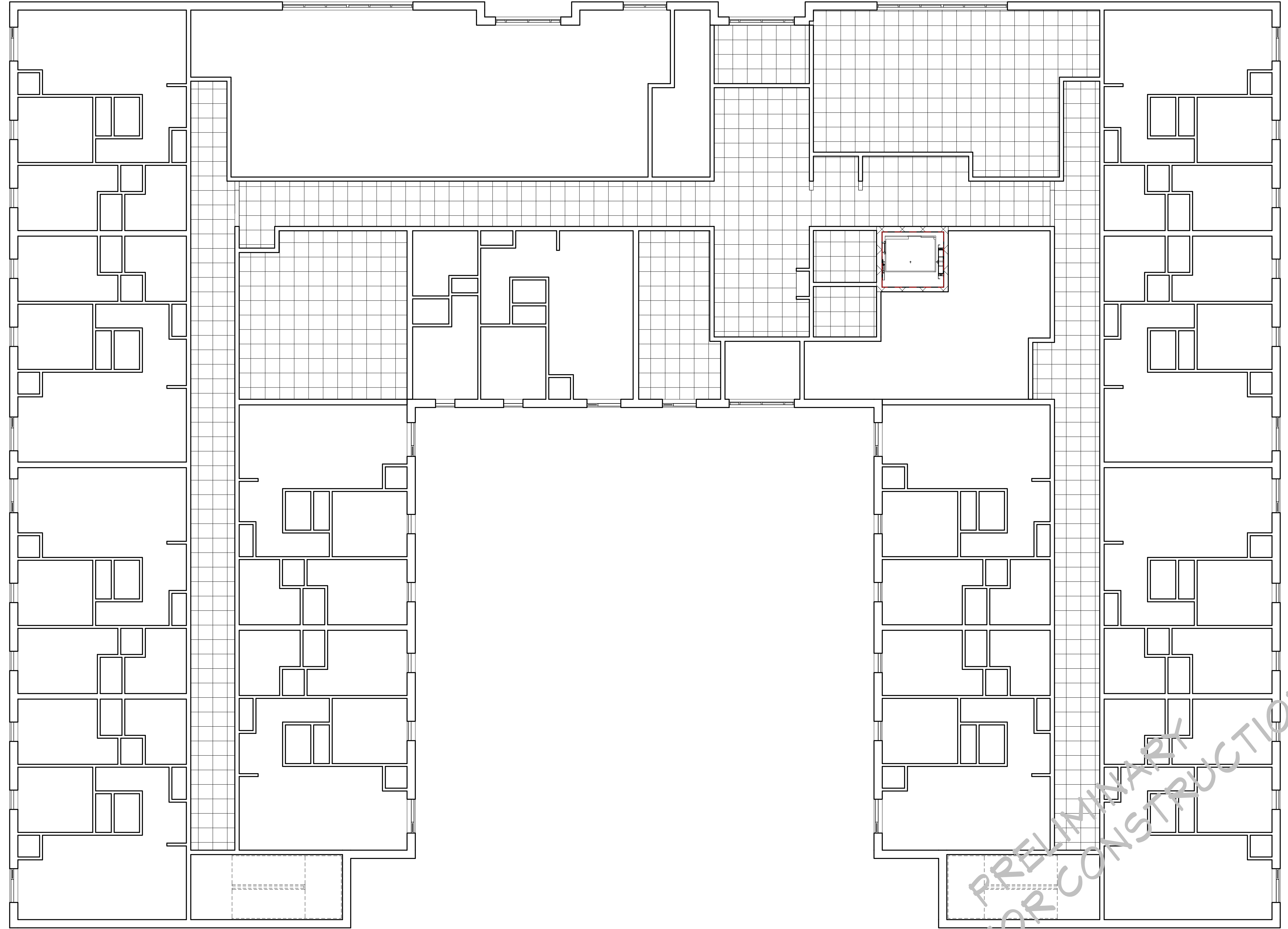
A3.02



01 SOUTH ELEVATION
 A3.02 1/8" = 1'-0"

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CEILING LEGEND	
	LINEAR STRIP LIGHT FIXTURE - REFER TO ELEC. DRAWINGS
	2x4' RECESSED LIGHT FIXTURE - REFER TO ELEC. DRAWINGS
	2x2' RECESSED LIGHT FIXTURE - REFER TO ELEC. DRAWINGS
	RECESSED CAN LIGHT FIXTURE - REFER TO ELEC. DRAWINGS
	SUPPLY GRILLE - REFER TO MECHANICAL DRAWINGS
	RETURN GRILLE - REFER TO MECHANICAL DRAWINGS

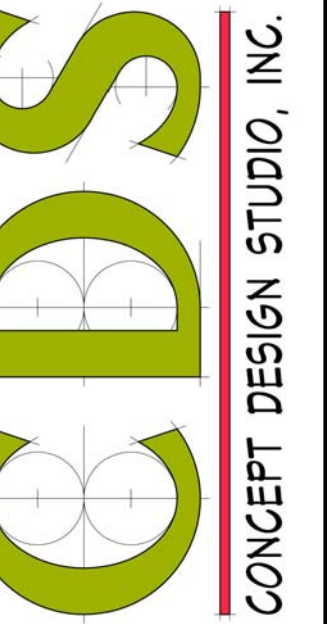


FIRST FLOOR REFLECTED CEILING PLAN

1/8" = 1'-0"

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SHEET TITLE
FIRST FLOOR REFLECTED CEILING PLAN

SHEET NO.

A8.01

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An affordable non-profit housing corporation serving Michigan's housing needs

Exhibit 1 - Project Narrative

Background

MHT Housing, Inc. (“MHT”) and its partner—St. Aloysius Parish Detroit (“SAP”)— are excited to move forward with the new construction of a three-story, mixed-use, affordable apartment community located at 60 Harper Avenue, Detroit, Michigan 48202. The property will feature 49 affordable apartment units managed by MHT’s affiliated managing agent MHT Management, LLC. In addition, the property will include roughly 1,469 square feet of commercial space facing Harper Avenue. MHT plans to target a wide range of residents in this development and has worked closely with the City of Detroit on a development that complements the neighborhood and aligns with the City’s design requirements. With MHT’s 30+ years of affordable housing development experience, MHT Management’s 15+ years of affordable management experience, and SAP’s 100+ years as a community member in the neighborhood, there will be sufficient strength and expertise to get this development completed. In looking at the landscape for those needing affordable housing options, the team feels that this housing development offers a great opportunity to provide affordable units in an area that is experiencing tremendous growth, creating a great outcome by allowing lower income wage earners to have access to amenities that they otherwise might not have. The project is consistent with the City of Detroit’s strategic targeted revitalization areas, and the development team has received support in the form of \$1,500,000 in HOME funding and a 4% Payment in Lieu of Taxes agreement from the City of Detroit (see Letter of Interest from the City of Detroit in Exhibit 8). Further, the development has received an award of 8 units of Project Based Vouchers from the Detroit Housing Commission, allowing housing to be provided to folks at the lowest income levels. MHT is submitting a 9% Low Income Housing Tax Credit application for this property in the proposed **October 2022 funding round**.

Project Location

The Property will be located at 60 Harper Avenue, Detroit, Michigan 48202, within the Medbury Park Neighborhood of Detroit. The development site will involve the combination of two adjacent sites – 60 Harper Avenue and 71 E. Edsel Ford – into one property address at 60 Harper. This location will provide access to all of the best amenities the City of Detroit has to offer for the residents living in the development. In fact, the development is within 1/3 of a mile to a Walk Score of 91, allowing it to be considered as a “walker’s paradise” area. In addition to the great walkability, there are a multitude of public transit opportunities within 1/3rd of a mile of the project location. The City of Detroit’s Q-Line has a stop just north on Woodward about 450 feet from the site as well



An affordable non-profit housing corporation serving Michigan's housing needs

as a bus stop just south on Woodward roughly 350 feet from the site. Due to its exceptional location, residents of 60 Harper Apartments will have access to a full-service grocery store, pharmacy, general practice doctor's office, clinic, and park, among other amenities. In addition to all of the amenities and opportunities that exist in this location for prospective low income residents today, there are many millions of dollars of planned future investment for this area as well, making this not only a great location to live in, but a great location to use Federal, State, and Local affordable housing resources.

Unit Mix, Amenities, & Services Offered

The apartment project will feature 49 2-bedroom units with approximately 785 square feet each. All units will feature energy efficient appliances including washers and dryers, dishwashers, frost-free refrigerators, ovens and ranges with hoods, disposals, microwaves, and central air conditioning. The development will feature a combination of private parking with an attached parking lot and street parking. Other features include building in accordance with Enterprise Green Criteria, accessible community space, 10% barrier free, 100% visitable units, and other amenities.

Of the 49 units in the project, 7 units will target families at or below 30% of the area median income ("AMI"), 7 units at or below 40% AMI, 8 units at or below 50% AMI and separately aided with Project Based Vouchers from the Detroit Housing Commission, and lastly, 27 units at or below 60% AMI. All units will be available for individuals and families regardless of their age. The wide range of incomes targeted in this development will allow for a diverse community and new housing options for families at every income level. Enclosed, in Exhibit 31, is a letter from DHC dated September 28th, 2022, confirming their award of 8 Project-Based Vouchers to the project. Please note that the Development Team is valuing the Section 8 rents using 110% of the 2023 HUD approved FMRs for Wayne County. Additional details related to the overall financing plan for the development can be found in the enclosed Primary Application.

Future residents will also benefit greatly from the availability of new services that will be made available that otherwise may not be if not for the construction of this development. Specifically, ownership has entered into an MOU to provide for the availability of the healthcare services to be provided on-site and has also received a commitment from a local workforce training center that is expanding its operations to allow for inclusion of residents of 60 Harper. Both of these service offerings will be significant for the future residents and further detail can be found in Exhibit 27.



An affordable non-profit housing corporation serving Michigan's housing needs

Planned Financing Sources

- \$11,869,228– City Real Estate Advisors (CREA) – 9% LIHTC Equity
- \$2,259,299 – Cinnaire – Permanent Mortgage
- \$1,500,000 – City of Detroit – HOME Gap Financing
- \$9,988,116 – Cinnaire – Construction Loan
- \$29,470 – Deferred Developer Fee

CREA and Cinnaire, as the investor and lender in the development respectively, have reviewed and approved of the following as a part of the development financing: 1) MHT Housing, Inc.'s financial capacity as the guarantor for the project; and 2) all proposed underwriting being used in the development, including the use of a 7% vacancy rate. Additionally, while first-floor commercial space is planned for the development, please note the Development Team is not underwriting to any commercial revenue in the proforma.

Co-Developers & Management

MHT has over 30 years of affordable housing development experience in the State of Michigan and is a 501(c)(3) nonprofit organization. MHT serves as the general or managing member of nearly 6,500 elderly and family units and has been successful in obtaining multiple funding sources for preservation, adaptive reuse, and new construction projects, that include HOME and NSP funding, Brownfield and Historic tax credits, HUD221(d)4 and Fannie Mae loans. The property will be managed by MHT's affiliated management company, MHT Management. MHT Management was formed in 2017 and has a shared service agreement with Continental Management. Continental Management was formed in 2003 and now oversees 122 affordable housing developments which are unsurpassed in terms of quality and efficiency. In fact, Continental Management was recently named Property Management Company of the Year by the Detroit Metropolitan Apartment Association in 2020 and also received the award in 2016. Continental Management has no unresolved compliance audit findings and all federally assisted properties have REAC scores that average 93.9. All management and occupancy reviews are satisfactory and above.

St. Aloysius Parish Detroit is a longstanding parish in the City of Detroit that oversees the spiritual success of St. Aloysius Catholic Church located at 1234 Washington Boulevard, Detroit, Michigan 48226 and the Our Lady of the Rosary Parish located at 5930 Woodward Avenue, Detroit, Michigan 48202. Given that the subject property is located adjacent to the Our Lady of the Rosary Parish, St. Aloysius Parish Detroit approached MHT to co-develop an affordable housing development in the City of Detroit. St. Aloysius Parish Detroit is a first time developer of



An affordable non-profit housing corporation serving Michigan's housing needs

affordable housing and hopes to continue the success of the 60 Harper Apartments into further affordable development in Wayne County. Please see Exhibit 12C for a detailed description on why St. Aloysius Parish Detroit qualifies for points under Section C.2 of MSHDA's Scoring Criteria.

Jobs Created

Based on MHT's previous experience in projects with a comparable size and scope, we estimate this project to create 75 temporary construction jobs along with at least three full-time positions within the property. These full-time positions will include a full time property manager, a part time leasing agent, a full time maintenance supervisor, and a part time maintenance technician.

Development Team

Co-Managing Member & Co-Sponsor

MHT Housing, Inc.

T. Van Fox

Ph: 248-833-0550 Email: vanfox@mhthousing.net

Co-Managing Member & Co-Sponsor

St. Aloysius Parish Detroit

Fr. Mario Amore

Ph: 313-237-5810 Email: amore.mario@staloysiusdetroit.com

General Contractor

MHT Construction, LLC

Paul Stoddard

Ph: 248-833-0550 Email: pstoddard@mhthousing.net



An affordable non-profit housing corporation serving Michigan's housing needs

Architect

Kem-Tec

Jeffrey Graham

Ph: 586-772-2222 Email: jgraham@kemtec-survey.com

Attorney

Andy Massmann

Applegate & Thorne-Thomsen, P.C.

Ph: 312-491-4433 Email: amassmann@att-law.com

Accountant

Doeren Mayhew

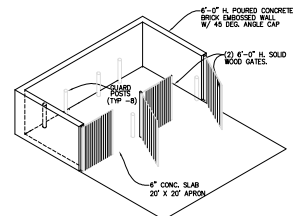
Chris Tull, CPA

248-244-3043 Email: tull@doeren.com

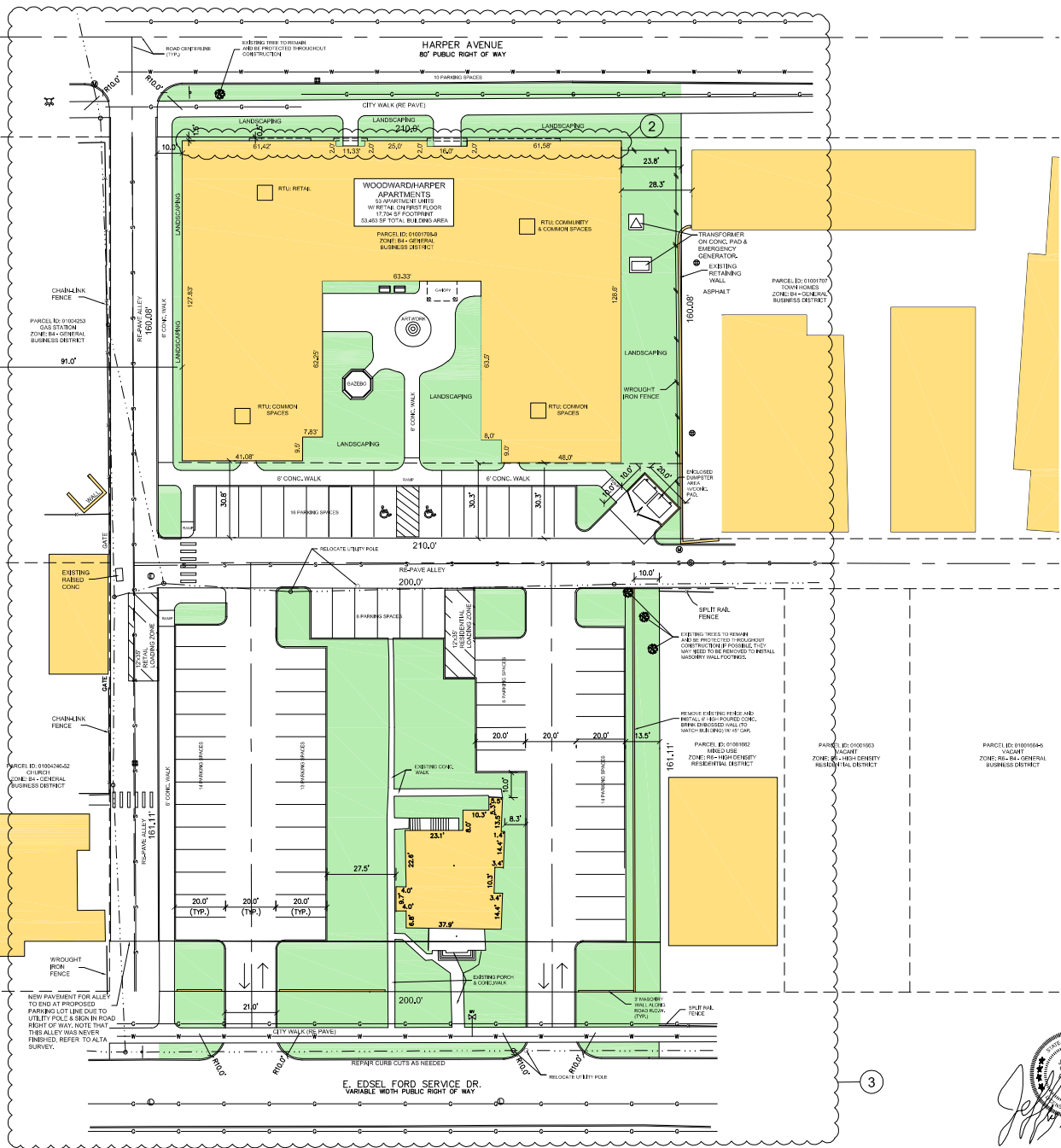
- NOTES:**
- ADA-COMPLIANT SIDEWALK WITH WARNING STRIPS MUST BE PROVIDED AT INTERSECTION CROSSWALKS AND SHALL CONFORM TO MICHIGAN DEPARTMENT OF TRANSPORTATION DETAIL R-28-L.
 - ALLEY IMPROVEMENTS WILL REQUIRE A MAINTENANCE AGREEMENT WITH THE CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS.
 - ALL UTILITIES SHOWN ARE EXISTING UNLESS OTHERWISE NOTED.
 - ALL WORK IN THE CITY RIGHT-OF-WAY SHOULD BE DONE IN ACCORDANCE WITH DPW STANDARDS AND ALL APPLICABLE DPW STANDARD DETAILS SHOULD BE INCLUDED ON THE PLANS.

LEGEND

⊙	ELECTRIC MANHOLE
⊙	ELECTRIC METER
⊙	UTILITY POLE
⊙	PUBLIC LIGHTING MANHOLE
⊙	CLEANOUT
⊙	SEWER MANHOLE
⊙	ROUND CATCH BASIN
⊙	SQUARE CATCH BASIN
⊙	FIRE HYDRANT
⊙	WATER VALVE
⊙	UNKNOWN MANHOLE
⊙	SINGLE POST SIGN
⊙	TREE (AS NOTED)
---	PARCEL BOUNDARY LINE
---	BUILDING
---	CONCRETE CURB
---	PAVEMENT
---	FENCE (AS NOTED)
---	OVERHEAD UTILITY LINE
---	GAS LINE
---	SEWER LINE
---	WATER LINE
---	BUILDING AREA



SITE PLAN
SCALE: 1"=20'



JEFFREY CRAMPTON
REGISTERED PROFESSIONAL ARCHITECT
NO. 3413

CLIENT:	MHT HOUSING, INC.
PROJECT:	WOODWARD/HARPER APARTMENTS
ADDRESS:	60 HARPER AVE
AREA:	65,838± SF OR 1.51± ACRES
DATE:	04/07/22
REVISION:	BY
DESCRIPTION:	
COMMENTS FROM CITY:	
COMMENTS FROM DPW:	
REMARKS FROM CLIENT:	
DATE:	
BY:	
DESCRIPTION:	

KEM-TEC & ASSOCIATES
PROFESSIONAL ARCHITECTS
PROFESSIONAL ENGINEERS
22556 GRATIOT AVENUE
BINGHAM FARMS, MI 48025
(586) 772-2222 PHONE
(586) 772-4048 FAX

APPLICANT:
MHT HOUSING, INC.
32000 WOODWARD AVE #402
BINGHAM FARMS, MI 48025
PHONE: 586-833-0550

DRAWN BY: ASP.1
SCALE: SEE DRAWING
DATE: 04/07/22
PROJECT: SP-00223



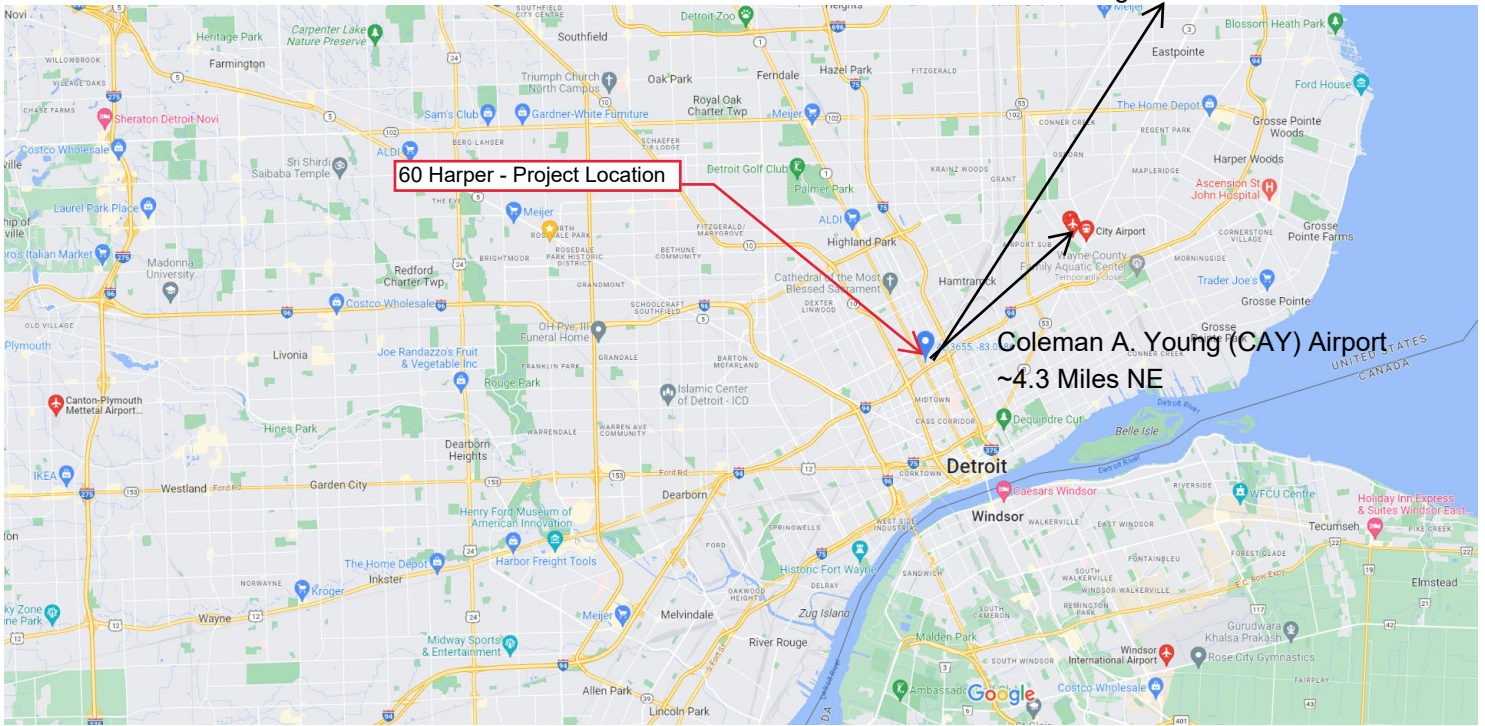
NEIGHBORHOOD SITE PLAN
NOT TO SCALE



APPLICANT: MHT HOUSING, INC. 3260 CHAMBERS AVE #403 BINGHAM FARMS, MI 48025 PHONE: 586.833-0550	PROFESSIONAL ARCHITECTS PROFESSIONAL ENGINEERS KEM-TEC & ASSOCIATES 22456 GRATIOT AVENUE BINGHAM FARMS, MI 48025 (586) 772-2222 PHONE (586) 772-4048 FAX	GENR: MHT HOUSING, INC. WOODWARD/HARPER CONDOMINIUMS ADDRESS: 60 HARPER AVE AREA: 65,638± SF OR 1.91± ACRES	REVISION: DATE BY DESCRIPTION	REVISION: DATE BY DESCRIPTION
DRAWN BY: SCALE: NOT TO SCALE	DATE: 04/07/22	DATE: 07/17/22	DATE: 07/27/22	DATE: 07/27/22
SHEET NO:	PROJECT: 22-00223			
ASP.2				

airport

Selfridge Air Base ~20 Miles NE



Map data ©2023 Google 2 mi

Rating Hours All filters

KDET Detroit City Airport
 4.2 (14)
 Airport · 11329 Conner St



Canton-Plymouth Mettetal Airport (1D2)
 4.3 (38)
 Airport · 8550 N Lilley Rd
 (734) 459-0012



Coleman A. Young International Airport
 4.7 (36)
 International airport · 11499 Conner St
 (313) 628-2146



Ray Community Airport (57D)
 4.8 (16)
 Airport · 59819 Indian Trail
 (586) 749-9558



Detroit Metropolitan Wayne County Airport
 4.3 (15,607)
 International airport
 Airport with a vibrant, musical tunnel
 (734) 247-7678



Windsor International Airport
 4.0 (297)
 International airport · 3200 County Rd 42 Unit #200
 +1 519-969-2430



Belleville Airport
 5.0 (1)
 Airport



Grosse Ile Municipal Airport (KONZ)
 4.5 (22)
 Airport · 9601 Groh Rd
 (734) 675-0155



The Project is not located within 15,000 feet of a military airport or 2,500 feet of a civilian airport.



U.S. Fish and Wildlife Service Coastal Barrier Resources System


60 Harper, Detroit, MI



May 31, 2023

 CBRS Buffer Zone
  System Unit

CBRS Units

 Otherwise Protected Area

This map is for general reference only. The Coastal Barrier Resources System (CBRS) boundaries depicted on this map are representations of the controlling CBRS boundaries, which are shown on the official maps, accessible at <https://www.fws.gov/cbra/maps/index.html>. All CBRS related data should be used in accordance with the layer metadata found on the CBRS Mapper website.

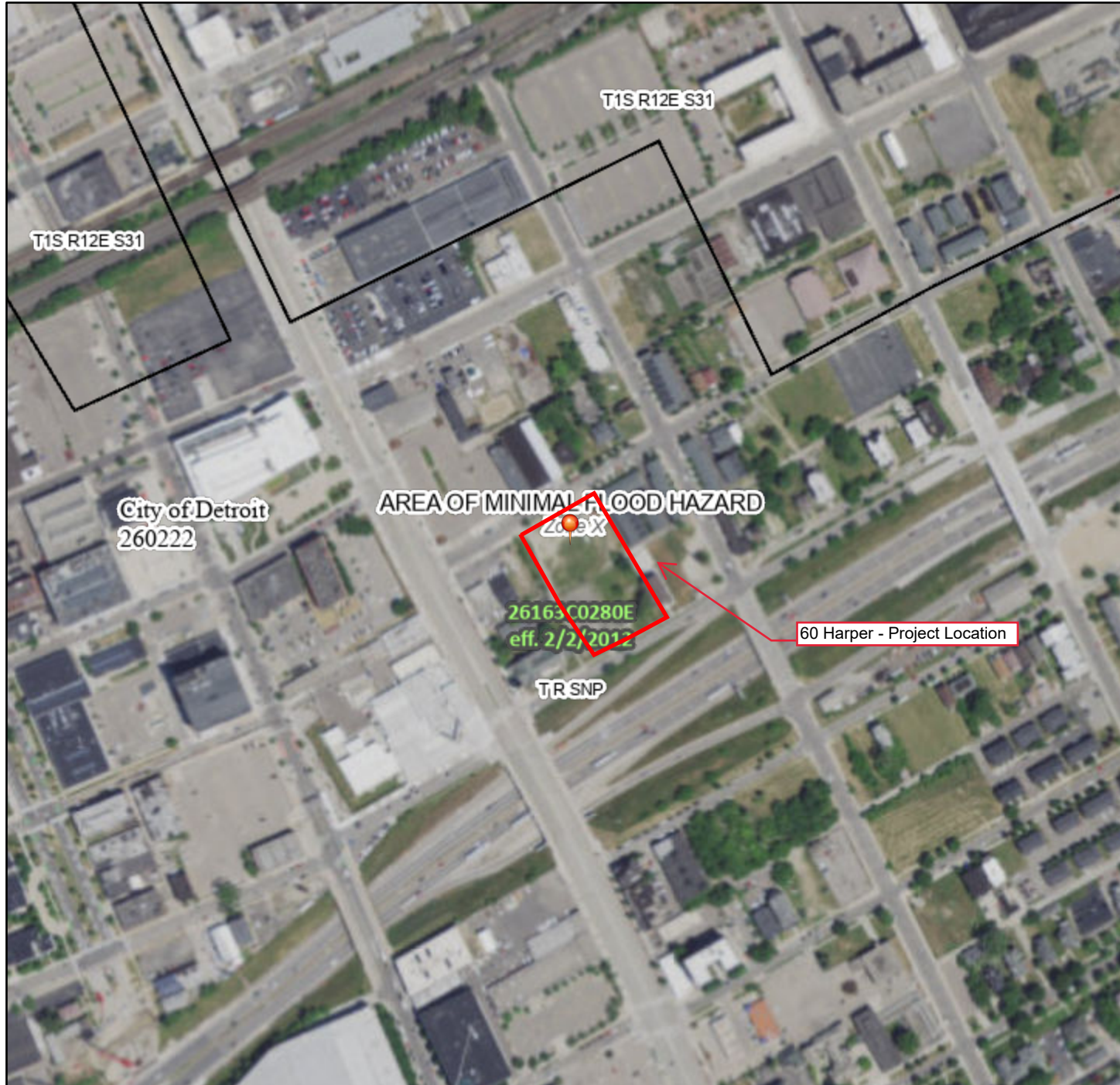
The CBRS Buffer Zone represents the area immediately adjacent to the CBRS boundary where users are advised to contact the Service for an official determination (<http://www.fws.gov/cbra/Determinations.html>) as to whether the property or project site is located "in" or "out" of the CBRS.

CBRS Units normally extend seaward out to the 20- or 30-foot bathymetric contour (depending on the location of the unit). The true seaward extent of the units is not shown in the CBRS mapper.

National Flood Hazard Layer FIRMMette



83°4'26"W 42°22'9"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

- | | | |
|------------------------------------|--|--|
| SPECIAL FLOOD HAZARD AREAS | | Without Base Flood Elevation (BFE)
<i>Zone A, V, A99</i> |
| | | With BFE or Depth <i>Zone AE, AO, AH, VE, AR</i> |
| | | Regulatory Floodway |
| OTHER AREAS OF FLOOD HAZARD | | 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile <i>Zone X</i> |
| | | Future Conditions 1% Annual Chance Flood Hazard <i>Zone X</i> |
| | | Area with Reduced Flood Risk due to Levee. See Notes. <i>Zone X</i> |
| | | Area with Flood Risk due to Levee <i>Zone D</i> |
| OTHER AREAS | | NO SCREEN Area of Minimal Flood Hazard <i>Zone X</i> |
| | | Effective LOMRs |
| GENERAL STRUCTURES | | Area of Undetermined Flood Hazard <i>Zone D</i> |
| | | Channel, Culvert, or Storm Sewer |
| | | Levee, Dike, or Floodwall |
| OTHER FEATURES | | 20.2 Cross Sections with 1% Annual Chance |
| | | 17.5 Water Surface Elevation |
| | | Coastal Transect |
| | | Base Flood Elevation Line (BFE) |
| | | Limit of Study |
| MAP PANELS | | Jurisdiction Boundary |
| | | Coastal Transect Baseline |
| | | Profile Baseline |
| | | Hydrographic Feature |
| | | Digital Data Available |
| | | No Digital Data Available |
| | | Unmapped |
| | | The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location. |



0 250 500 1,000 1,500 2,000 Feet 1:6,000

83°3'48"W 42°21'42"N

Basemap: USGS National Map: Orthoimagery: Data refreshed October, 2020

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **6/16/2022 at 2:36 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



GRETCHEN WHITMER
GOVERNOR

STATE OF MICHIGAN
DEPARTMENT OF
ENVIRONMENT, GREAT LAKES, AND ENERGY
LANSING



AARON B. KEATLEY
ACTING DIRECTOR

June 2, 2023

Julie Pratt
Environmental Consulting Solutions
523 West Sunnybrook Drive
Royal Oak, Michigan 48073

Dear Julie Pratt:

Subject: 60 Harper Project in Detroit, Michigan

The Michigan Department of Environment, Great Lakes, and Energy (EGLE), Air Quality Division (AQD), has reviewed the federal regulations related to general conformity of projects with state implementation plans (SIP) for air quality. In particular, 40 Code of Federal Regulations (CFR) Section 93.150 et seq, which states that any federally funded project in a nonattainment or maintenance area must conform to the Clean Air Act requirements, including the State's SIP, if they may constitute a significant new source of air pollution.

On August 3, 2018, Wayne County was designated nonattainment for the 2015 ozone standard; and thus, general conformity must be evaluated when completing construction projects of a given size and scope. EGLE completed the required SIP submittals for this area and on May 19, 2023, the United States Environmental Protection Agency (USEPA) redesignated the 7-county Southeast Michigan area (including Wayne County) from nonattainment to attainment/maintenance. General conformity does, however, still require an evaluation during the maintenance period. For his evaluation, EGLE considered the following information from the USEPA general conformity guidance, which states "historical analysis of similar actions can be used in cases where the proposed projects are similar in size and scope to previous projects."

EGLE has reviewed the project proposed to be located at 60 Harper Avenue in Detroit, Michigan, which is to be completed with federal grant monies, including the construction of a 3-story, mixed use, affordable apartment community with a 17,704 square foot (ft²) footprint and 53,463 ft² in total building area. The project does not yet have a set construction schedule but is expected to tentatively begin in the fall of 2023 and will be completed by December 2024.

In reviewing the "*Air Quality and Greenhouse Gas Study: Uptown Orange Apartments in Orange, California*," dated December 2012, prepared for KTG Y Group, Inc. by UltraSystems Environmental, Inc., it was determined that emission levels for the project were below the de minimis levels for general conformity.

Julie Pratt
Page 2
June 2, 2023

The Uptown Orange Apartments project and related parking structure construction was estimated to take 33 months to complete, would encompass an area of 5.57 acres, and included two four-story residential units with a total of 334 apartments, and two parking structures with a total of 494 and 679 parking stalls, respectively.

The size, scope, and duration of the proposed 60 Harper mixed use apartment community project is much smaller in scale than the Uptown Orange Apartments project described above and should not exceed the de minimis levels included in the federal general conformity requirements. Therefore, it does not require a detailed conformity analysis.

If you have any further questions regarding this matter, please contact me at 517-648-6314; BukowskiB@Michigan.gov; or EGLE, AQD, P.O. Box 30260, Lansing, Michigan 48909-7760.

Sincerely,

A handwritten signature in blue ink that reads "Breanna Bukowski".

Breanna Bukowski
Environmental Quality Analyst

cc: Michael Leslie, USEPA Region 5
Kim Siegel, City of Detroit

Attainment Status for the National Ambient Air Quality Standards

The National Ambient Air Quality Standards (NAAQS) are health-based pollution standards set by EPA.

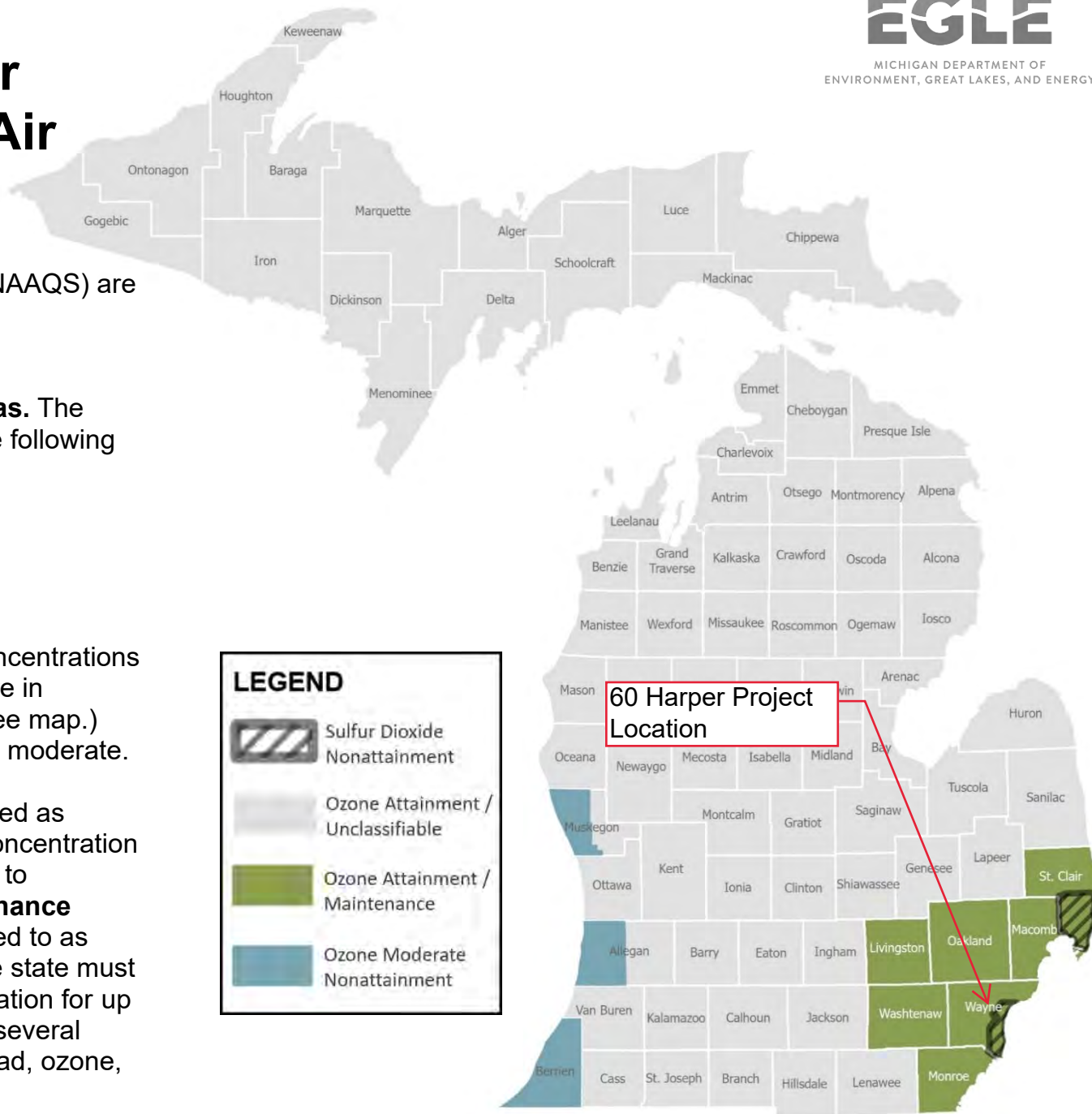
Areas of the state that are below the NAAQS concentration level are called **attainment areas**. The entire state of Michigan is in attainment for the following pollutants:

- Carbon Monoxide (CO)
- Lead (Pb)
- Nitrogen Dioxide (NO₂)
- Particulate Matter (PM₁₀ & PM_{2.5})

Nonattainment areas are those that have concentrations over the NAAQS level. Portions of the state are in nonattainment for sulfur dioxide and ozone (see map.) The ozone nonattainment area is classified as moderate.

Areas of the state that were previously classified as nonattainment but have since reduced their concentration levels below the NAAQS can be redesignated to attainment and are called **attainment/maintenance areas**. These areas are also commonly referred to as “attainment” after reclassification, however the state must continue monitoring and submitting documentation for up to 20 years after the redesignated. There are several maintenance areas throughout the state for lead, ozone, and particulate matter.

**For readability purposes the map only includes the most recently reclassified ozone maintenance area in southeast Michigan. For more information, please consult the Michigan.gov/AIR webpage or contact the division directly.*



LEGEND

- Sulfur Dioxide Nonattainment
- Ozone Attainment / Unclassifiable
- Ozone Attainment / Maintenance
- Ozone Moderate Nonattainment

**See Page 2 for close-up maps of partial county nonattainment areas.*

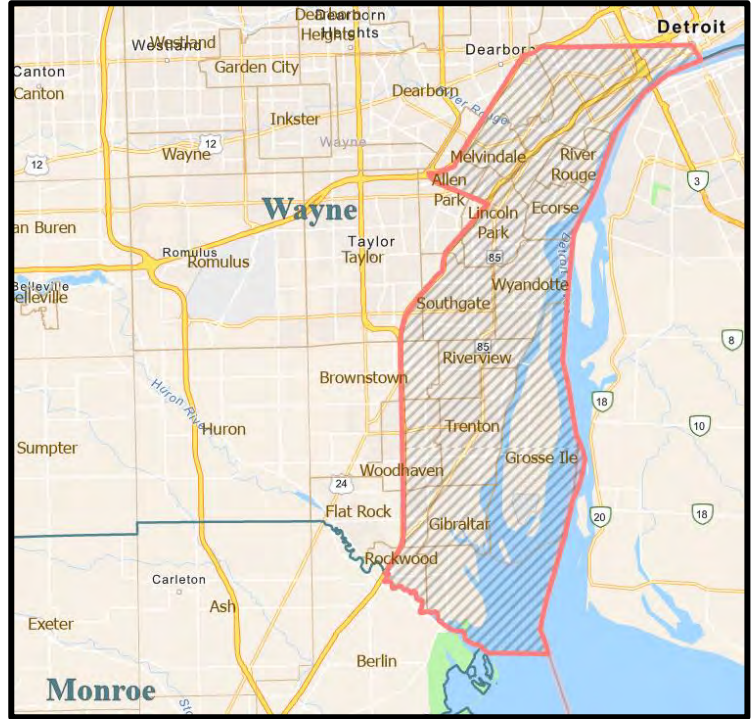
Close-Up Maps of Partial County Nonattainment Areas

Sulfur Dioxide Nonattainment Areas

St. Clair County



Wayne County



Ozone Moderate Nonattainment Areas

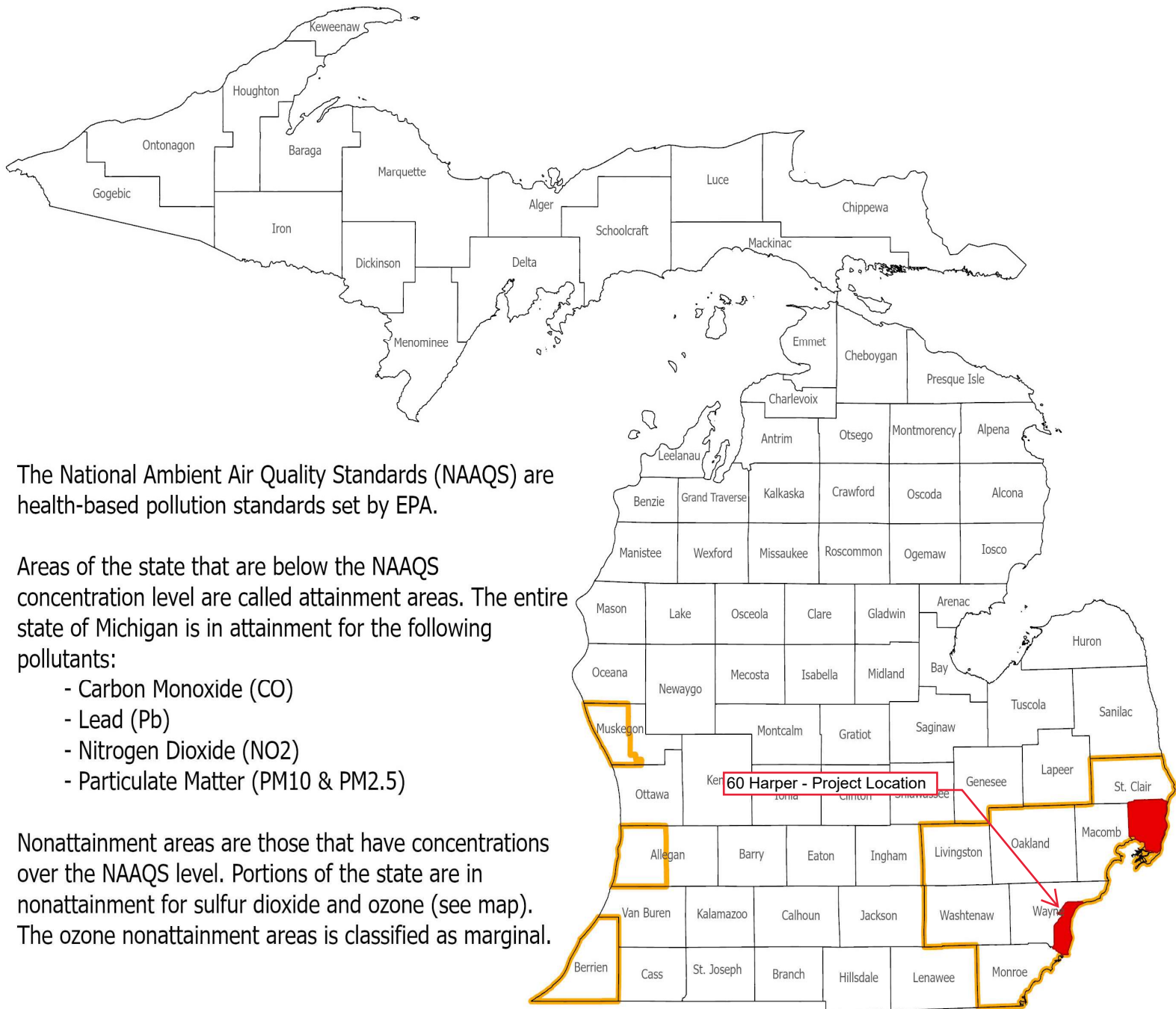
Allegan County



Muskegon County



Attainment Status for the National Ambient Air Quality Standards



The National Ambient Air Quality Standards (NAAQS) are health-based pollution standards set by EPA.

Areas of the state that are below the NAAQS concentration level are called attainment areas. The entire state of Michigan is in attainment for the following pollutants:

- Carbon Monoxide (CO)
- Lead (Pb)
- Nitrogen Dioxide (NO₂)
- Particulate Matter (PM₁₀ & PM_{2.5})

Nonattainment areas are those that have concentrations over the NAAQS level. Portions of the state are in nonattainment for sulfur dioxide and ozone (see map). The ozone nonattainment areas is classified as marginal.

LEGEND

- Sulfur Dioxide Nonattainment Area
- Ozone Nonattainment Area

See Page 2 for close-up maps of partial county nonattainment areas.

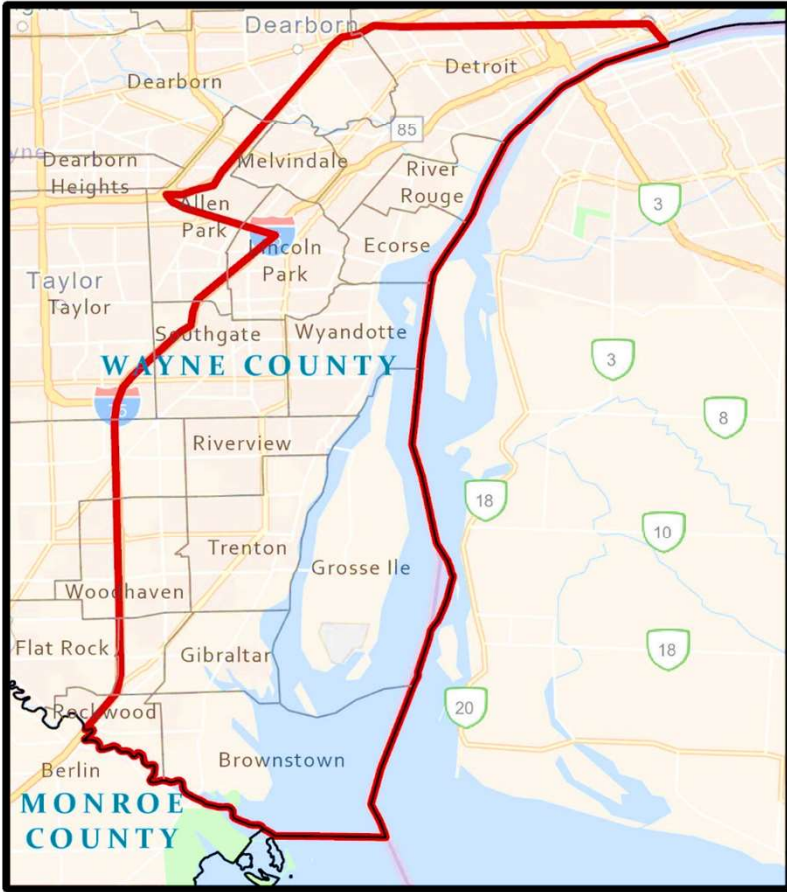
Updated February 5, 2021

Prepared by EGLE, Air Quality Division, State Implementation Plan Development Unit

Close-Up Maps of Partial County Nonattainment Areas

Sulfur Dioxide Nonattainment Areas

Wayne County Area



St. Clair County Area

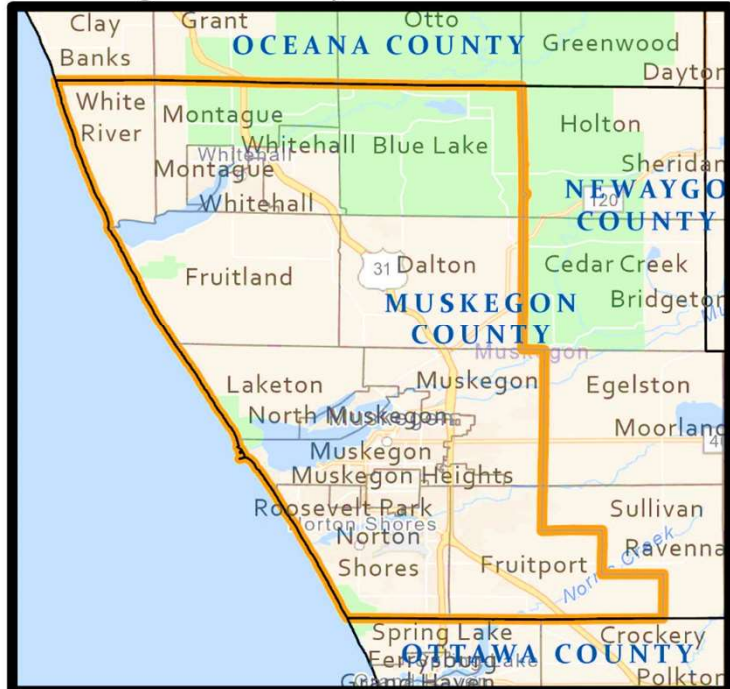


Ozone Nonattainment Areas

Allegan County Area



Muskegon County Area



Wayne County
Grosse Pointe Township, Grosse Pointe Woods, Grosse Pointe Farms
Grosse Pointe, Grosse Pointe Park, and Detroit, T1S R14E
Detroit, T1S R14E, T2S R13E, and T2S R12E
River Rouge, T2S R11E

The heavy red line is the **Coastal Zone Management Boundary**
The red hatched area is the **Coastal Zone Management Area**.





GRETCHEN WHITMER
GOVERNOR

STATE OF MICHIGAN
DEPARTMENT OF
ENVIRONMENT, GREAT LAKES, AND ENERGY
LANSING



PHILLIP D. ROOS
DIRECTOR

August 17, 2023

VIA EMAIL

T. Van Fox
60 Harper Limited Dividend Housing Association, LLC
32500 Telegraph Road, Suite 100
Bingham Farms, Michigan 48025

Dear T. Van Fox:

SUBJECT: Notice of Approval of Response Activity Plan to Comply with 20107a(1)(b)
60 Harper Apartments
60 Harper Avenue, Detroit, Wayne County, Michigan
Property Tax ID No.: 01001708-9 and 01001630-61
Facility/Site ID No: 82008916

The Department of Environment, Great Lakes, and Energy (EGLE), Remediation and Redevelopment Division (RRD), has reviewed the Response Activity Plan to comply with Section 20107a(1)(b) of Part 201, Environmental Remediation, of the Natural Resources and Environmental Protection Act, 1994 PA 451, as amended (NREPA). The Response Activity Plan outlines the response activities to be undertaken at the property identified as 60 Harper Avenue, Detroit, Wayne County, Michigan. It was submitted by April Hehir, Hamp, Mathews & Associates, Inc. on the behalf of T. Van Fox, 60 Harper Limited Dividend Housing Association, LLC and received by EGLE on July 6, 2023. The Response Activity Plan was submitted pursuant to Section 20114b of the NREPA and based upon representations and information contained in the submittal, the Response Activity Plan is approved.

This approval is specific to Section 8.0 of the Response Activity Plan to comply with Section 20107a(1)(b) of the NREPA to address unacceptable exposures via the direct contact and volatilization to indoor air pathways and is based upon the representations and information contained in the submittal; therefore, EGLE expresses no opinion as to whether other conditions that may exist will be adequately addressed by the response activities that are proposed in the plan.

The owner and operator of this property may also have responsibility under applicable state and federal laws, including, but not limited to Part 201, Environmental Remediation; Part 111, Hazardous Waste Management; Part 211, Underground Storage Tank Regulations; Part 213, Leaking Underground Storage Tanks; Part 615, Supervisor of Wells, of the NREPA; and the Michigan Fire Prevention Code, 1941 PA 207, as amended.

This approval is pursuant to the applicable requirements of the NREPA. The Michigan State Housing Development Authority (MSHDA) may have additional site selection requirements beyond the NREPA statutory obligations for site characterization and remedial actions or response activities necessary to prevent, minimize, or mitigate injury to the public health, safety, or welfare, or to the environment.

If you should have further questions or concerns, please contact Jolene Melchiori, RRD, Brownfield Assessment and Redevelopment Section, at 906-250-2076 or email at MelchioriJ@michigan.gov.

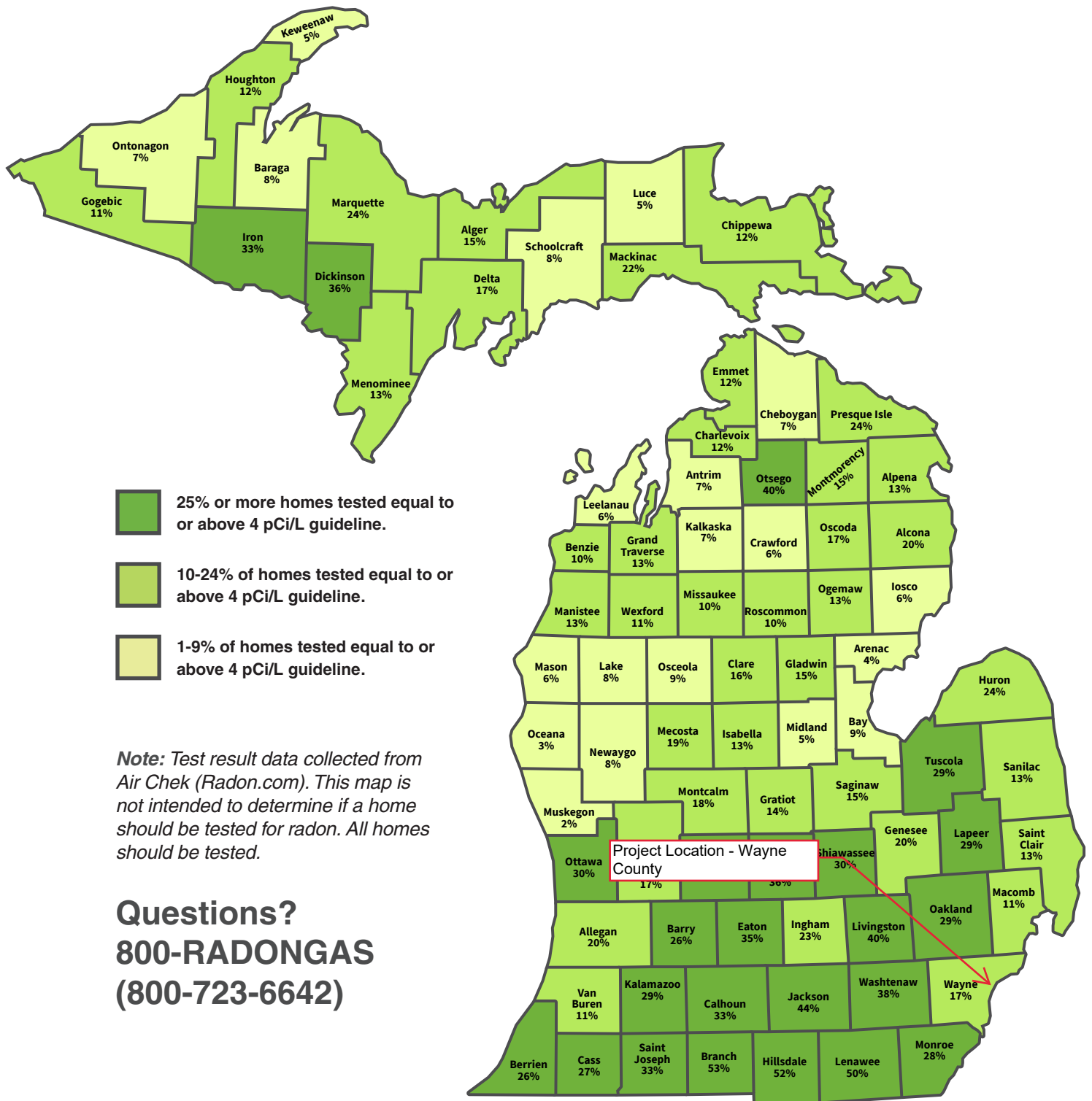
Sincerely,

A handwritten signature in black ink that reads "Carrie A. Geyer". The signature is fluid and cursive, with the first letters of each word being capitalized and prominent.

Carrie Geyer
Manager
Brownfield Assessment and Redevelopment
Section
Remediation and Redevelopment Division
GeyerC1@Michigan.gov

cc: April Hehir, Hamp, Mathews and Associates, Inc.

Percentage of Elevated Radon Test Results by County



- 25% or more homes tested equal to or above 4 pCi/L guideline.
- 10-24% of homes tested equal to or above 4 pCi/L guideline.
- 1-9% of homes tested equal to or above 4 pCi/L guideline.

Note: Test result data collected from Air Chek (Radon.com). This map is not intended to determine if a home should be tested for radon. All homes should be tested.

Questions?
800-RADONGAS
(800-723-6642)



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Michigan Ecological Services Field Office
2651 Coolidge Road Suite 101
East Lansing, MI 48823-6360
Phone: (517) 351-2555 Fax: (517) 351-1443

In Reply Refer To:
Project Code: 2023-0127021
Project Name: 60 Harper, Detroit

September 11, 2023

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

Official Species List

The attached species list identifies any Federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Under 50 CFR 402.12(e) (the regulations that implement section 7 of the Endangered Species Act), the accuracy of this species list should be verified after 90 days. You may verify the list by visiting the IPaC website (<https://ipac.ecosphere.fws.gov/>) at regular intervals during project planning and implementation. To update an Official Species List in IPaC: from the My Projects page, find the project, expand the row, and click Project Home. In the What's Next box on the Project Home page, there is a Request Updated List button to update your species list. Be sure to select an "official" species list for all projects.

Consultation requirements and next steps

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize Federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-Federal representative) must consult with the Fish and Wildlife Service if they determine their project may affect listed species or critical habitat.

There are two approaches to evaluating the effects of a project on listed species.

Approach 1. Use the All-species Michigan determination key in IPaC. This tool can assist you in making determinations for listed species for some projects. In many cases, the determination key

will provide an automated concurrence that completes all or significant parts of the consultation process. Therefore, we strongly recommend screening your project with the **All-Species Michigan Determination Key (Dkey)**. For additional information on using IPaC and available Determination Keys, visit <https://www.fws.gov/media/mifo-ipac-instructions> (and click on the attachment). Please carefully review your Dkey output letter to determine whether additional steps are needed to complete the consultation process.

Approach 2. Evaluate the effects to listed species on your own without utilizing a determination key. Once you obtain your official species list, you are not required to continue in IPaC, although in most cases using a determination key should expedite your review. If the project is a Federal action, you should review our section 7 step-by-step instructions before making your determinations: <https://www.fws.gov/office/midwest-region-headquarters/midwest-section-7-technical-assistance>. If you evaluate the details of your project and conclude “no effect,” document your findings, and your listed species review is complete; you do not need our concurrence on “no effect” determinations. If you cannot conclude “no effect,” you should coordinate/consult with the Michigan Ecological Services Field Office. The preferred method for submitting your project description and effects determination (if concurrence is needed) is electronically to EastLansing@fws.gov. Please include a copy of this official species list with your request.

For all **wind energy projects** and **projects that include installing communications towers >450 feet that use guy wires**, please contact this field office directly for assistance, even if no Federally listed plants, animals or critical habitat are present within your proposed project area or may be affected by your proposed project.

Migratory Birds

Please see the “Migratory Birds” section below for important information regarding incorporating migratory birds into your project planning. Our Migratory Bird Program has developed recommendations, best practices, and other tools to help project proponents voluntarily reduce impacts to birds and their habitats. The Bald and Golden Eagle Protection Act prohibits the take and disturbance of eagles without a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <https://www.fws.gov/program/eagle-management/eagle-permits> to help you avoid impacting eagles or determine if a permit may be necessary.

Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your consideration of threatened and endangered species during your project

planning. Please include a copy of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List
- USFWS National Wildlife Refuges and Fish Hatcheries
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Michigan Ecological Services Field Office

2651 Coolidge Road Suite 101

East Lansing, MI 48823-6360

(517) 351-2555

PROJECT SUMMARY

Project Code: 2023-0127021
Project Name: 60 Harper, Detroit
Project Type: Federal Grant / Loan Related
Project Description: Redevelopment of currently vacant land with residential multi-family housing and commercial space. The Project will include 49 residential apartment units and ~1,500 sq. ft. of commercial space fronting Harper Avenue.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@42.365214949999995,-83.0685251153622,14z>



Counties: Wayne County, Michigan

ENDANGERED SPECIES ACT SPECIES

There is a total of 8 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 2 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 General project design guidelines: https://ipac.ecosphere.fws.gov/project/FXJF5KLSLBH53AMN2SQ3XEFEAI/documents/generated/6982.pdf	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Piping Plover <i>Charadrius melodus</i> Population: [Great Lakes watershed DPS] - Great Lakes, watershed in States of IL, IN, MI, MN, NY, OH, PA, and WI and Canada (Ont.) There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/6039	Endangered
Red Knot <i>Calidris canutus rufa</i> There is proposed critical habitat for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> Only actions that occur along coastal areas during the Red Knot migratory window of MAY 1 - SEPTEMBER 30. Species profile: https://ecos.fws.gov/ecp/species/1864	Threatened

REPTILES

NAME	STATUS
Eastern Massasauga (=rattlesnake) <i>Sistrurus catenatus</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> For all Projects: Project is within EMR Range Species profile: https://ecos.fws.gov/ecp/species/2202 General project design guidelines: https://ipac.ecosphere.fws.gov/project/FXJF5KLSLBH53AMN2SQ3XEFEAI/documents/generated/5280.pdf	Threatened

CLAMS

NAME	STATUS
Northern Riffleshell <i>Epioblasma rangiana</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/527	Endangered

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

FLOWERING PLANTS

NAME	STATUS
Eastern Prairie Fringed Orchid <i>Platanthera leucophaea</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/601	Threatened

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

USFWS NATIONAL WILDLIFE REFUGE LANDS AND FISH HATCHERIES

Any activity proposed on lands managed by the [National Wildlife Refuge](#) system must undergo a 'Compatibility Determination' conducted by the Refuge. Please contact the individual Refuges to discuss any questions or concerns.

THERE ARE NO REFUGE LANDS OR FISH HATCHERIES WITHIN YOUR PROJECT AREA.

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

The birds listed below are birds of particular concern either because they occur on the [USFWS Birds of Conservation Concern \(BCC\)](#) list or warrant special attention in your project location. To learn more about the levels of concern for birds on your list and how this list is generated, see the FAQ [below](#). This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the [E-bird data mapping tool](#) (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found [below](#).

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE

SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Canada Warbler <i>Cardellina canadensis</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 20 to Aug 10
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Mar 15 to Aug 25
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA	Breeds elsewhere
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is $0.25/0.25 = 1$; at week 20 it is $0.05/0.25 = 0.2$.

- The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

Breeding Season (■)

Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)

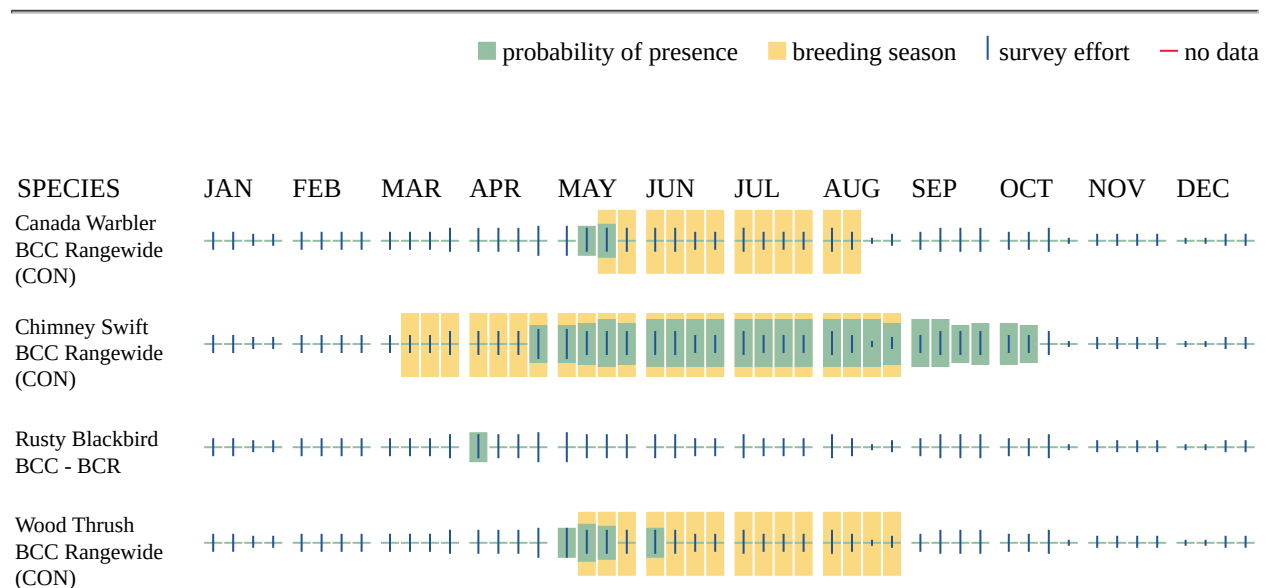
Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

No Data (-)

A week is marked as having no data if there were no survey events for that week.

Survey Timeframe

Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.



Additional information can be found using the following links:

- Birds of Conservation Concern <https://www.fws.gov/program/migratory-birds/species>
 - Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
 - Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
-

MIGRATORY BIRDS FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the list of migratory birds that potentially occur in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [Rapid Avian Information Locator \(RAIL\) Tool](#).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering or migrating in my area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may query your location using the [RAIL Tool](#) and look at the range maps provided for birds in your area at the bottom of the profiles provided for each bird in your results. If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point

within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

What if I have eagles on my list?

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no

data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

THERE ARE NO WETLANDS WITHIN YOUR PROJECT AREA.

IPAC USER CONTACT INFORMATION

Agency: ECS

Name: Julie Pratt

Address: 523 W. Sunnybrook Drive

City: Royal Oak

State: MI

Zip: 48034

Email: jpratt@environmentalconsultingsolutions.com

Phone: 5864247355



analytical data, the west adjoining gasoline filling station at 5996 Woodward Avenue is not a VEC to the Subject Property.

9.10 Aboveground Storage Tanks

HMA utilized the EDR Database Report, Google Earth™ aerial imagery, and observations from the site reconnaissance to evaluate the presence of ASTs in the vicinity of the Subject Property. As discussed in Section 6.3.1 of this report, one AST was observed at the eastern edge of the Subject Property within the electrical transmission utility easement. The AST appeared as an approximately 500 to 1,000-gallon pressurized, steel AST for propane use. The contents and age of the AST could not be verified; however, the AST appeared out of use at the time of the site reconnaissance. HMA confirmed with the User that this AST will be removed prior to development activities, thus, this AST does not represent a fire or explosion hazard to the proposed development.

Based on the EDR Database Report, aerial imagery, and/or observations from the site reconnaissance, the following active ASTs were identified within the vicinity of the Subject Property:

NEARBY ASTs				
Site Name	Address	Tank Information	Distance/Direction	Largest ASD
Wayne State University Computing Service Center	5925 Woodward Avenue	8,000-gallons of Diesel	325 feet West	657.70 feet
City of Detroit Public Works	5740 Russell Street	Suspected 500,000-gallons of Flammable Liquid	3,800 feet East	3,682.91 feet

HMA utilized the HUD electronic Acceptable Separation Distance (ASD) assessment tool to calculate the acceptable separation distance (ASD). The 8,000-gallon AST related to the Wayne State University Computing Service Center was identified within the ASD. However, this AST was identified by the EDR Database Report; HMA was not able to visually locate the AST during the site reconnaissance or with aerial imagery. Therefore, HMA believes it is unlikely that this 8,000-gallon diesel AST is truly located within the ASD. Lastly, the AST at the City of Detroit Public Works is located beyond the ASD. A copy of the ASD documentation is included in **Appendix 10.7F**.

9.11 Drinking Water Quality

Compliance with Michigan’s revised Lead and Copper rule (EGLE) is required by MSHDA for all municipally supplied and Type I Community Water supply systems. HMA reviewed the Water Supply Lead Results provided by the Michigan Department of Health and Human Services. Based on the results, the City of Detroit (Public Water Supply ID MI0001800) last monitoring event ended on December 31, 2022. The lead 90th percentile was reported as 12 parts per billion (ppb), which is below the Action Level Exceedance (ALE)

[Home \(/\)](#) > [Programs \(/programs/\)](#) > [Environmental Review \(/programs/environmental-review/\)](#) > ASD Calculator

Acceptable Separation Distance (ASD) Electronic Assessment Tool

The Environmental Planning Division (EPD) has developed an electronic-based assessment tool that calculates the Acceptable Separation Distance (ASD) from stationary hazards. The ASD is the distance from above ground stationary containerized hazards of an explosive or fire prone nature, to where a HUD assisted project can be located. The ASD is consistent with the Department's standards of blast overpressure (0.5 psi-buildings) and thermal radiation (450 BTU/ft² - hr - people and 10,000 BTU/ft² - hr - buildings). Calculation of the ASD is the first step to assess site suitability for proposed HUD-assisted projects near stationary hazards. Additional guidance on ASDs is available in the Department's guidebook "Siting of HUD- Assisted Projects Near Hazardous Facilities" and the regulation 24 CFR Part 51, Subpart C, Siting of HUD-Assisted Projects Near Hazardous Operations Handling Conventional Fuels or Chemicals of an Explosive or Flammable Nature.

Note: Tool tips, containing field specific information, have been added in this tool and may be accessed by hovering over the ASD result fields with the mouse.

Acceptable Separation Distance Assessment Tool

Is the container above ground?	Yes: <input checked="" type="checkbox"/> No: <input type="checkbox"/>
Is the container under pressure?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>
Does the container hold a cryogenic liquified gas?	Yes: <input type="checkbox"/> No: <input type="checkbox"/>
Is the container diked?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>
What is the volume (gal) of the container?	<input type="text" value="500000"/>
What is the Diked Area Length (ft)?	<input type="text"/>
What is the Diked Area Width (ft)?	<input type="text"/>
<input type="button" value="Calculate Acceptable Separation Distance"/>	
Diked Area (sqft)	<input type="text"/>
ASD for Blast Over Pressure (ASDBOP)	<input type="text"/>
ASD for Thermal Radiation for People (ASDPPU)	<input type="text" value="3682.91"/>
ASD for Thermal Radiation for Buildings (ASDBPU)	<input type="text" value="889.48"/>
ASD for Thermal Radiation for People (ASDPNPD)	<input type="text"/>
ASD for Thermal Radiation for Buildings (ASDBNPD)	<input type="text"/>

For mitigation options, please click on the following link: [Mitigation Options \(/resource/3846/acceptable-separation-distance-asd-hazard-mitigation-options/\)](#)

Providing Feedback & Corrections

After using the ASD Assessment Tool following the directions in this User Guide, users are encouraged to provide feedback on how the ASD Assessment Tool may be improved. Users are also encouraged to send comments or corrections for the improvement of the tool.

Please send comments or other input using the **Contact Us** (<https://www.hudexchange.info/contact-us/>) form.

[Home \(/\)](#) > [Programs \(/programs/\)](#) > [Environmental Review \(/programs/environmental-review/\)](#) > ASD Calculator

Acceptable Separation Distance (ASD) Electronic Assessment Tool

The Environmental Planning Division (EPD) has developed an electronic-based assessment tool that calculates the Acceptable Separation Distance (ASD) from stationary hazards. The ASD is the distance from above ground stationary containerized hazards of an explosive or fire prone nature, to where a HUD assisted project can be located. The ASD is consistent with the Department's standards of blast overpressure (0.5 psi-buildings) and thermal radiation (450 BTU/ft² - hr - people and 10,000 BTU/ft² - hr - buildings). Calculation of the ASD is the first step to assess site suitability for proposed HUD-assisted projects near stationary hazards. Additional guidance on ASDs is available in the Department's guidebook "Siting of HUD- Assisted Projects Near Hazardous Facilities" and the regulation 24 CFR Part 51, Subpart C, Siting of HUD-Assisted Projects Near Hazardous Operations Handling Conventional Fuels or Chemicals of an Explosive or Flammable Nature.

Note: Tool tips, containing field specific information, have been added in this tool and may be accessed by hovering over the ASD result fields with the mouse.

Acceptable Separation Distance Assessment Tool

Is the container above ground?	Yes: <input checked="" type="checkbox"/> No: <input type="checkbox"/>
Is the container under pressure?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>
Does the container hold a cryogenic liquified gas?	Yes: <input type="checkbox"/> No: <input type="checkbox"/>
Is the container diked?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>
What is the volume (gal) of the container?	<input type="text" value="8000"/>
What is the Diked Area Length (ft)?	<input type="text"/>
What is the Diked Area Width (ft)?	<input type="text"/>
<input type="button" value="Calculate Acceptable Separation Distance"/>	
Diked Area (sqft)	<input type="text"/>
ASD for Blast Over Pressure (ASDBOP)	<input type="text"/>
ASD for Thermal Radiation for People (ASDPPU)	<input type="text" value="657.70"/>
ASD for Thermal Radiation for Buildings (ASDBPU)	<input type="text" value="131.49"/>
ASD for Thermal Radiation for People (ASDPNPD)	<input type="text"/>
ASD for Thermal Radiation for Buildings (ASDBNPD)	<input type="text"/>

For mitigation options, please click on the following link: [Mitigation Options \(/resource/3846/acceptable-separation-distance-asd-hazard-mitigation-options/\)](/resource/3846/acceptable-separation-distance-asd-hazard-mitigation-options/)

Providing Feedback & Corrections

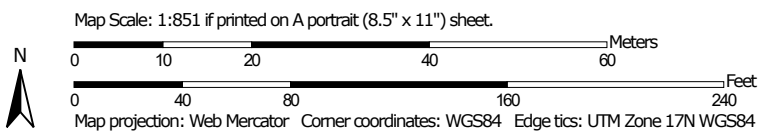
After using the ASD Assessment Tool following the directions in this User Guide, users are encouraged to provide feedback on how the ASD Assessment Tool may be improved. Users are also encouraged to send comments or corrections for the improvement of the tool.

Please send comments or other input using the **Contact Us** (<https://www.hudexchange.info/contact-us/>) form.

Soil Map—Wayne County, Michigan



Soil Map may not be valid at this scale.



MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)

Soils

 Soil Map Unit Polygons

 Soil Map Unit Lines

 Soil Map Unit Points

Special Point Features



Blowout



Borrow Pit



Clay Spot



Closed Depression



Gravel Pit



Gravelly Spot



Landfill



Lava Flow



Marsh or swamp



Mine or Quarry



Miscellaneous Water



Perennial Water



Rock Outcrop



Saline Spot



Sandy Spot



Severely Eroded Spot



Sinkhole



Slide or Slip



Sodic Spot



Spoil Area



Stony Spot



Very Stony Spot



Wet Spot



Other



Special Line Features

Water Features



Streams and Canals

Transportation



Rails



Interstate Highways



US Routes



Major Roads



Local Roads

Background



Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:12,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Wayne County, Michigan

Survey Area Data: Version 8, Aug 29, 2022

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Oct 9, 2022—Oct 21, 2022

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
MiduaB	Midtown-Urban land complex, 0 to 4 percent slopes	1.8	100.0%
Totals for Area of Interest		1.8	100.0%



Coleman A. Young Municipal Center
2 Woodward Avenue, Suite 908
Detroit, Michigan 48226

Phone: 313.224.6380
Fax: 313.224.1629
www.detroitmi.gov

August 24, 2023

Penny Dwoinen
City of Detroit Housing & Revitalization Department
Coleman A. Young Municipal Center
2 Woodward Avenue, Suite 908
Detroit, MI 48226

RE: Section 106 Review of a HOME Funded Project Located at 60 Harper in the City of Detroit, Wayne County, Michigan

Dear Mrs. Dwoinen,

In accordance with Section 106 of the National Historic Preservation Act and its implementing regulations, 36 CFR Part 800, I am providing a determination of historic eligibility regarding the above-referenced project under the authority of the "Programmatic Agreement between the Michigan State Historic Preservation Office and the City of Detroit, Michigan..." dated December 21, 2022.

The proposed project is to construct a three-story tall apartment building with a small retail space on the first floor on a large vacant site on the south side of Harper just east of Woodward in the Midtown area of Detroit, approximately three miles north of downtown Detroit. The proposed building has a U-shaped footprint with the building facing Harper and the courtyard facing south toward an alley. Three separate parking lots with lighting are proposed to the south of the building, north and south of the alley and will be accessed by the alley to the west and the I-94 Service Drive to the south. Two of the parking lots are on the property south of the alley, on both sides of an existing house. There is currently a parking lot on the west side of the house where the new lot is proposed. Open lawn areas are proposed on the west side of the proposed building and between the parking lots. The courtyard in the rear of the building is proposed to be landscaped and have a gazebo and artwork.

Based on research of the property the Area of Potential Effect (APE) has been defined as the two properties at 60 Harper and 71 E. Edsel Ford Service Drive and the properties adjacent to the two parcels.

Per Stipulation VI of Programmatic Agreement (PA), the proposed undertaking qualified for review by SHPO's archaeologist and consultation with Tribes. In a letter dated, 6/2/2023, SHPO's archaeologist concurred with the recommendation of "no historic properties affected within the area of potential effects of this undertaking."

On 4/25/2023, a request for Tribal Consultation was submitted to the following Tribes:

Bay Mills Indian Community
Forest County Potawatomi Community of Wisconsin
Grand Traverse Band of Ottawa & Chippewa Indians



Hannahville Indian Community
Ketegitigaaning Ojibwe Nation/Lac Vieux Desert Band of Lake Superior Chippewa
Indians
Keweenaw Bay Indian Community of the Lake Superior Band of Chippewa Indians
Lac du Flambeau Band of Lake Superior Chippewa Indians
Little River Band of Ottawa Indians
Little Traverse Bay Bands of Odawa Indians
Menominee Indian Tribe of Wisconsin
Match-E-Be-Nash-She-Wish (Gun Lake) Band of Pottawatomis Indians
Miami Tribe of Oklahoma
Michigan Anishinaabek Cultural Preservation and Repatriation Alliance
Nottawaseppi Huron Band of the Potawatomi
Pokagon Band of Potawatomi Indians, Michigan and Indiana
Saginaw Chippewa Indian Tribe of Michigan
Sault Ste. Marie Tribe of Chippewa Indians
Seneca Cayuga Nation

The Nottawaseppi Huron Band of the Potawatomi and Miami Tribe of Oklahoma provided responses. This consultation concluded with no objections to the proposed activities related to this undertaking. In the event of an unanticipated discovery, Tribal Consultation will be reinitiated under the direction of the unanticipated discoveries plan for this project.

Based on historic research, The project is directly east of St. Joseph's Episcopal Church (now Holy Rosary Roman Catholic Church) and directly across Harper from a small portion of the Piquette Avenue Industrial Historic District, both of which are listed in the National Register of Historic Places. Therefore, per Stipulation V.B of the PA, the project shall be carried out in accordance with the *Secretary of the Interior's Standards for the Treatment of Historic Properties*.

This project has been given a **Conditional No Adverse Effect** determination (Federal Regulations 36 CFR Part 800.5(b)) on properties that are listed or eligible for listing in the National Register of Historic Places, as long as the following conditions are met:

- The scope of work provided on 8/23/22 is followed, and any changes to the scope of work are submitted to the PS for additional review and approval.
- In the event of an unanticipated discovery during construction, the unanticipated discoveries plan is followed.
- Photos of the completed work are submitted to the Preservation Specialist.

Please note that the Section 106 Review process will not be complete until the above-mentioned conditions are met. If you have any questions, you may direct them to the Preservation Specialist at Ciavatonet@detroitmi.gov.

Sincerely,



**Housing and Revitalization
Department**

Coleman A. Young Municipal Center
2 Woodward Avenue, Suite 908
Detroit, Michigan 48226

Phone: 313.224.6380
Fax: 313.224.1629
www.detroitmi.gov

Tiffany Ciavattone
Preservation Specialist
City of Detroit
Housing & Revitalization Department

[Home \(/\)](#) > [Programs \(/programs/\)](#) > [Environmental Review \(/programs/environmental-review/\)](#) > [DNL Calculator](#)

DNL Calculator

The Day/Night Noise Level Calculator is an electronic assessment tool that calculates the Day/Night Noise Level (DNL) from roadway and railway traffic. For more information on using the DNL calculator, view the [Day/Night Noise Level Calculator Electronic Assessment Tool Overview \(/programs/environmental-review/daynight-noise-level-electronic-assessment-tool/\)](#).

Guidelines

- To display the Road and/or Rail DNL calculator(s), click on the "Add Road Source" and/or "Add Rail Source" button(s) below.
- All Road and Rail input values must be positive non-decimal numbers.
- All Road and/or Rail DNL value(s) must be calculated separately before calculating the Site DNL.
- All checkboxes that apply must be checked for vehicles and trains in the tables' headers.
- **Note #1:** Tooltips, containing field specific information, have been added in this tool and may be accessed by hovering over all the respective data fields (site identification, roadway and railway assessment, DNL calculation results, roadway and railway input variables) with the mouse.
- **Note #2:** DNL Calculator assumes roadway data is always entered.

DNL Calculator

Site ID	Harper Edsel
Record Date	03/16/2023
User's Name	Maura Gibbons - HMA

Road # 1 Name:	Woodward
-----------------------	-----------------

Road #1	Cars <input checked="" type="checkbox"/>	Medium Trucks <input checked="" type="checkbox"/>	Heavy Trucks <input checked="" type="checkbox"/>
Effective Distance	214	214	214
Distance to Stop Sign			
Average Speed	30	30	30
Average Daily Trips (ADT)	19004	826	826
Night Fraction of ADT	15	15	15
Road Gradient (%)			2
Vehicle DNL	56	52	64
Calculate Road #1 DNL	65	Reset	

Road # 2 Name:	Edsel Ford
-----------------------	-------------------

Road #2	Cars <input checked="" type="checkbox"/>	Medium Trucks <input checked="" type="checkbox"/>	Heavy Trucks <input checked="" type="checkbox"/>
Effective Distance	289	289	289
Distance to Stop Sign			
Average Speed	25	25	25
Average Daily Trips (ADT)	4268	186	186
Night Fraction of ADT	15	15	15
Road Gradient (%)			2
Vehicle DNI	46	47	56

Vehicle DNL	10	15	30
-------------	----	----	----

Calculate Road #2 DNL	56	Reset
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Road # 3 Name:	I-94 W Ramp
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Road #3

Vehicle Type	Cars <input checked="" type="checkbox"/>	Medium Trucks <input checked="" type="checkbox"/>	Heavy Trucks <input checked="" type="checkbox"/>
Effective Distance	365	365	365
Distance to Stop Sign			
Average Speed	40	40	40
Average Daily Trips (ADT)	4980	217	217
Night Fraction of ADT	15	15	15
Road Gradient (%)			2
Vehicle DNL	49	45	55

Calculate Road #3 DNL	56	Reset
-----------------------	----	-------

Road # 4 Name:	I-94
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Road #4

Vehicle Type	Cars <input checked="" type="checkbox"/>	Medium Trucks <input checked="" type="checkbox"/>	Heavy Trucks <input checked="" type="checkbox"/>
Effective Distance	439	439	439
Distance to Stop Sign			
Average Speed	55	55	55
Average Daily Trips (ADT)	31402	1365	1365
Night Fraction of ADT	15	15	15
Road Gradient (%)			2
Vehicle DNL	59	55	62

Calculate Road #4 DNL	64	Reset
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Railroad #1 Track Identifier:	Grand Trunk
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Rail # 1

Train Type	Electric <input type="checkbox"/>	Diesel <input checked="" type="checkbox"/>
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Effective Distance	<input type="text" value="1102"/>
Average Train Speed	<input type="text" value="35"/>
Engines per Train	<input type="text" value="2"/>
Railway cars per Train	<input type="text" value="8"/>
Average Train Operations (ATO)	<input type="text" value="11"/>
Night Fraction of ATO	<input type="text" value="15"/>
Railway whistles or horns?	Yes: <input type="checkbox"/> No: <input type="checkbox"/> Yes: <input checked="" type="checkbox"/> No: <input type="checkbox"/>
Bolted Tracks?	Yes: <input type="checkbox"/> No: <input type="checkbox"/> Yes: <input checked="" type="checkbox"/> No: <input type="checkbox"/>
Train DNL	<input type="text" value="0"/> <input type="text" value="58"/>

Calculate Rail #1 DNL	<input type="text" value="58"/>	<input type="button" value="Reset"/>
-----------------------	---------------------------------	--------------------------------------

Railroad #2 Track Identifier:

Rail # 2

Train Type	Electric <input checked="" type="checkbox"/>	Diesel <input type="checkbox"/>
Effective Distance	<input type="text" value="214"/>	<input type="text"/>
Average Train Speed	<input type="text" value="30"/>	<input type="text"/>
Engines per Train	<input type="text" value="1"/>	<input type="text"/>
Railway cars per Train	<input type="text" value="2"/>	<input type="text"/>
Average Train Operations (ATO)	<input type="text" value="6"/>	<input type="text"/>
Night Fraction of ATO	<input type="text" value="15"/>	<input type="text"/>
Railway whistles or horns?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/> No: <input type="checkbox"/>
Bolted Tracks?	Yes: <input type="checkbox"/> No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/> No: <input type="checkbox"/>

Train DNL	<input type="text" value="37"/>	<input type="text" value="0"/>
Calculate Rail #2 DNL	<input type="text" value="37"/>	<input type="button" value="Reset"/>

Airport Noise Level

Loud Impulse Sounds? Yes No

Combined DNL for all Road and Rail sources	69
Combined DNL including Airport	N/A
Site DNL with Loud Impulse Sound	

Mitigation Options

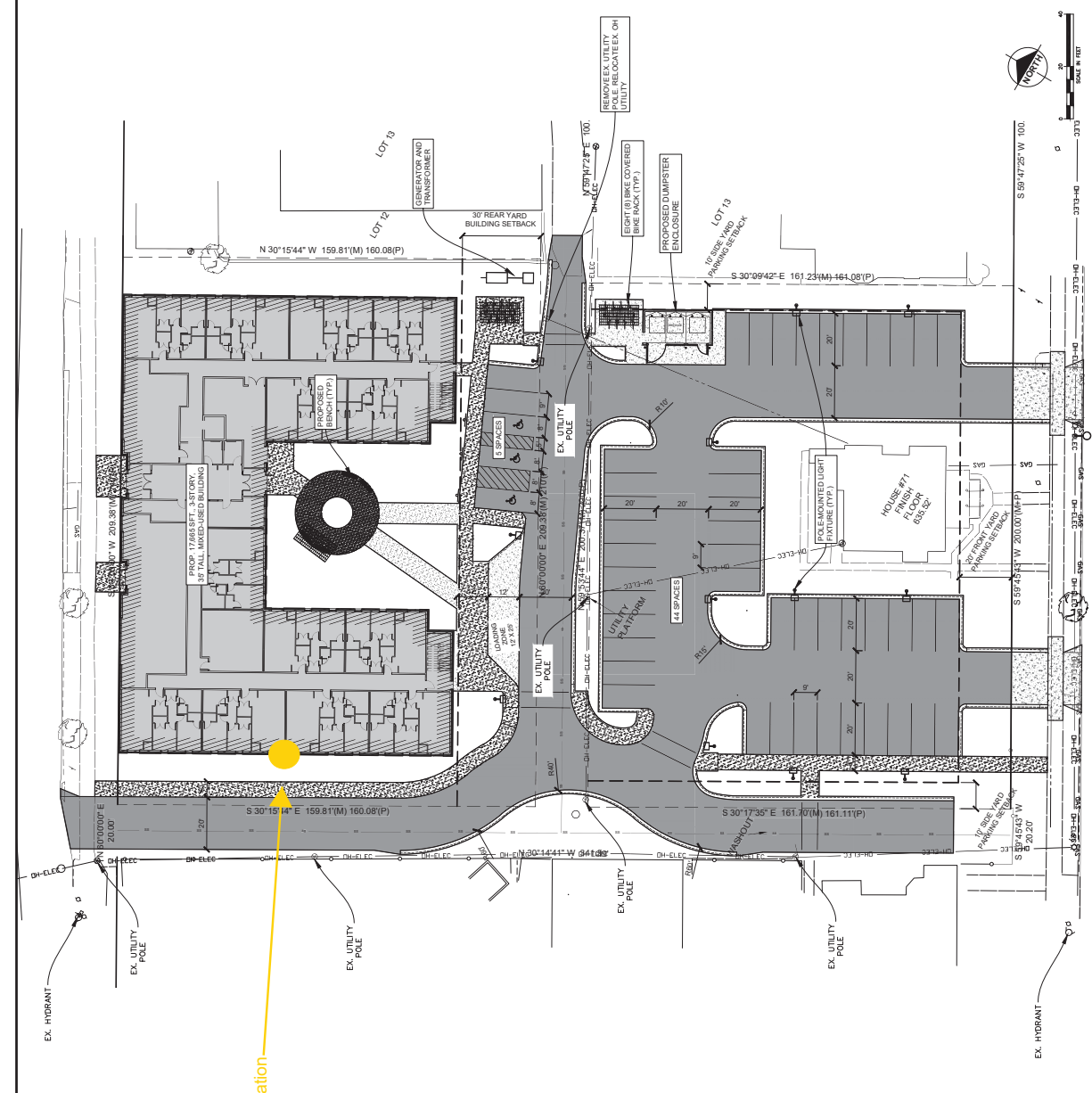
If your site DNL is in Excess of 65 decibels, your options are:

- **No Action Alternative:** Cancel the project at this location
- **Other Reasonable Alternatives:** Choose an alternate site
- **Mitigation**
 - Contact your Field or Regional Environmental Officer (</programs/environmental-review/hud-environmental-staff-contacts/>)
 - Increase mitigation in the building walls (only effective if no outdoor, noise sensitive areas)
 - Reconfigure the site plan to increase the distance between the noise source and noise-sensitive uses
 - Incorporate natural or man-made barriers. See *The Noise Guidebook* (</resource/313/hud-noise-guidebook/>)
 - Construct noise barrier. See the **Barrier Performance Module** (</programs/environmental-review/bpm-calculator/>)

Tools and Guidance




[Day/Night Noise Level Assessment Tool User Guide \(/resource/3822/day-night-noise-level-assessment-tool-user-guide/\)](/resource/3822/day-night-noise-level-assessment-tool-user-guide/)

[Day/Night Noise Level Assessment Tool Flowcharts \(/resource/3823/day-night-noise-level-assessment-tool-flowcharts/\)](/resource/3823/day-night-noise-level-assessment-tool-flowcharts/)



NAL Location

PROPOSED FEATURES LEGEND

-  NEW HMA PAVEMENT AREA
-  NEW LIGHT DUTY CONCRETE SIDEWALK AREA
-  NEW HEAVY DUTY CONCRETE SIDEWALK AREA



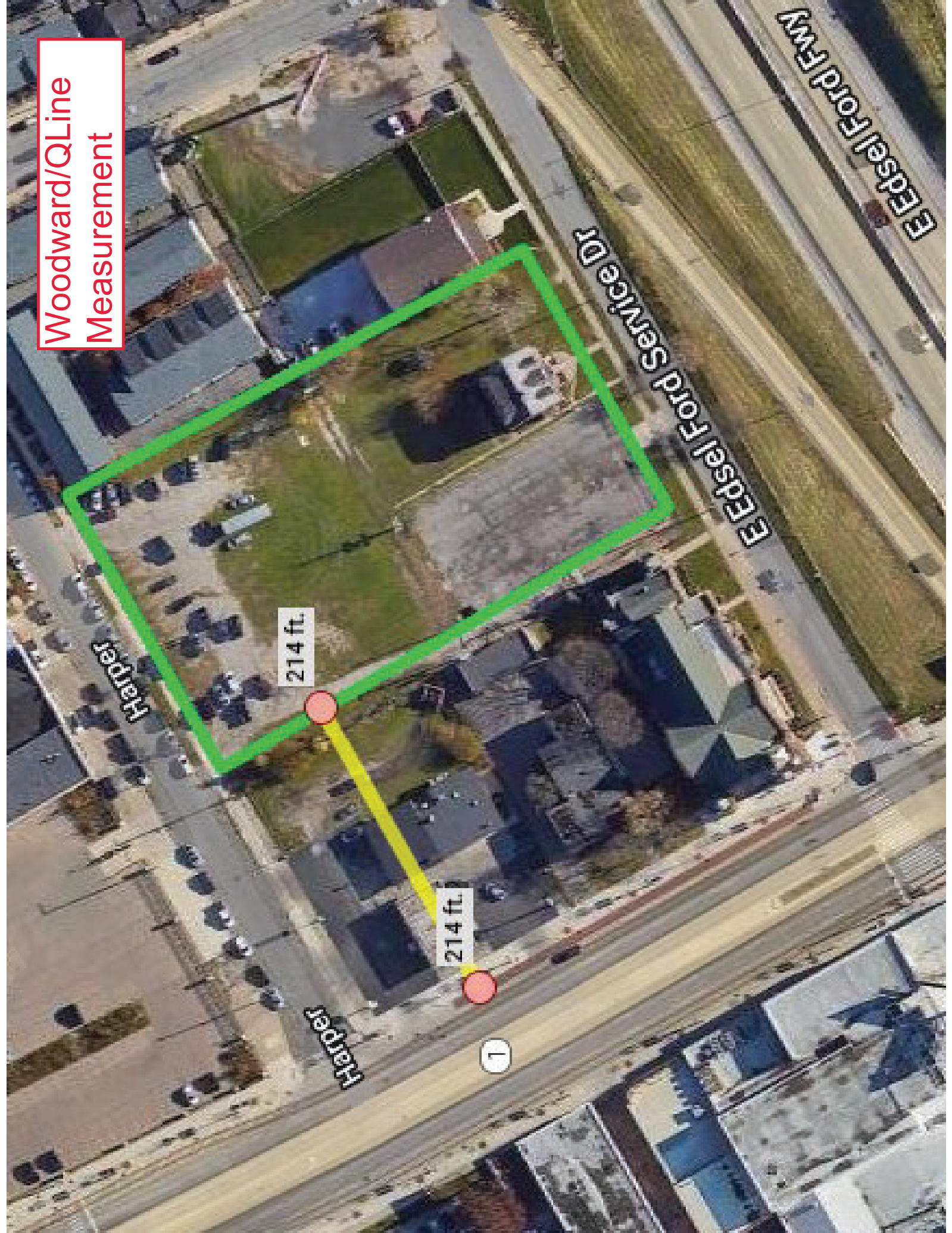
Callen Engineering, Inc.
 108 East Sawdige Street
 Spring Lake, MI 49456
 T.616.414.5260
 www.callenengineering.com

**WOODWARD / HARPER
 APARTMENTS
 SCHEMATIC PLAN**

60 Harper Avenue & 71 E. Edsel Ford Service Drive
 City of Detroit
 Wayne County, Michigan

February 11, 2023

Woodward/QLine
Measurement

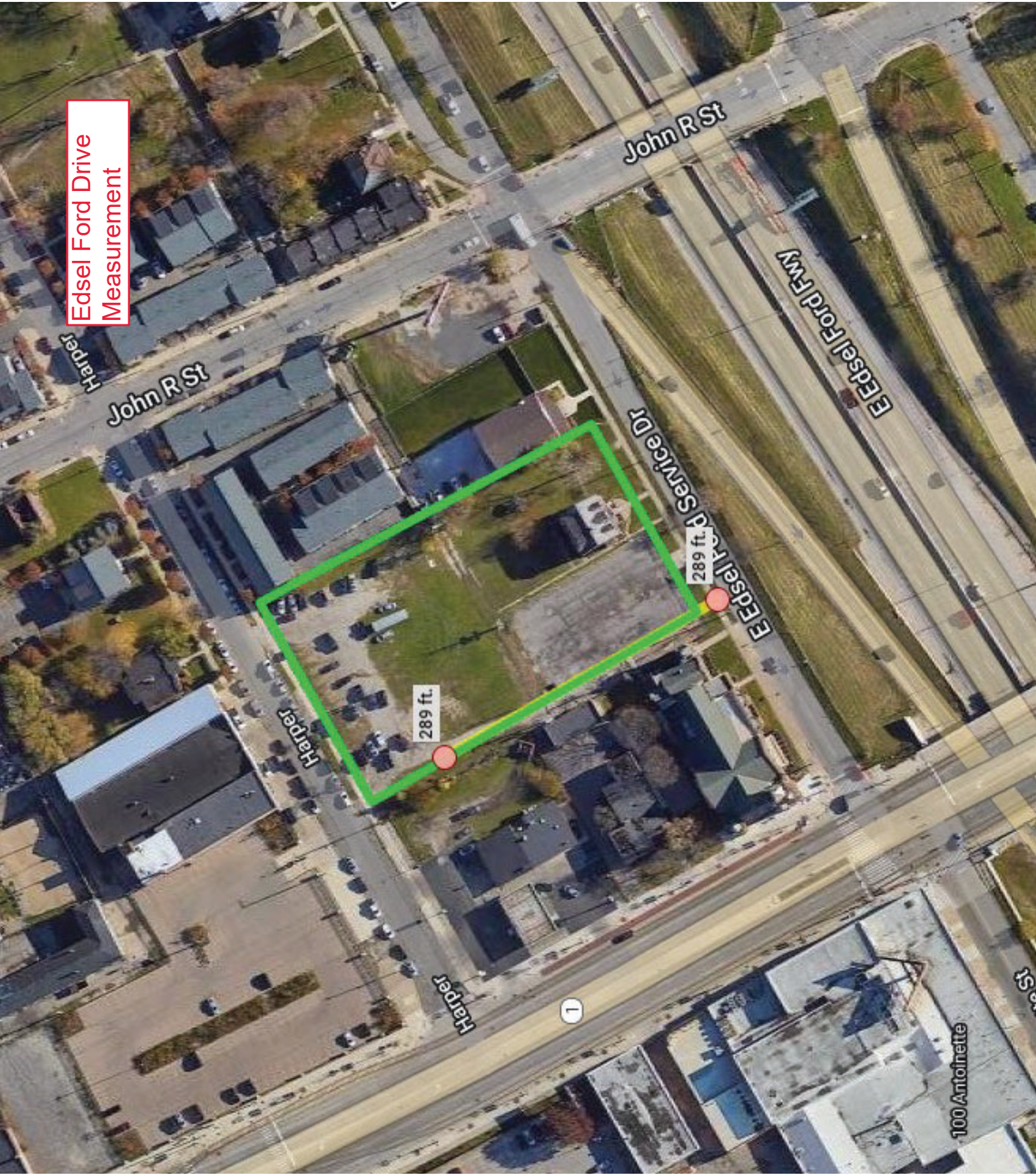


214 ft

214 ft

1

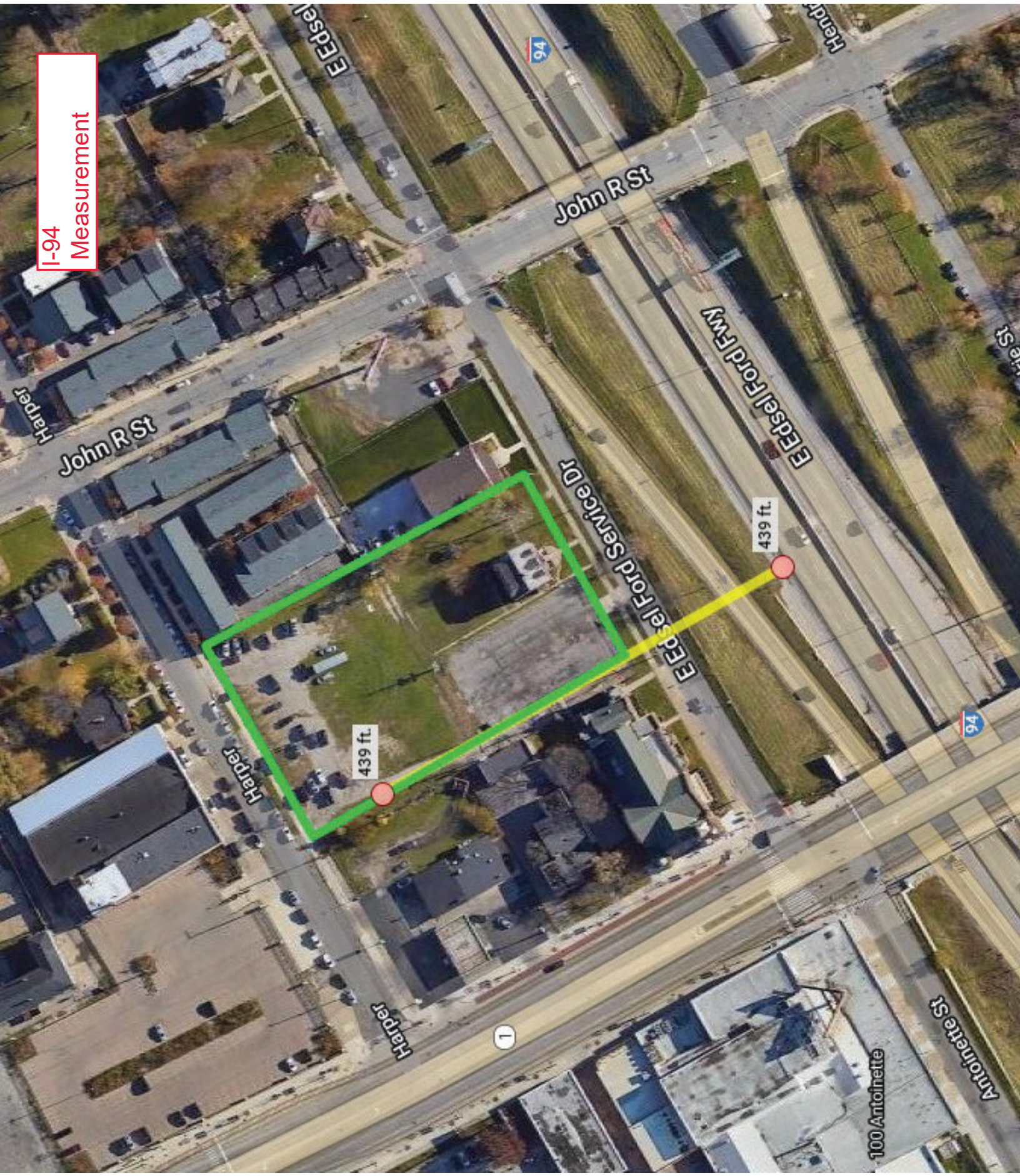
Edsel Ford Drive
Measurement



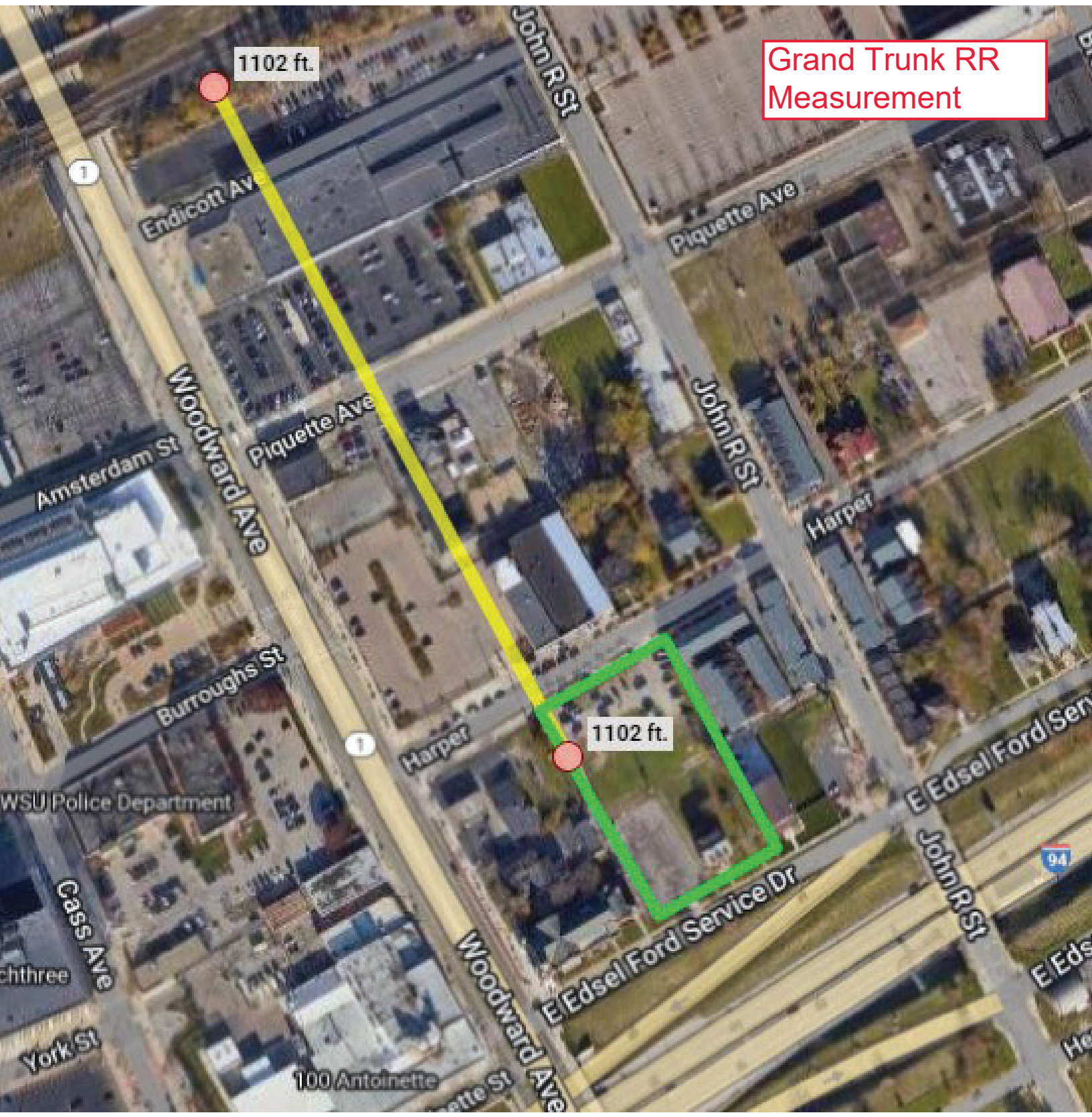
I-94 Ramp
Measurement

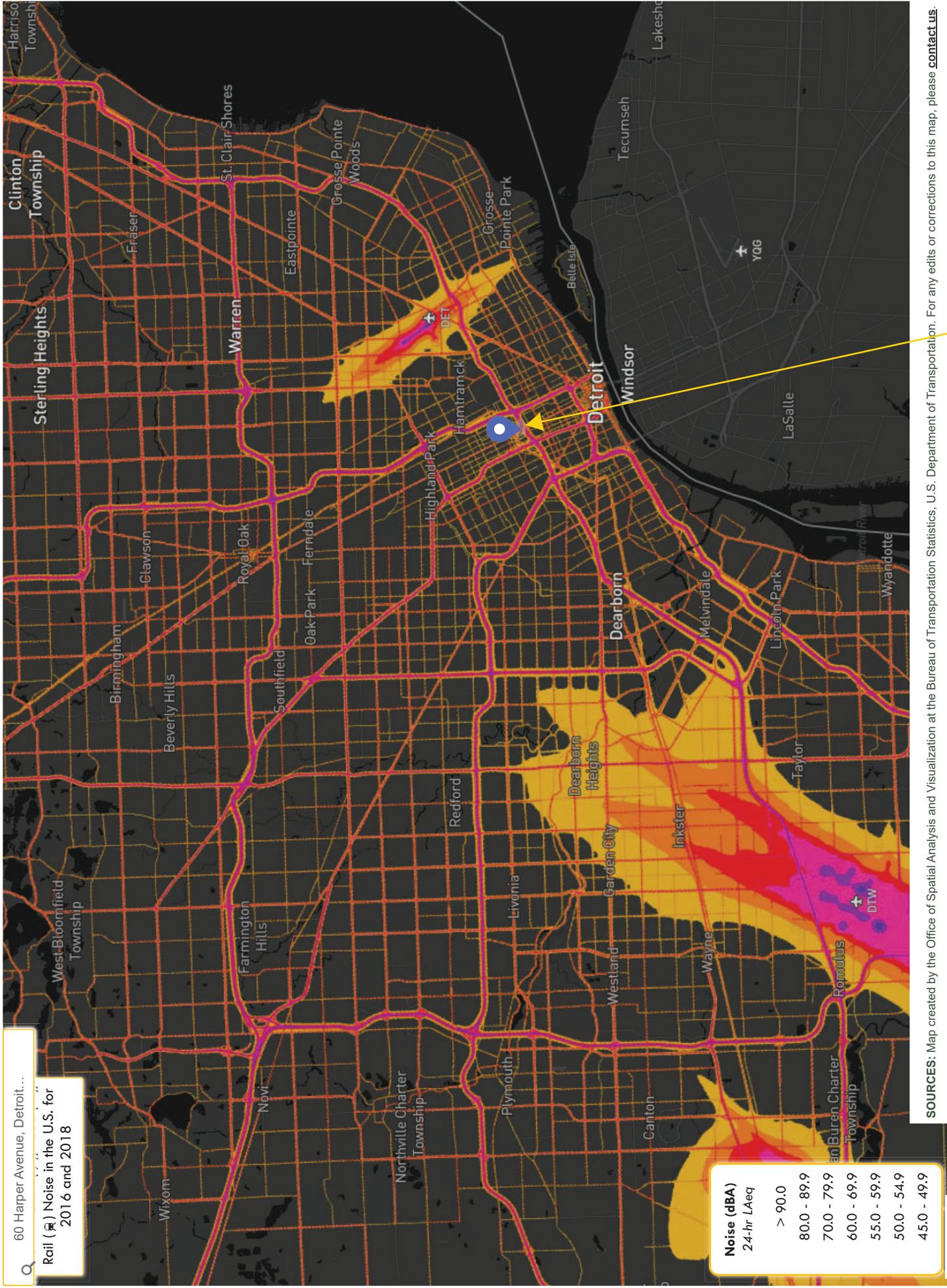


I-94
Measurement



Grand Trunk RR
Measurement





SOURCES: Map created by the Office of Spatial Analysis and Visualization at the Bureau of Transportation Statistics, U.S. Department of Transportation. For any edits or corrections to this map, please [contact us](#).

© Mapbox © OpenStreetMap

Subject Property

Detroit Q-Line Information

Source of Information: <https://www.qlinedetroit.com/faq/>

Date Accessed: March 15, 2023

12 stops with estimated 15-minute arrival between stops = 3 hours for one trip.

Total of 16 hours of operation per day = Assumes about 6 trips are made per day.

▼ **Where does QLINE go?**

The QLINE has 12 stops from Downtown to Grand Blvd with access to museums, sports and performance venues, and many of Detroit's favorite restaurants, retail and bars.

▼ **What are QLINE's Hours?**

Our current hours of operation are as follows:

Monday - Saturday: 8AM - 12AM

Sunday: 8AM - 9PM

▼ **What is the streetcar schedule?**

The QLINE arrives every 15 minutes or less.

U. S. DOT CROSSING INVENTORY FORM

DEPARTMENT OF TRANSPORTATION
FEDERAL RAILROAD ADMINISTRATION

OMB No. 2130-0017

Instructions for the initial reporting of the following types of new or previously unreported crossings: For public highway-rail grade crossings, complete the entire inventory Form. For private highway-rail grade crossings, complete the Header, Parts I and II, and the Submission Information section. For public pathway grade crossings (including pedestrian station grade crossings), complete the Header, Parts I and II, and the Submission Information section. For Private pathway grade crossings, complete the Header, Parts I and II, and the Submission Information section. For grade-separated highway-rail or pathway crossings (including pedestrian station crossings), complete the Header, Part I, and the Submission Information section. For changes to existing data, complete the Header, Part I Items 1-3, and the Submission Information section, in addition to the updated data fields. Note: For private crossings only, Part I Item 20 and Part III Item 2.K. are required unless otherwise noted. An asterisk * denotes an optional field.

A. Revision Date (MM/DD/YYYY) 09 / 04 / 2020	B. Reporting Agency <input checked="" type="checkbox"/> Railroad <input type="checkbox"/> Transit <input type="checkbox"/> State <input type="checkbox"/> Other	C. Reason for Update (Select only one) <input checked="" type="checkbox"/> Change in Data <input type="checkbox"/> Re-Open <input type="checkbox"/> New Crossing <input type="checkbox"/> Date Change Only <input type="checkbox"/> Closed <input type="checkbox"/> Change in Primary Operating RR <input type="checkbox"/> No Train Traffic <input type="checkbox"/> Quiet Zone Update <input type="checkbox"/> Admin. Correction	D. DOT Crossing Inventory Number 284036S
---	--	--	--

Part I: Location and Classification Information

1. Primary Operating Railroad GRAND TRUNK WESTERN RAILROAD INC. [GTW]		2. State MICHIGAN		3. County WAYNE	
4. City / Municipality <input checked="" type="checkbox"/> In <input type="checkbox"/> Near DETROIT		5. Street/Road Name & Block Number JOHN R SEVEN MI (Street/Road Name) * (Block Number)		6. Highway Type & No. CITY	
7. Do Other Railroads Operate a Separate Track at Crossing? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, Specify RR			8. Do Other Railroads Operate Over Your Track at Crossing? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Specify RR ATK		
9. Railroad Division or Region <input type="checkbox"/> None MICHIGAN		10. Railroad Subdivision or District <input type="checkbox"/> None SHORE LINE		11. Branch or Line Name <input type="checkbox"/> None MAIN	
12. RR Milepost 0053.830 (prefix) (nnnn.nnn) (suffix)		13. Line Segment * SC00022948		14. Nearest RR Timetable Station * WOODWARD	
15. Parent RR (if applicable) <input type="checkbox"/> N/A CN		16. Crossing Owner (if applicable) <input type="checkbox"/> N/A GTW		17. Crossing Type <input checked="" type="checkbox"/> Public <input type="checkbox"/> Private	
18. Crossing Purpose <input checked="" type="checkbox"/> Highway <input type="checkbox"/> Pathway, Ped. <input type="checkbox"/> Station, Ped.		19. Crossing Position <input type="checkbox"/> At Grade <input type="checkbox"/> RR Under <input checked="" type="checkbox"/> RR Over		20. Public Access (if Private Crossing) <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
21. Type of Train <input checked="" type="checkbox"/> Freight <input checked="" type="checkbox"/> Intercity Passenger <input type="checkbox"/> Commuter		<input type="checkbox"/> Transit <input type="checkbox"/> Shared Use Transit <input type="checkbox"/> Tourist/Other		22. Average Passenger Train Count Per Day <input type="checkbox"/> Less Than One Per Day <input checked="" type="checkbox"/> Number Per Day 8	
23. Type of Land Use <input type="checkbox"/> Open Space <input type="checkbox"/> Farm <input type="checkbox"/> Residential <input type="checkbox"/> Commercial <input checked="" type="checkbox"/> Industrial <input type="checkbox"/> Institutional <input type="checkbox"/> Recreational <input type="checkbox"/> RR Yard					
24. Is there an Adjacent Crossing with a Separate Number? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, Provide Crossing Number			25. Quiet Zone (FRA provided) <input checked="" type="checkbox"/> No <input type="checkbox"/> 24 Hr <input type="checkbox"/> Partial <input type="checkbox"/> Chicago Excused Date Established		
26. HSR Corridor ID <input checked="" type="checkbox"/> N/A		27. Latitude in decimal degrees (WGS84 std: nn.nnnnnnn) 42.368301		28. Longitude in decimal degrees (WGS84 std: -nnn.nnnnnnn) -83.069397	
29. Lat/Long Source <input checked="" type="checkbox"/> Actual <input type="checkbox"/> Estimated		30.A. Railroad Use *			
30.B. Railroad Use *		31.A. State Use *			
30.C. Railroad Use *		31.B. State Use *			
30.D. Railroad Use *		31.C. State Use *			
30.E. Railroad Use *		31.D. State Use *			
32.A. Narrative (Railroad Use) *			32.B. Narrative (State Use) *		
33. Emergency Notification Telephone No. (posted) 800-465-9239		34. Railroad Contact (Telephone No.) 888-888-5909		35. State Contact (Telephone No.) 517-335-2592	

Part II: Railroad Information

1. Estimated Number of Daily Train Movements				
1.A. Total Day Thru Trains (6 AM to 6 PM) 6	1.B. Total Night Thru Trains (6 PM to 6 AM) 5	1.C. Total Switching Trains 0	1.D. Total Transit Trains 0	1.E. Check if Less Than One Movement Per Day <input type="checkbox"/> How many trains per week? _____
2. Year of Train Count Data (YYYY) 2015		3. Speed of Train at Crossing 3.A. Maximum Timetable Speed (mph) 35 3.B. Typical Speed Range Over Crossing (mph) From 1 to 35		
4. Type and Count of Tracks Main 2 Siding 0 Yard 0 Transit 0 Industry 0				
5. Train Detection (Main Track only) <input type="checkbox"/> Constant Warning Time <input type="checkbox"/> Motion Detection <input type="checkbox"/> AFO <input type="checkbox"/> PTC <input type="checkbox"/> DC <input type="checkbox"/> Other <input checked="" type="checkbox"/> None				
6. Is Track Signaled? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No		7.A. Event Recorder <input type="checkbox"/> Yes <input type="checkbox"/> No		7.B. Remote Health Monitoring <input type="checkbox"/> Yes <input type="checkbox"/> No

U. S. DOT CROSSING INVENTORY FORM

A. Revision Date (MM/DD/YYYY) 09/04/2020		PAGE 2		D. Crossing Inventory Number (7 char.) 284036S	
Part III: Highway or Pathway Traffic Control Device Information					
1. Are there Signs or Signals? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		2. Types of Passive Traffic Control Devices associated with the Crossing			
2.A. Crossbuck Assemblies (count) 0		2.B. STOP Signs (R1-1) (count) 0	2.C. YIELD Signs (R1-2) (count) 0	2.D. Advance Warning Signs (Check all that apply; include count) <input checked="" type="checkbox"/> None <input type="checkbox"/> W10-1 0 <input type="checkbox"/> W10-3 0 <input type="checkbox"/> W10-11 0 <input type="checkbox"/> W10-2 0 <input type="checkbox"/> W10-4 0 <input type="checkbox"/> W10-12 0	
2.E. Low Ground Clearance Sign (W10-5) <input type="checkbox"/> Yes (count 0) <input checked="" type="checkbox"/> No		2.F. Pavement Markings <input type="checkbox"/> Stop Lines <input type="checkbox"/> Dynamic Envelope <input type="checkbox"/> RR Xing Symbols <input type="checkbox"/> None		2.G. Channelization Devices/Medians <input type="checkbox"/> All Approaches <input type="checkbox"/> Median <input type="checkbox"/> One Approach <input type="checkbox"/> None	
2.H. EXEMPT Sign (R15-3) <input type="checkbox"/> Yes <input type="checkbox"/> No		2.I. ENS Sign (I-13) Displayed <input type="checkbox"/> Yes <input type="checkbox"/> No			
2.J. Other MUTCD Signs <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Specify Type _____ Count 0 Specify Type _____ Count 0 Specify Type _____ Count 0		2.K. Private Crossing Signs (if private) <input type="checkbox"/> Yes <input type="checkbox"/> No		2.L. LED Enhanced Signs (List types)	
3. Types of Train Activated Warning Devices at the Grade Crossing (specify count of each device for all that apply)					
3.A. Gate Arms (count) Roadway 0 Pedestrian 0		3.B. Gate Configuration <input type="checkbox"/> 2 Quad <input type="checkbox"/> Full (Barrier) Resistance <input type="checkbox"/> 3 Quad <input type="checkbox"/> Median Gates		3.C. Cantilevered (or Bridged) Flashing Light Structures (count) Over Traffic Lane 0 <input type="checkbox"/> Incandescent Not Over Traffic Lane 0 <input type="checkbox"/> LED	
3.F. Installation Date of Current Active Warning Devices: (MM/YYYY) ____/____/____ <input type="checkbox"/> Not Required		3.G. Wayside Horn <input type="checkbox"/> Yes Installed on (MM/YYYY) ____/____/____ <input type="checkbox"/> No		3.H. Highway Traffic Signals Controlling Crossing <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
3.I. Bells (count) 0		3.J. Non-Train Active Warning <input type="checkbox"/> Flagging/Flagman <input type="checkbox"/> Manually Operated Signals <input type="checkbox"/> Watchman <input type="checkbox"/> Floodlighting <input type="checkbox"/> None			
3.K. Other Flashing Lights or Warning Devices Count 0 Specify type _____		4.A. Does nearby Hwy Intersection have Traffic Signals? <input type="checkbox"/> Yes <input type="checkbox"/> No			
4.B. Hwy Traffic Signal Interconnection <input type="checkbox"/> Not Interconnected <input type="checkbox"/> For Traffic Signals <input type="checkbox"/> For Warning Signs		4.C. Hwy Traffic Signal Preemption <input type="checkbox"/> Simultaneous <input type="checkbox"/> Advance		5. Highway Traffic Pre-Signals <input type="checkbox"/> Yes <input type="checkbox"/> No Storage Distance * 0 Stop Line Distance * 0	
6. Highway Monitoring Devices (Check all that apply) <input type="checkbox"/> Yes - Photo/Video Recording <input type="checkbox"/> Yes - Vehicle Presence Detection <input type="checkbox"/> None					
Part IV: Physical Characteristics					
1. Traffic Lanes Crossing Railroad Number of Lanes 0		<input type="checkbox"/> One-way Traffic <input type="checkbox"/> Two-way Traffic <input type="checkbox"/> Divided Traffic		2. Is Roadway/Pathway Paved? <input type="checkbox"/> Yes <input type="checkbox"/> No	
3. Does Track Run Down a Street? <input type="checkbox"/> Yes <input type="checkbox"/> No		4. Is Crossing Illuminated? (Street lights within approx. 50 feet from nearest rail) <input type="checkbox"/> Yes <input type="checkbox"/> No			
5. Crossing Surface (on Main Track, multiple types allowed) Installation Date * (MM/YYYY) ____/____/____ Width * _____ Length * _____ <input type="checkbox"/> 1 Timber <input type="checkbox"/> 2 Asphalt <input type="checkbox"/> 3 Asphalt and Timber <input type="checkbox"/> 4 Concrete <input type="checkbox"/> 5 Concrete and Rubber <input type="checkbox"/> 6 Rubber <input type="checkbox"/> 7 Metal <input type="checkbox"/> 8 Unconsolidated <input type="checkbox"/> 9 Composite <input type="checkbox"/> 10 Other (specify) _____					
6. Intersecting Roadway within 500 feet? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Approximate Distance (feet) _____		7. Smallest Crossing Angle <input type="checkbox"/> 0° - 29° <input type="checkbox"/> 30° - 59° <input type="checkbox"/> 60° - 90°		8. Is Commercial Power Available? * <input type="checkbox"/> Yes <input type="checkbox"/> No	
Part V: Public Highway Information					
1. Highway System <input type="checkbox"/> (01) Interstate Highway System <input type="checkbox"/> (02) Other Nat Hwy System (NHS) <input type="checkbox"/> (03) Federal AID, Not NHS <input type="checkbox"/> (08) Non-Federal Aid		2. Functional Classification of Road at Crossing <input type="checkbox"/> (0) Rural <input type="checkbox"/> (1) Urban <input type="checkbox"/> (1) Interstate <input type="checkbox"/> (5) Major Collector <input type="checkbox"/> (2) Other Freeways and Expressways <input type="checkbox"/> (3) Other Principal Arterial <input type="checkbox"/> (6) Minor Collector <input type="checkbox"/> (4) Minor Arterial <input type="checkbox"/> (7) Local		3. Is Crossing on State Highway System? <input type="checkbox"/> Yes <input type="checkbox"/> No	
7. Annual Average Daily Traffic (AADT) Year 1970 AADT 1		8. Estimated Percent Trucks 0 %		9. Regularly Used by School Buses? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Average Number per Day 0	
4. Highway Speed Limit 0 MPH <input type="checkbox"/> Posted <input type="checkbox"/> Statutory		5. Linear Referencing System (LRS Route ID) *			
10. Emergency Services Route <input type="checkbox"/> Yes <input type="checkbox"/> No		6. LRS Milepost *			
Submission Information - This information is used for administrative purposes and is not available on the public website.					
Submitted by _____ Organization _____ Phone _____ Date _____					
Public reporting burden for this information collection is estimated to average 30 minutes per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed and completing and reviewing the collection of information. According to the Paperwork Reduction Act of 1995, a federal agency may not conduct or sponsor, and a person is not required to, nor shall a person be subject to a penalty for failure to comply with, a collection of information unless it displays a currently valid OMB control number. The valid OMB control number for information collection is 2130-0017. Send comments regarding this burden estimate or any other aspect of this collection, including for reducing this burden to: Information Collection Officer, Federal Railroad Administration, 1200 New Jersey Ave. SE, MS-25 Washington, DC 20590.					

U. S. DOT CROSSING INVENTORY FORM

DEPARTMENT OF TRANSPORTATION
FEDERAL RAILROAD ADMINISTRATION

OMB No. 2130-0017

Instructions for the initial reporting of the following types of new or previously unreported crossings: For public highway-rail grade crossings, complete the entire inventory Form. For private highway-rail grade crossings, complete the Header, Parts I and II, and the Submission Information section. For public pathway grade crossings (including pedestrian station grade crossings), complete the Header, Parts I and II, and the Submission Information section. For Private pathway grade crossings, complete the Header, Parts I and II, and the Submission Information section. For grade-separated highway-rail or pathway crossings (including pedestrian station crossings), complete the Header, Part I, and the Submission Information section. For changes to existing data, complete the Header, Part I Items 1-3, and the Submission Information section, in addition to the updated data fields. Note: For private crossings only, Part I Item 20 and Part III Item 2.K. are required unless otherwise noted. An asterisk * denotes an optional field.

A. Revision Date (MM/DD/YYYY) 09 / 04 / 2020	B. Reporting Agency <input checked="" type="checkbox"/> Railroad <input type="checkbox"/> Transit <input type="checkbox"/> State <input type="checkbox"/> Other	C. Reason for Update (Select only one) <input checked="" type="checkbox"/> Change in Data <input type="checkbox"/> Re-Open <input type="checkbox"/> New Crossing <input type="checkbox"/> Date Change Only <input type="checkbox"/> Closed <input type="checkbox"/> Change in Primary Operating RR <input type="checkbox"/> No Train Traffic <input type="checkbox"/> Quiet Zone Update <input type="checkbox"/> Admin. Correction	D. DOT Crossing Inventory Number 284035K
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Part I: Location and Classification Information

1. Primary Operating Railroad GRAND TRUNK WESTERN RAILROAD INC. [GTW]		2. State MICHIGAN		3. County WAYNE	
4. City / Municipality <input checked="" type="checkbox"/> In <input type="checkbox"/> Near DETROIT		5. Street/Road Name & Block Number WOODWARD AVE (Street/Road Name) * (Block Number)		6. Highway Type & No. M1	
7. Do Other Railroads Operate a Separate Track at Crossing? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, Specify RR			8. Do Other Railroads Operate Over Your Track at Crossing? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Specify RR ATK		
9. Railroad Division or Region <input type="checkbox"/> None MICHIGAN		10. Railroad Subdivision or District <input type="checkbox"/> None SHORE LINE		11. Branch or Line Name <input type="checkbox"/> None MAIN	
12. RR Milepost 0053.710 (prefix) (nnnn.nnn) (suffix)		13. Line Segment * SC00022947		14. Nearest RR Timetable Station * AVERY	
15. Parent RR (if applicable) <input type="checkbox"/> N/A CN		16. Crossing Owner (if applicable) <input type="checkbox"/> N/A GTW		17. Crossing Type <input checked="" type="checkbox"/> Public <input type="checkbox"/> Private	
18. Crossing Purpose <input checked="" type="checkbox"/> Highway <input type="checkbox"/> Pathway, Ped. <input type="checkbox"/> Station, Ped.		19. Crossing Position <input type="checkbox"/> At Grade <input type="checkbox"/> RR Under <input checked="" type="checkbox"/> RR Over		20. Public Access (if Private Crossing) <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
21. Type of Train <input checked="" type="checkbox"/> Freight <input checked="" type="checkbox"/> Intercity Passenger <input type="checkbox"/> Commuter		<input type="checkbox"/> Transit <input type="checkbox"/> Shared Use Transit <input type="checkbox"/> Tourist/Other		22. Average Passenger Train Count Per Day <input type="checkbox"/> Less Than One Per Day <input checked="" type="checkbox"/> Number Per Day 8	
23. Type of Land Use <input type="checkbox"/> Open Space <input type="checkbox"/> Farm <input type="checkbox"/> Residential <input type="checkbox"/> Commercial <input checked="" type="checkbox"/> Industrial <input type="checkbox"/> Institutional <input type="checkbox"/> Recreational <input type="checkbox"/> RR Yard					
24. Is there an Adjacent Crossing with a Separate Number? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, Provide Crossing Number			25. Quiet Zone (FRA provided) <input checked="" type="checkbox"/> No <input type="checkbox"/> 24 Hr <input type="checkbox"/> Partial <input type="checkbox"/> Chicago Excused Date Established		
26. HSR Corridor ID <input checked="" type="checkbox"/> N/A		27. Latitude in decimal degrees (WGS84 std: nn.nnnnnnn) 42.367990		28. Longitude in decimal degrees (WGS84 std: -nnn.nnnnnnn) -83.071820	
29. Lat/Long Source <input checked="" type="checkbox"/> Actual <input type="checkbox"/> Estimated		30.A. Railroad Use *			
30.B. Railroad Use *		31.A. State Use *			
30.C. Railroad Use *		31.B. State Use *			
30.D. Railroad Use *		31.C. State Use *			
30.E. Railroad Use *		31.D. State Use *			
32.A. Narrative (Railroad Use) *			32.B. Narrative (State Use) *		
33. Emergency Notification Telephone No. (posted) 800-465-9239		34. Railroad Contact (Telephone No.) 888-888-5909		35. State Contact (Telephone No.) 517-335-2592	

Part II: Railroad Information

1. Estimated Number of Daily Train Movements				
1.A. Total Day Thru Trains (6 AM to 6 PM) 6	1.B. Total Night Thru Trains (6 PM to 6 AM) 5	1.C. Total Switching Trains 0	1.D. Total Transit Trains 0	1.E. Check if Less Than One Movement Per Day <input type="checkbox"/> How many trains per week? _____
2. Year of Train Count Data (YYYY) 2015		3. Speed of Train at Crossing 3.A. Maximum Timetable Speed (mph) 35 3.B. Typical Speed Range Over Crossing (mph) From 1 to 35		
4. Type and Count of Tracks Main 2 Siding 0 Yard 0 Transit 0 Industry 0				
5. Train Detection (Main Track only) <input type="checkbox"/> Constant Warning Time <input type="checkbox"/> Motion Detection <input type="checkbox"/> AFO <input type="checkbox"/> PTC <input type="checkbox"/> DC <input type="checkbox"/> Other <input checked="" type="checkbox"/> None				
6. Is Track Signaled? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No		7.A. Event Recorder <input type="checkbox"/> Yes <input type="checkbox"/> No		7.B. Remote Health Monitoring <input type="checkbox"/> Yes <input type="checkbox"/> No

U. S. DOT CROSSING INVENTORY FORM

A. Revision Date (MM/DD/YYYY) 09/04/2020		PAGE 2		D. Crossing Inventory Number (7 char.) 284035K		
Part III: Highway or Pathway Traffic Control Device Information						
1. Are there Signs or Signals? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		2. Types of Passive Traffic Control Devices associated with the Crossing				
2.A. Crossbuck Assemblies (count) 0		2.B. STOP Signs (R1-1) (count) 0	2.C. YIELD Signs (R1-2) (count) 0	2.D. Advance Warning Signs (Check all that apply; include count) <input checked="" type="checkbox"/> None <input type="checkbox"/> W10-1 0 <input type="checkbox"/> W10-3 0 <input type="checkbox"/> W10-11 0 <input type="checkbox"/> W10-2 0 <input type="checkbox"/> W10-4 0 <input type="checkbox"/> W10-12 0		
2.E. Low Ground Clearance Sign (W10-5) <input type="checkbox"/> Yes (count 0) <input checked="" type="checkbox"/> No		2.F. Pavement Markings <input type="checkbox"/> Stop Lines <input type="checkbox"/> Dynamic Envelope <input type="checkbox"/> RR Xing Symbols <input type="checkbox"/> None		2.G. Channelization Devices/Medians <input type="checkbox"/> All Approaches <input type="checkbox"/> Median <input type="checkbox"/> One Approach <input type="checkbox"/> None	2.H. EXEMPT Sign (R15-3) <input type="checkbox"/> Yes <input type="checkbox"/> No	2.I. ENS Sign (I-13) Displayed <input type="checkbox"/> Yes <input type="checkbox"/> No
2.J. Other MUTCD Signs Specify Type _____ Count 0 Specify Type _____ Count 0 Specify Type _____ Count 0		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	2.K. Private Crossing Signs (if private) <input type="checkbox"/> Yes <input type="checkbox"/> No	2.L. LED Enhanced Signs (List types)		
3. Types of Train Activated Warning Devices at the Grade Crossing (specify count of each device for all that apply)						
3.A. Gate Arms (count) Roadway 0 Pedestrian 0	3.B. Gate Configuration <input type="checkbox"/> 2 Quad <input type="checkbox"/> Full (Barrier) Resistance <input type="checkbox"/> 3 Quad <input type="checkbox"/> Median Gates	3.C. Cantilevered (or Bridged) Flashing Light Structures (count) Over Traffic Lane 0 <input type="checkbox"/> Incandescent Not Over Traffic Lane 0 <input type="checkbox"/> LED		3.D. Mast Mounted Flashing Lights (count of masts) 0 <input type="checkbox"/> Incandescent <input type="checkbox"/> LED <input type="checkbox"/> Back Lights Included <input type="checkbox"/> Side Lights Included	3.E. Total Count of Flashing Light Pairs 0	
3.F. Installation Date of Current Active Warning Devices: (MM/YYYY) ____/____/____ <input type="checkbox"/> Not Required		3.G. Wayside Horn <input type="checkbox"/> Yes Installed on (MM/YYYY) ____/____/____ <input type="checkbox"/> No		3.H. Highway Traffic Signals Controlling Crossing <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	3.I. Bells (count) 0	
3.J. Non-Train Active Warning <input type="checkbox"/> Flagging/Flagman <input type="checkbox"/> Manually Operated Signals <input type="checkbox"/> Watchman <input type="checkbox"/> Floodlighting <input type="checkbox"/> None				3.K. Other Flashing Lights or Warning Devices Count 0 Specify type _____		
4.A. Does nearby Hwy Intersection have Traffic Signals? <input type="checkbox"/> Yes <input type="checkbox"/> No	4.B. Hwy Traffic Signal Interconnection <input type="checkbox"/> Not Interconnected <input type="checkbox"/> For Traffic Signals <input type="checkbox"/> For Warning Signs	4.C. Hwy Traffic Signal Preemption <input type="checkbox"/> Simultaneous <input type="checkbox"/> Advance	5. Highway Traffic Pre-Signals <input type="checkbox"/> Yes <input type="checkbox"/> No Storage Distance * 0 Stop Line Distance * 0	6. Highway Monitoring Devices (Check all that apply) <input type="checkbox"/> Yes - Photo/Video Recording <input type="checkbox"/> Yes - Vehicle Presence Detection <input type="checkbox"/> None		
Part IV: Physical Characteristics						
1. Traffic Lanes Crossing Railroad Number of Lanes 0		<input type="checkbox"/> One-way Traffic <input type="checkbox"/> Two-way Traffic <input type="checkbox"/> Divided Traffic	2. Is Roadway/Pathway Paved? <input type="checkbox"/> Yes <input type="checkbox"/> No	3. Does Track Run Down a Street? <input type="checkbox"/> Yes <input type="checkbox"/> No	4. Is Crossing Illuminated? (Street lights within approx. 50 feet from nearest rail) <input type="checkbox"/> Yes <input type="checkbox"/> No	
5. Crossing Surface (on Main Track, multiple types allowed) Installation Date * (MM/YYYY) ____/____/____ Width * _____ Length * _____ <input type="checkbox"/> 1 Timber <input type="checkbox"/> 2 Asphalt <input type="checkbox"/> 3 Asphalt and Timber <input type="checkbox"/> 4 Concrete <input type="checkbox"/> 5 Concrete and Rubber <input type="checkbox"/> 6 Rubber <input type="checkbox"/> 7 Metal <input type="checkbox"/> 8 Unconsolidated <input type="checkbox"/> 9 Composite <input type="checkbox"/> 10 Other (specify) _____						
6. Intersecting Roadway within 500 feet? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Approximate Distance (feet) _____			7. Smallest Crossing Angle <input type="checkbox"/> 0° - 29° <input type="checkbox"/> 30° - 59° <input type="checkbox"/> 60° - 90°		8. Is Commercial Power Available? * <input type="checkbox"/> Yes <input type="checkbox"/> No	
Part V: Public Highway Information						
1. Highway System <input type="checkbox"/> (01) Interstate Highway System <input type="checkbox"/> (02) Other Nat Hwy System (NHS) <input type="checkbox"/> (03) Federal AID, Not NHS <input type="checkbox"/> (08) Non-Federal Aid		2. Functional Classification of Road at Crossing <input type="checkbox"/> (0) Rural <input type="checkbox"/> (1) Urban <input type="checkbox"/> (1) Interstate <input type="checkbox"/> (5) Major Collector <input type="checkbox"/> (2) Other Freeways and Expressways <input type="checkbox"/> (3) Other Principal Arterial <input type="checkbox"/> (6) Minor Collector <input type="checkbox"/> (4) Minor Arterial <input type="checkbox"/> (7) Local		3. Is Crossing on State Highway System? <input type="checkbox"/> Yes <input type="checkbox"/> No	4. Highway Speed Limit 0 MPH <input type="checkbox"/> Posted <input type="checkbox"/> Statutory	
5. Linear Referencing System (LRS Route ID) *						
6. LRS Milepost *						
7. Annual Average Daily Traffic (AADT) Year 1970 AADT 1		8. Estimated Percent Trucks 0 %	9. Regularly Used by School Buses? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Average Number per Day 0		10. Emergency Services Route <input type="checkbox"/> Yes <input type="checkbox"/> No	
Submission Information - This information is used for administrative purposes and is not available on the public website.						
Submitted by _____ Organization _____ Phone _____ Date _____						
Public reporting burden for this information collection is estimated to average 30 minutes per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed and completing and reviewing the collection of information. According to the Paperwork Reduction Act of 1995, a federal agency may not conduct or sponsor, and a person is not required to, nor shall a person be subject to a penalty for failure to comply with, a collection of information unless it displays a currently valid OMB control number. The valid OMB control number for information collection is 2130-0017. Send comments regarding this burden estimate or any other aspect of this collection, including for reducing this burden to: Information Collection Officer, Federal Railroad Administration, 1200 New Jersey Ave. SE, MS-25 Washington, DC 20590.						

Harper and Edsel Project, Detroit, Michigan

Street	Source	Traffic Count	Projected Increase	SUMMARY OF NOISE ASSESSMENT TRAFFIC ASSUMPTIONS									
				Year 1	Year 2	Year 3	Year 4	Year 5	Year 6	Year 7	Year 8	Year 9	Year 10
Woodward Avenue (30 mph)	SEMCOG AADT Map	Total	18,887	19,076	19,267	19,459	19,654	19,850	20,049	20,249	20,452	20,656	
		Cars	17,376	17,550	17,725	17,903	18,082	18,262	18,445	18,629	18,816	19,004	
		Medium Trucks	748	755	771	778	786	794	802	810	818	826	
		Heavy Trucks	748	755	771	778	786	794	802	810	818	826	
		Total	4,200	4,242	4,284	4,327	4,414	4,458	4,503	4,548	4,593	4,639	
Edsel Ford Drive/Medbury Avenue (25 mph)	SEMCOG AADT Map	Cars	3,864	3,903	3,942	4,021	4,061	4,102	4,143	4,184	4,226	4,268	
		Medium Trucks	168	170	173	175	177	178	180	182	184	186	
		Heavy Trucks	168	170	173	175	177	178	180	182	184	186	
		Total	4,900	4,949	4,998	5,048	5,150	5,201	5,253	5,306	5,359	5,413	
		Cars	4,508	4,553	4,599	4,645	4,738	4,785	4,833	4,882	4,930	4,980	
I-94 Ramp West Bound (40 mph)	SEMCOG AADT Map	Medium Trucks	196	198	200	202	204	206	208	210	212	214	
		Heavy Trucks	196	198	200	202	204	206	208	210	212	214	
		Total	30,900	31,209	31,521	31,836	32,155	32,476	32,801	33,129	33,460	33,795	
		Cars	28,428	28,712	28,999	29,289	29,582	29,878	30,177	30,479	30,783	31,091	
		Total	1,236	1,248	1,261	1,273	1,286	1,299	1,312	1,325	1,338	1,352	
I-94 West Bound (55 mph)	SEMCOG AADT Map	Medium Trucks	1,236	1,248	1,261	1,273	1,286	1,299	1,312	1,325	1,338	1,352	
		Heavy Trucks	1,236	1,248	1,261	1,273	1,286	1,299	1,312	1,325	1,338	1,352	

Notes:

Traffic count is based on two-way counts

Truck breakdown percentages in an urban environment assumes 92% auto, 4% medium trucks, and 4% heavy trucks

Project traffic growth is assumed as 1% increase per year

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Typ. INT. UNIT

Sound Transmission Classification Assessment Tool (STraCAT)

Overview

The Sound Transmission Classification Assessment Tool (STraCAT) is an electronic version of Figures 17 and 19 in The HUD Noise Guidebook. The purpose of this tool is to document sound attenuation performance of wall systems. Based on wall, window, and door Sound Transmission Classification (STC) values, the STraCAT generates a composite STC value for the wall assembly as a whole. Users can enter the calculated noise level related to a specific Noise Assessment Location in front of a building façade and STraCAT will generate a target required attenuation value for the wall assembly in STC. Based on wall materials, the tool will state whether the composite wall assembly STC meets the required attenuation value.

How to Use This Tool

Location, Noise Level and Wall Configuration to Be Analyzed

STraCAT is designed to calculate the attenuation provided by the wall assembly for one wall of one unit. If unit exterior square footage and window/door configuration is identical around the structure, a single STraCAT may be sufficient. If units vary, at least one STraCAT should be completed for each different exterior unit wall configuration to document that all will achieve the required attenuation. Additionally, if attenuation is not based on a single worst-case NAL, but there are multiple NALs which require different levels of attenuation around the structure, a STraCAT should be completed for each differing exterior wall configuration associated with each NAL.

Exterior wall configurations associated with an NAL include those with parallel (facing) or near-parallel exposure as well as those with perpendicular exposure. When a façade has parallel or perpendicular exposure to two or more NALs, you should base the required attenuation on the NAL with the highest calculated noise level. For corner units where the unit interior receives exterior noise through two facades, the STraCAT calculation should incorporate the area of wall, window and door materials pertaining to the corner unit's total exterior wall area (i.e., from both walls).

Information to Be Entered

Users first enter basic project information and the NAL noise level that will be used as the basis for required attenuation. This noise level must be entered in whole numbers. STraCAT users then enter information on wall, window and door component type and area. Again, as noted above, the wall, window and door entries are based on one unit, and one wall (except for corner units as discussed above). The tool sums total wall square footage based on the combined area of walls, doors and windows for the façade being evaluated.

Users may input STC values for materials in one of two ways. The tool includes a dropdown menu of common construction materials with STC values prefilled. If selected construction materials

manually. Verification of the component STC must be included in the ERR. Documentation includes the architect or construction manager's project plans showing wall material specifications. For new construction or for components that will be newly installed in an existing wall, documentation also includes the manufacturer's product specification sheet (cut sheet) documenting the STC rating of selected doors and windows.

Required STC Rating and Determination of Compliance

Finally, based on project information entered the tool will indicate the required STC rating for the wall assembly being evaluated and whether or not the materials specified will produce a combined rating that meets this requirement. Note that for noise levels above 75 dB DNL, either HUD (for 24 CFR Part 50 reviews) or the Responsible Entity (for 24 CFR Part 58 reviews) must approve the level and type of attenuation, among other processing requirements. Required attenuation values generated by STraCAT for NALs above 75 dB DNL should therefore be considered tentative pending approval by HUD or the RE.

Project Description

Project

60 Harper

Sponsor/Developer

MHT Housing

Location

60 Harper, Detroit, MI

Prepared by

Concept Design Studio

Noise Level

69

Date

6/9/2023



Primary Source(s)

Interstate

Part II - Wall Components

Wall Construction Detail		Area	STC	
2x4" wood studs 16"o.c.; 5/8" gypsum board		240	28	
<input type="button" value="Add new wall"/>				
		240 Sq. Feet	28	
Window				
Construction Detail	Quantity	Sq Ft/Unit	STC	
dual pane low e	1	25	30	
dual pane low e	2	15	30	
<input type="button" value="Add new window"/>				
Door Construction Detail		Quantity	Sq Ft/Unit	STC
<input type="button" value="Add new door"/>				

FORM RESULTS

Wall Statistics

Stat	Value
Area:	240 ft ²
Wall STC:	28

Aperture Statistics

Aperture	Count	Area	% of wall
Windows:	3	55 ft ²	22.92%
Doors:	0	0 ft ²	0%

Evaluation Criteria

Criteria	Value
Noise source sound level (dB):	69
Combined STC for wall assembly:	28.38
Required STC rating:	27
Does wall assembly meet requirements?	Yes

[Print](#)

Quick Tips

What do you do if the preferred wall design is not sufficient to achieve the required attenuation? Another wall design with more substantial materials will work, but may not be the most cost-effective solution. Try adding some other elements for just a little more attenuation.

For example:

- Staggering the studs in a wall offers approximately 4dB of additional protection.
- Increasing the stud spacing from 16" on center to 24" can increase the STC from 2-5dB.
- Adding a 2" air space can provide 3dB more attenuation.
- Increasing a wall's air space from 3" to 6" can reduce noise levels by an additional 5dB.
- Adding a layer of ½" gypsum board on "Z" furring channels adds 2dB of attenuation.
- Using resilient channels and clips between wall panels and studs can improve the STC from 2-5dB.
- Adding a layer of ½" gypsum board on resilient channels adds 5dB of attenuation.
- Adding acoustical or isolation blankets to a wall's airspace can add 4-10dB of attenuation.
- A 1" rockwool acoustical blanket adds 3dB to the wall's STC.
- Filling the cells of lightweight concrete masonry units with expanded mineral loose-fill insulation adds 2dB to the STC.

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Typ 2nd - 3rd CORNER UNIT

Sound Transmission Classification Assessment Tool (STraCAT)

Overview

The Sound Transmission Classification Assessment Tool (STraCAT) is an electronic version of Figures 17 and 19 in The HUD Noise Guidebook. The purpose of this tool is to document sound attenuation performance of wall systems. Based on wall, window, and door Sound Transmission Classification (STC) values, the STraCAT generates a composite STC value for the wall assembly as a whole. Users can enter the calculated noise level related to a specific Noise Assessment Location in front of a building façade and STraCAT will generate a target required attenuation value for the wall assembly in STC. Based on wall materials, the tool will state whether the composite wall assembly STC meets the required attenuation value.

How to Use This Tool

Location, Noise Level and Wall Configuration to Be Analyzed

STraCAT is designed to calculate the attenuation provided by the wall assembly for one wall of one unit. If unit exterior square footage and window/door configuration is identical around the structure, a single STraCAT may be sufficient. If units vary, at least one STraCAT should be completed for each different exterior unit wall configuration to document that all will achieve the required attenuation. Additionally, if attenuation is not based on a single worst-case NAL, but there are multiple NALs which require different levels of attenuation around the structure, a STraCAT should be completed for each differing exterior wall configuration associated with each NAL.

Exterior wall configurations associated with an NAL include those with parallel (facing) or near-parallel exposure as well as those with perpendicular exposure. When a façade has parallel or perpendicular exposure to two or more NALs, you should base the required attenuation on the NAL with the highest calculated noise level. For corner units where the unit interior receives exterior noise through two facades, the STraCAT calculation should incorporate the area of wall, window and door materials pertaining to the corner unit's total exterior wall area (i.e., from both walls).

Information to Be Entered

Users first enter basic project information and the NAL noise level that will be used as the basis for required attenuation. This noise level must be entered in whole numbers. STraCAT users then enter information on wall, window and door component type and area. Again, as noted above, the wall, window and door entries are based on one unit, and one wall (except for corner units as discussed above). The tool sums total wall square footage based on the combined area of walls, doors and windows for the façade being evaluated.

Users may input STC values for materials in one of two ways. The tool includes a dropdown menu of common construction materials with STC values prefilled. If selected construction materials

manually. Verification of the component STC must be included in the ERR. Documentation includes the architect or construction manager's project plans showing wall material specifications. For new construction or for components that will be newly installed in an existing wall, documentation also includes the manufacturer's product specification sheet (cut sheet) documenting the STC rating of selected doors and windows.

Required STC Rating and Determination of Compliance

Finally, based on project information entered the tool will indicate the required STC rating for the wall assembly being evaluated and whether or not the materials specified will produce a combined rating that meets this requirement. Note that for noise levels above 75 dB DNL, either HUD (for 24 CFR Part 50 reviews) or the Responsible Entity (for 24 CFR Part 58 reviews) must approve the level and type of attenuation, among other processing requirements. Required attenuation values generated by STraCAT for NALs above 75 dB DNL should therefore be considered tentative pending approval by HUD or the RE.

Project Description

Project

60 Harper

Sponsor/Developer

MHT Housing

Location

60 Harper, Detroit, MI

Prepared by

Concept Design Studio

Noise Level

69

Date

6/9/2023



Primary Source(s)

Interstate

Part II - Wall Components

Wall Construction Detail		Area	STC
4" face brick one course		<input type="text" value="240"/>	45
2x4" wood studs 16"o.c.; 5/8" gypsum board		<input type="text" value="200"/>	28
<input type="button" value="Add new wall"/>			
		440 Sq. Feet	31.32

Window			
Construction Detail	Quantity	Sq Ft/Unit	STC
dual pane low e	<input type="text" value="1"/>	<input type="text" value="25"/>	<input type="text" value="30"/>
dual pane low e	<input type="text" value="2"/>	<input type="text" value="15"/>	<input type="text" value="30"/>
<input type="button" value="Add new window"/>			

Door Construction Detail	Quantity	Sq Ft/Unit	STC
<input type="button" value="Add new door"/>			

PART III RESULTS

Wall Statistics

Stat	Value
Area:	440 ft ²
Wall STC:	31.32

Aperture Statistics

Aperture	Count	Area	% of wall
Windows:	3	55 ft ²	12.5%
Doors:	0	0 ft ²	0%

Evaluation Criteria

Criteria	Value
Noise source sound level (dB):	69
Combined STC for wall assembly:	31.13
Required STC rating:	27
Does wall assembly meet requirements?	Yes

[Print](#)

FACT TIPS

What do you do if the preferred wall design is not sufficient to achieve the required attenuation? Another wall design with more substantial materials will work, but may not be the most cost-effective solution. Try adding some other elements for just a little more attenuation.

For example:

- Staggering the studs in a wall offers approximately 4dB of additional protection.
- Increasing the stud spacing from 16" on center to 24" can increase the STC from 2-5dB.
- Adding a 2" air space can provide 3dB more attenuation.
- Increasing a wall's air space from 3" to 6" can reduce noise levels by an additional 5dB.
- Adding a layer of ½" gypsum board on "Z" furring channels adds 2dB of attenuation.
- Using resilient channels and clips between wall panels and studs can improve the STC from 2-5dB.
- Adding a layer of ½" gypsum board on resilient channels adds 5dB of attenuation.
- Adding acoustical or isolation blankets to a wall's airspace can add 4-10dB of attenuation.
- A 1" rockwool acoustical blanket adds 3dB to the wall's STC.
- Filling the cells of lightweight concrete masonry units with expanded mineral loose-fill insulation adds 2dB to the STC.

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Typ. 1st flr unit

Sound Transmission Classification Assessment Tool (STraCAT)

Overview

The Sound Transmission Classification Assessment Tool (STraCAT) is an electronic version of Figures 17 and 19 in The HUD Noise Guidebook. The purpose of this tool is to document sound attenuation performance of wall systems. Based on wall, window, and door Sound Transmission Classification (STC) values, the STraCAT generates a composite STC value for the wall assembly as a whole. Users can enter the calculated noise level related to a specific Noise Assessment Location in front of a building façade and STraCAT will generate a target required attenuation value for the wall assembly in STC. Based on wall materials, the tool will state whether the composite wall assembly STC meets the required attenuation value.

How to Use This Tool

Location, Noise Level and Wall Configuration to Be Analyzed

STraCAT is designed to calculate the attenuation provided by the wall assembly for one wall of one unit. If unit exterior square footage and window/door configuration is identical around the structure, a single STraCAT may be sufficient. If units vary, at least one STraCAT should be completed for each different exterior unit wall configuration to document that all will achieve the required attenuation. Additionally, if attenuation is not based on a single worst-case NAL, but there are multiple NALs which require different levels of attenuation around the structure, a STraCAT should be completed for each differing exterior wall configuration associated with each NAL.

Exterior wall configurations associated with an NAL include those with parallel (facing) or near-parallel exposure as well as those with perpendicular exposure. When a façade has parallel or perpendicular exposure to two or more NALs, you should base the required attenuation on the NAL with the highest calculated noise level. For corner units where the unit interior receives exterior noise through two facades, the STraCAT calculation should incorporate the area of wall, window and door materials pertaining to the corner unit's total exterior wall area (i.e., from both walls).

Information to Be Entered

Users first enter basic project information and the NAL noise level that will be used as the basis for required attenuation. This noise level must be entered in whole numbers. STraCAT users then enter information on wall, window and door component type and area. Again, as noted above, the wall, window and door entries are based on one unit, and one wall (except for corner units as discussed above). The tool sums total wall square footage based on the combined area of walls, doors and windows for the façade being evaluated.

Users may input STC values for materials in one of two ways. The tool includes a dropdown menu of common construction materials with STC values prefilled. If selected construction materials

manually. Verification of the component STC must be included in the ERR. Documentation includes the architect or construction manager's project plans showing wall material specifications. For new construction or for components that will be newly installed in an existing wall, documentation also includes the manufacturer's product specification sheet (cut sheet) documenting the STC rating of selected doors and windows.

Required STC Rating and Determination of Compliance

Finally, based on project information entered the tool will indicate the required STC rating for the wall assembly being evaluated and whether or not the materials specified will produce a combined rating that meets this requirement. Note that for noise levels above 75 dB DNL, either HUD (for 24 CFR Part 50 reviews) or the Responsible Entity (for 24 CFR Part 58 reviews) must approve the level and type of attenuation, among other processing requirements. Required attenuation values generated by STraCAT for NALs above 75 dB DNL should therefore be considered tentative pending approval by HUD or the RE.

Part Description

Project

60 Harper

Sponsor/Developer

MHT Housing

Location

60 Harper, Detroit, MI

Prepared by

Concept Design Studio

Noise Level

69

Date

6/9/2023



Primary Source(s)

Interstate

Part II - Wall Components

Wall Construction Detail

Area

STC

4" face brick one course

240

45

Add new wall

240 Sq. Feet

45

Window

Construction Detail

Quantity

Sq Ft/Unit

STC

dual pane low e

1

25

30

dual pane low e

2

15

30

Add new window

Door Construction Detail

Quantity

Sq Ft/Unit

STC

Add new door

PART III RESULTS

Wall Statistics

Stat	Value
Area:	240 ft ²
Wall STC:	45

Aperture Statistics

Aperture	Count	Area	% of wall
Windows:	3	55 ft ²	22.92%
Doors:	0	0 ft ²	0%

Evaluation Criteria

Criteria	Value
Noise source sound level (dB):	69
Combined STC for wall assembly:	35.96
Required STC rating:	27
Does wall assembly meet requirements?	Yes

[Print](#)

FACT TIPS

What do you do if the preferred wall design is not sufficient to achieve the required attenuation? Another wall design with more substantial materials will work, but may not be the most cost-effective solution. Try adding some other elements for just a little more attenuation.

For example:

- Staggering the studs in a wall offers approximately 4dB of additional protection.
- Increasing the stud spacing from 16" on center to 24" can increase the STC from 2-5dB.
- Adding a 2" air space can provide 3dB more attenuation.
- Increasing a wall's air space from 3" to 6" can reduce noise levels by an additional 5dB.
- Adding a layer of ½" gypsum board on "Z" furring channels adds 2dB of attenuation.
- Using resilient channels and clips between wall panels and studs can improve the STC from 2-5dB.
- Adding a layer of ½" gypsum board on resilient channels adds 5dB of attenuation.
- Adding acoustical or isolation blankets to a wall's airspace can add 4-10dB of attenuation.
- A 1" rockwool acoustical blanket adds 3dB to the wall's STC.
- Filling the cells of lightweight concrete masonry units with expanded mineral loose-fill insulation adds 2dB to the STC.

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Typ. First Floor Corner Unit

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How to Use This Tool

Location, Noise Level and Wall Configuration to Be Analyzed

STraCAT is designed to calculate the attenuation provided by the wall assembly for one wall of one unit. If unit exterior square footage and window/door configuration is identical around the structure, a single STraCAT may be sufficient. If units vary, at least one STraCAT should be completed for each different exterior unit wall configuration to document that all will achieve the required attenuation. Additionally, if attenuation is not based on a single worst-case NAL, but there are multiple NALs which require different levels of attenuation around the structure, a STraCAT should be completed for each differing exterior wall configuration associated with each NAL.

Exterior wall configurations associated with an NAL include those with parallel (facing) or near-parallel exposure as well as those with perpendicular exposure. When a façade has parallel or perpendicular exposure to two or more NALs, you should base the required attenuation on the NAL with the highest calculated noise level. For corner units where the unit interior receives exterior noise through two facades, the STraCAT calculation should incorporate the area of wall, window and door materials pertaining to the corner unit's total exterior wall area (i.e., from both walls).

Information to Be Entered

Users first enter basic project information and the NAL noise level that will be used as the basis for required attenuation. This noise level must be entered in whole numbers. STraCAT users then enter information on wall, window and door component type and area. Again, as noted above, the wall, window and door entries are based on one unit, and one wall (except for corner units as discussed above). The tool sums total wall square footage based on the combined area of walls, doors and windows for the façade being evaluated.

Users may input STC values for materials in one of two ways. The tool includes a dropdown menu of common construction materials with STC values prefilled. If selected construction materials

manually. Verification of the component STC must be included in the ERR. Documentation includes the architect or construction manager's project plans showing wall material specifications. For new construction or for components that will be newly installed in an existing wall, documentation also includes the manufacturer's product specification sheet (cut sheet) documenting the STC rating of selected doors and windows.

Required STC Rating and Determination of Compliance

Finally, based on project information entered the tool will indicate the required STC rating for the wall assembly being evaluated and whether or not the materials specified will produce a combined rating that meets this requirement. Note that for noise levels above 75 dB DNL, either HUD (for 24 CFR Part 50 reviews) or the Responsible Entity (for 24 CFR Part 58 reviews) must approve the level and type of attenuation, among other processing requirements. Required attenuation values generated by STraCAT for NALs above 75 dB DNL should therefore be considered tentative pending approval by HUD or the RE.

Part Description

Project

60 Harper

Sponsor/Developer

MHT Housing

Location

60 Harper, Detroit, MI

Prepared by

Concept Design Studio

Noise Level

69

Date

6/9/2023

**Primary Source(s)**

Interstate

Part II - Wall Components

Wall Construction Detail	Area	STC
--------------------------	------	-----

4" face brick one course	448	45
--------------------------	-----	----

Add new wall

448 Sq. Feet	45
---------------------	-----------

Window Construction Detail	Quantity	Sq Ft/Unit	STC
----------------------------	----------	------------	-----

dual pane low e	1	25	30
-----------------	---	----	----

dual pane low e	2	15	30
-----------------	---	----	----

Add new window

Door Construction Detail	Quantity	Sq Ft/Unit	STC
--------------------------	----------	------------	-----

Add new door

PART III RESULTS

Wall Statistics

Stat	Value
Area:	448 ft ²
Wall STC:	45

Aperture Statistics

Aperture	Count	Area	% of wall
Windows:	3	55 ft ²	12.28%
Doors:	0	0 ft ²	0%

Evaluation Criteria

Criteria	Value
Noise source sound level (dB):	69
Combined STC for wall assembly:	38.22
Required STC rating:	27
Does wall assembly meet requirements?	<input type="button" value="Yes"/>

FACT TIPS

What do you do if the preferred wall design is not sufficient to achieve the required attenuation? Another wall design with more substantial materials will work, but may not be the most cost-effective solution. Try adding some other elements for just a little more attenuation.

For example:

- Staggering the studs in a wall offers approximately 4dB of additional protection.
- Increasing the stud spacing from 16" on center to 24" can increase the STC from 2-5dB.
- Adding a 2" air space can provide 3dB more attenuation.
- Increasing a wall's air space from 3" to 6" can reduce noise levels by an additional 5dB.
- Adding a layer of ½" gypsum board on "Z" furring channels adds 2dB of attenuation.
- Using resilient channels and clips between wall panels and studs can improve the STC from 2-5dB.
- Adding a layer of ½" gypsum board on resilient channels adds 5dB of attenuation.
- Adding acoustical or isolation blankets to a wall's airspace can add 4-10dB of attenuation.
- A 1" rockwool acoustical blanket adds 3dB to the wall's STC.
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Typ 2-3 unit w/ (3)+(1) window

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Information to Be Entered

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Part Description

Project

60 Harper

Sponsor/Developer

MHT Housing

Location

60 Harper, Detroit, MI

Prepared by

Concept Design Studio

Noise Level

69

Date

6/9/2023



Primary Source(s)

Interstate

Part II - Wall Components

Wall Construction Detail

Area

STC

2x4" wood studs 16"o.c.; 5/8" gypsum board

512

28

Add new wall

512 Sq. Feet

28

Window

Construction Detail

Quantity

Sq Ft/Unit

STC

dual pane low e

1

25

30

dual pane low e

3

15

30

Add new window

Door Construction Detail

Quantity

Sq Ft/Unit

STC

Add new door

PART III RESULTS

Wall Statistics

Stat	Value
Area:	512 ft ²
Wall STC:	28

Aperture Statistics

Aperture	Count	Area	% of wall
Windows:	4	70 ft ²	13.67%
Doors:	0	0 ft ²	0%

Evaluation Criteria

Criteria	Value
Noise source sound level (dB):	69
Combined STC for wall assembly:	28.22
Required STC rating:	27
Does wall assembly meet requirements?	Yes

[Print](#)

STC Tips

What do you do if the preferred wall design is not sufficient to achieve the required attenuation? Another wall design with more substantial materials will work, but may not be the most cost-effective solution. Try adding some other elements for just a little more attenuation.

For example:


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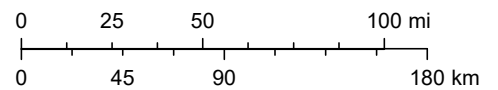
ArcGIS Web AppBuilder



9/29/2022, 2:08:29 PM

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Esri, HERE, Garmin, NGA, USGS, NPS

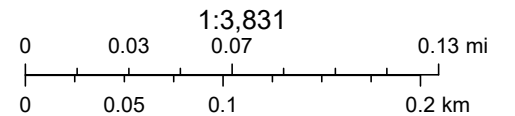
Wetlands Map Viewer



May 31, 2023

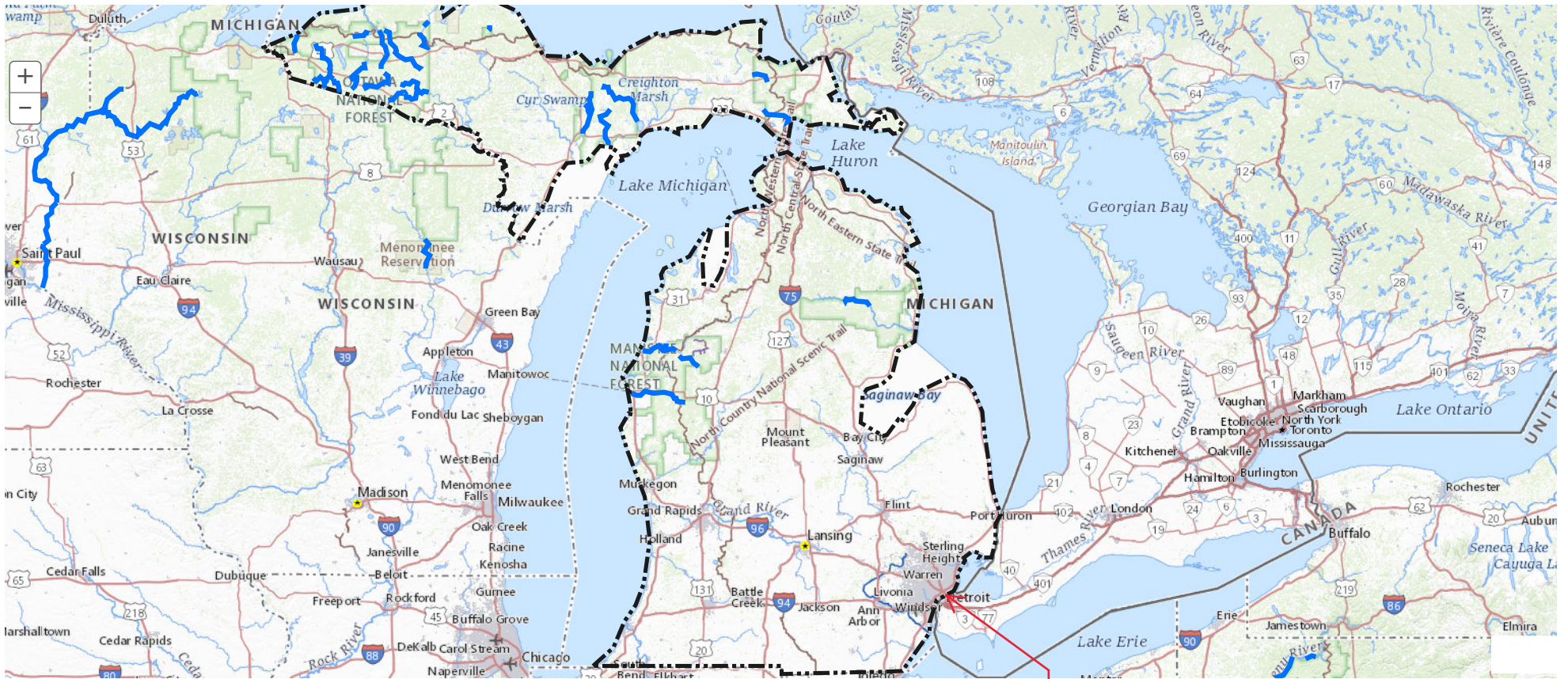
Part 303 Final Wetlands Inventory

-  Wetlands as identified on NWI and MIRIS maps
-  Soil areas which include wetland soils
-  Wetlands as identified on NWI and MIRIS maps and soil areas which include wetland soils



Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community. Source: Esri, Maxar, Earthstar Geographics, and the GIS User Community

Disclaimer: This map is not intended to be used to determine the specific



Project is not located in proximity to designated Wild and Scenic Rivers

EJScreen Report (Version 2.11)

1 mile Ring around the Area

MICHIGAN, EPA Region 5

Approximate Population: 17,073

Input Area (sq. miles): 3.37

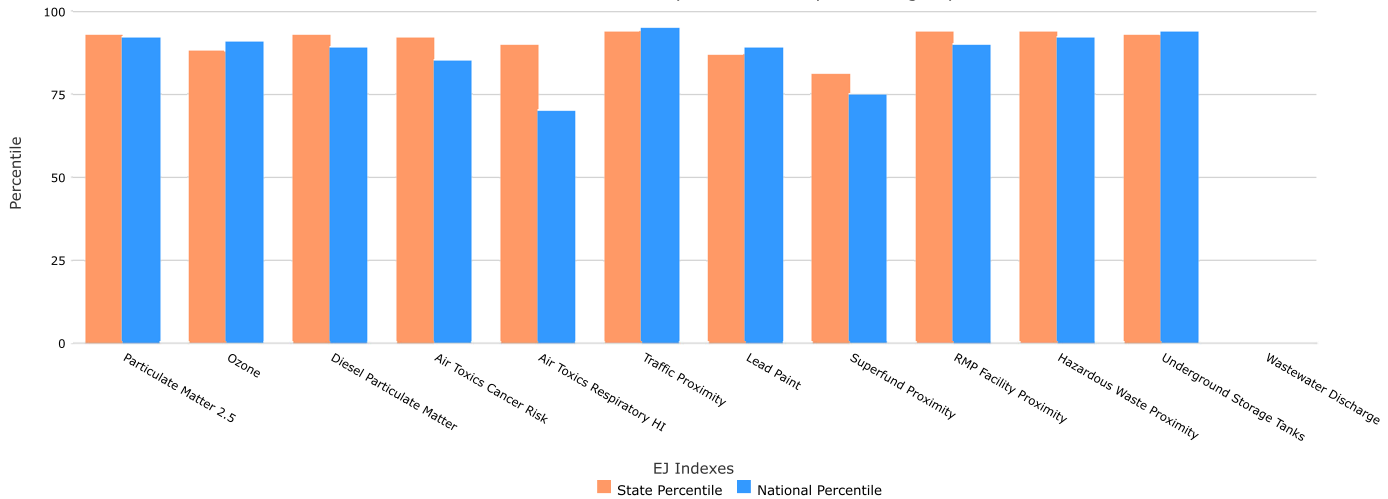
60 Harper

(The study area contains 2 blockgroup(s) with zero population.)

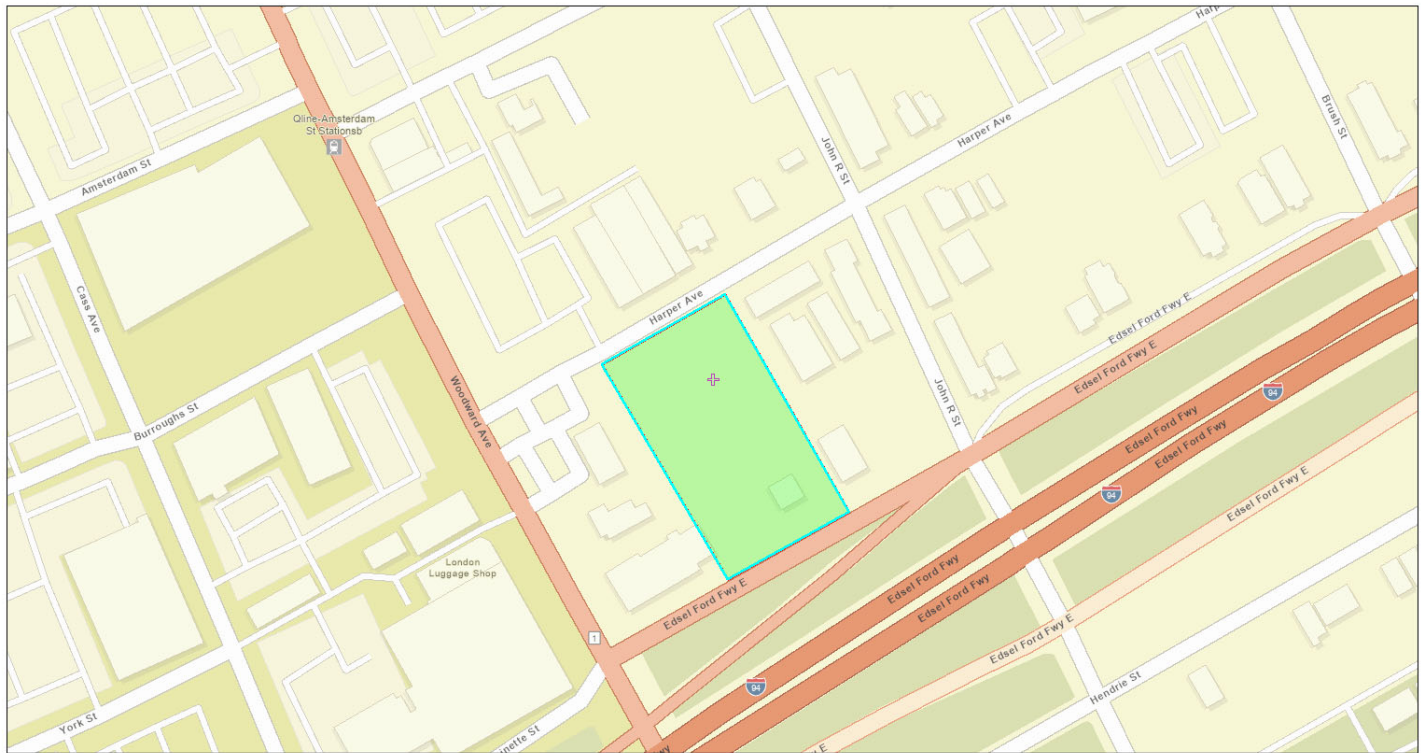
Selected Variables	Percentile in State	Percentile in USA
Environmental Justice Indexes		
Particulate Matter 2.5 EJ Index	93	92
Ozone EJ Index	88	91
Diesel Particulate Matter EJ Index*	93	89
Air Toxics Cancer Risk EJ Index*	92	85
Air Toxics Respiratory HI EJ Index*	90	70
Traffic Proximity EJ Index	94	95
Lead Paint EJ Index	87	89
Superfund Proximity EJ Index	81	75
RMP Facility Proximity EJ Index	94	90
Hazardous Waste Proximity EJ Index	94	92
Underground Storage Tanks EJ Index	93	94
Wastewater Discharge EJ Index	0	0

EJ Indexes - The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator

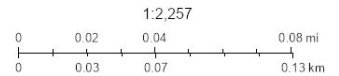
EJ Index for the Selected Area Compared to All People's Blockgroups in the State/US



*Diesel particulate matter, air toxics cancer risk, and air toxics respiratory hazard index are from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. Cancer risks and hazard indices from the Air Toxics Data Update are reported to one significant figure and any additional significant figures here are due to rounding. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>. (<https://www.epa.gov/haps/air-toxics-data-update>)



June 12, 2023
 60 Harper
 Search Result (point)



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Sites reporting to EPA	
Superfund NPL	0
Hazardous Waste Treatment, Storage, and Disposal Facilities (TSDF)	3

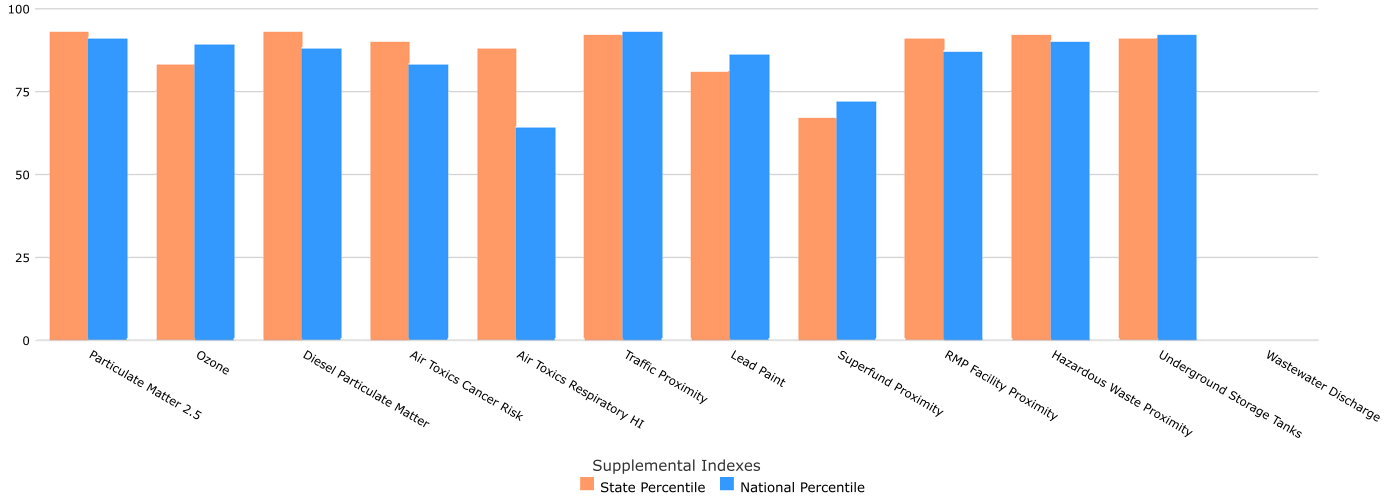
Selected Variables	Value	State		USA	
		Avg.	%tile	Avg.	%tile
Pollution and Sources					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	10.1	8.73	98	8.67	86
Ozone (ppb)	44.8	43.8	57	42.5	74
Diesel Particulate Matter* ($\mu\text{g}/\text{m}^3$)	0.408	0.211	95	0.294	70-80th
Air Toxics Cancer Risk* (lifetime risk per million)	30	23	99	28	80-90th
Air Toxics Respiratory HI*	0.3	0.25	99	0.36	<50th
Traffic Proximity (daily traffic count/distance to road)	6300	910	98	760	98
Lead Paint (% Pre-1960 Housing)	0.55	0.37	70	0.27	77
Superfund Proximity (site count/km distance)	0.051	0.15	41	0.13	44
RMP Facility Proximity (facility count/km distance)	1.4	0.54	88	0.77	82
Hazardous Waste Proximity (facility count/km distance)	5.3	1.1	98	2.2	88
Underground Storage Tanks (count/km ²)	45	8	98	3.9	99
Wastewater Discharge (toxicity-weighted concentration/m distance)	2.9E-09	0.45	0	12	0
Socioeconomic Indicators					
Demographic Index	62%	28%	89	35%	84
Supplemental Demographic Index	21%	14%	83	15%	80
People of Color	66%	26%	87	40%	76
Low Income	58%	31%	86	30%	86
Unemployment Rate	9%	6%	74	5%	77
Limited English Speaking	4%	2%	88	5%	73
Less Than High School Education	12%	9%	72	12%	62
Under Age 5	3%	6%	29	6%	29
Over Age 64	11%	17%	26	16%	30
Low Life Expectancy	15%	20%	8	20%	14

EJScreen is a screening tool for pre-decisional use only. It can help identify areas that may warrant additional consideration, analysis, or outreach. It does not provide a basis for decision-making, but it may help identify potential areas of EJ concern. Users should keep in mind that screening tools are subject to substantial uncertainty in their demographic and environmental data, particularly when looking at small geographic areas. Important caveats and uncertainties apply to this screening-level information, so it is essential to understand the limitations on appropriate interpretations and applications of these indicators. Please see EJScreen documentation for discussion of these issues before using reports. This screening tool does not provide data on every environmental impact and demographic factor that may be relevant to a particular location. EJScreen outputs should be supplemented with additional information and local knowledge before taking any action to address potential EJ concerns.

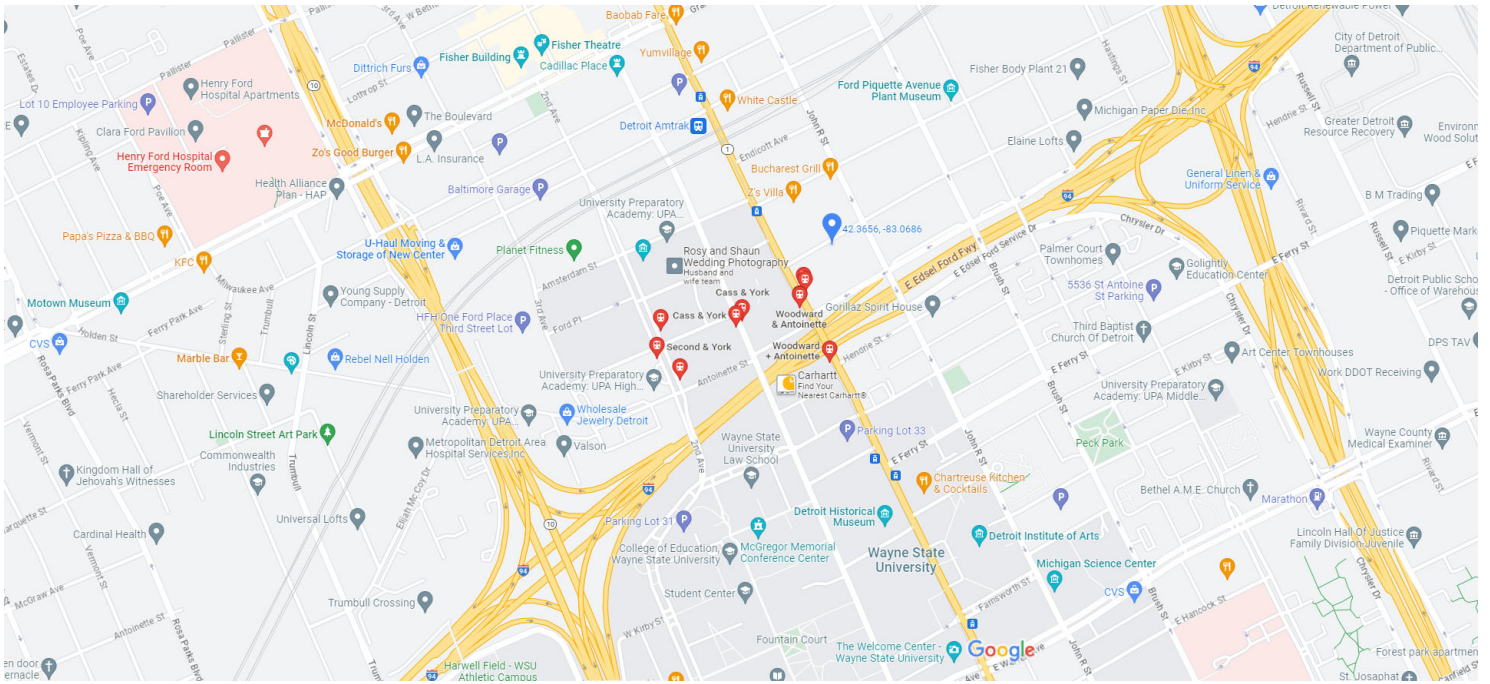
Selected Variables	Percentile in State	Percentile in USA
Supplemental Indexes		
Particulate Matter 2.5 Supplemental Index	93	91
Ozone Supplemental Index	83	89
Diesel Particulate Matter Supplemental Index*	93	88
Air Toxics Cancer Risk Supplemental Index*	90	83
Air Toxics Respiratory HI Supplemental Index*	88	64
Traffic Proximity Supplemental Index	92	93
Lead Paint Supplemental Index	81	86
Superfund Proximity Supplemental Index	67	72
RMP Facility Proximity Supplemental Index	91	87
Hazardous Waste Proximity Supplemental Index	92	90
Underground Storage Tanks Supplemental Index	91	92
Wastewater Discharge Supplemental Index	0	0

Supplemental Indexes - The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on low-income, limited English speaking, less than high school education, unemployed, and low life expectancy populations with a single environmental indicator.

Supplemental Index for the Selected Area Compared to All People's Blockgroups in the State/US



This report shows the values for environmental and demographic indicators, EJScreen indexes, and supplemental indexes. It shows environmental and demographic raw data (e.g., the estimated concentration of ozone in the air), and also shows what percentile each raw data value represents. These percentiles provide perspective on how the selected block group or buffer area compares to the entire state, EPA region, or nation. For example, if a given location is at the 95th percentile nationwide, this means that only 5 percent of the US population has a higher block group value than the average person in the location being analyzed. The years for which the data are available, and the methods used, vary across these indicators. Important caveats and uncertainties apply to this screening-level information, so it is essential to understand the limitations on appropriate interpretations and applications of these indicators. Please see EJScreen documentation for discussion of these issues before using reports. For additional information, see: www.epa.gov/environmentaljustice



Map data ©2023 Google 500 ft

Cass & York
Bus stop



[Directions](#)

Cass & York
Bus stop



[Directions](#)

Woodward & Antoinette
Bus stop



[Directions](#)

Woodward + Amsterdam
Bus stop



[Directions](#)

Second & Antoinette
Bus stop



[Directions](#)

Second & York
Bus stop



[Directions](#)

Woodward & I-94 Service Dr
Bus stop



[Directions](#)

Woodward + Ford Fwy
Bus stop



[Directions](#)

Second & York
Bus stop



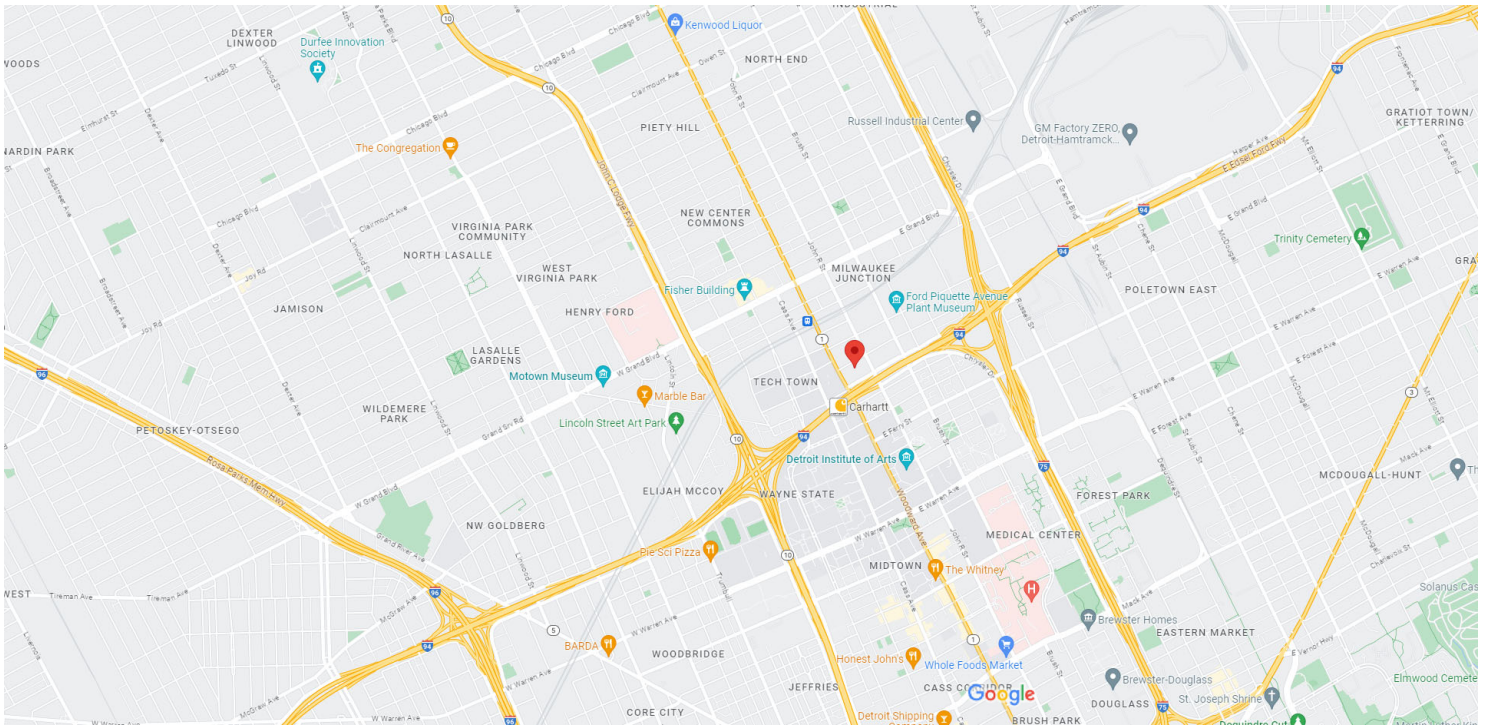
[Directions](#)

Woodward + Antoinette
Bus stop



[Directions](#)






You've reached the end of the list.




Map data ©2023 Google 1000 ft

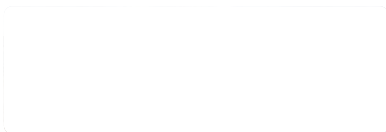


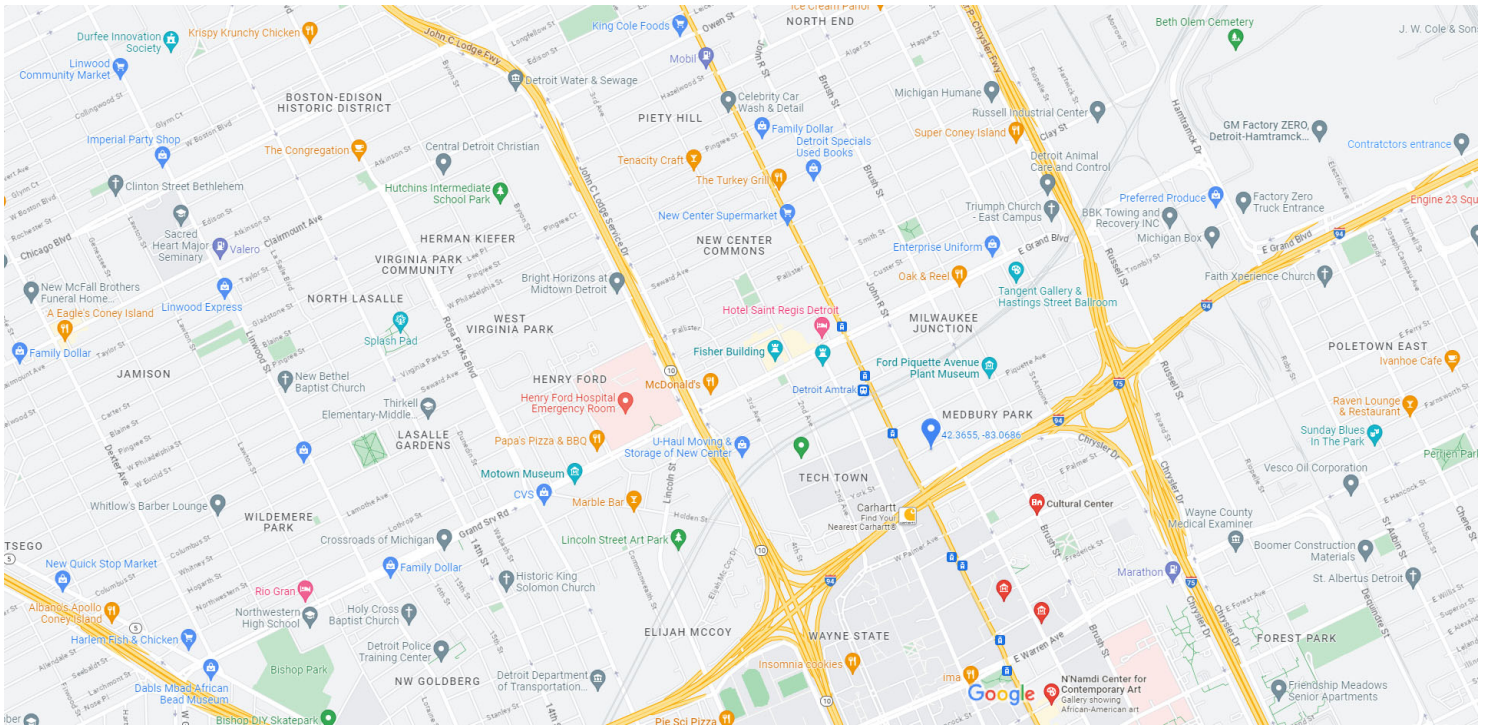
60 Harper Ave

-  Directions
-  Save
-  Nearby
-  Send to phone
-  Share

 60 Harper Ave, Detroit, MI 48202
9W8J+5H Detroit, Michigan

Photos





Map data ©2023 Google 1000 ft

Cultural Center

No reviews
Detroit, MI



Bint Jebail Cultural Center

4.5 (151)
Banquet hall · 14201 Prospect St
Bldg. 2
(313) 584-0011



"Sparkling clean facility"

Mackenzie Hall Cultural Centre

4.4 (273)
Cultural center · 3277 Sandwich St.
W
Open · Closes 4:30 PM · +1 519-255-7600



"Beautiful facility, staff pleasant."

N'Namdi Center for Contemporary Art

4.8 (143)
Art gallery · 52 E Forest Ave
Gallery showing African-American art
Open · Closes 5 PM · (313) 831-8700



"I attended a fundraiser brunch and the facility was tastefully decorated."

Detroit Institute of Arts

4.8 (10,504)
Art museum · 5200 Woodward Ave
Large art museum displaying global works
Open · Closes 4 PM · (313) 833-7900



"The facility has benefited from the deep pockets of the patrons of the ..."

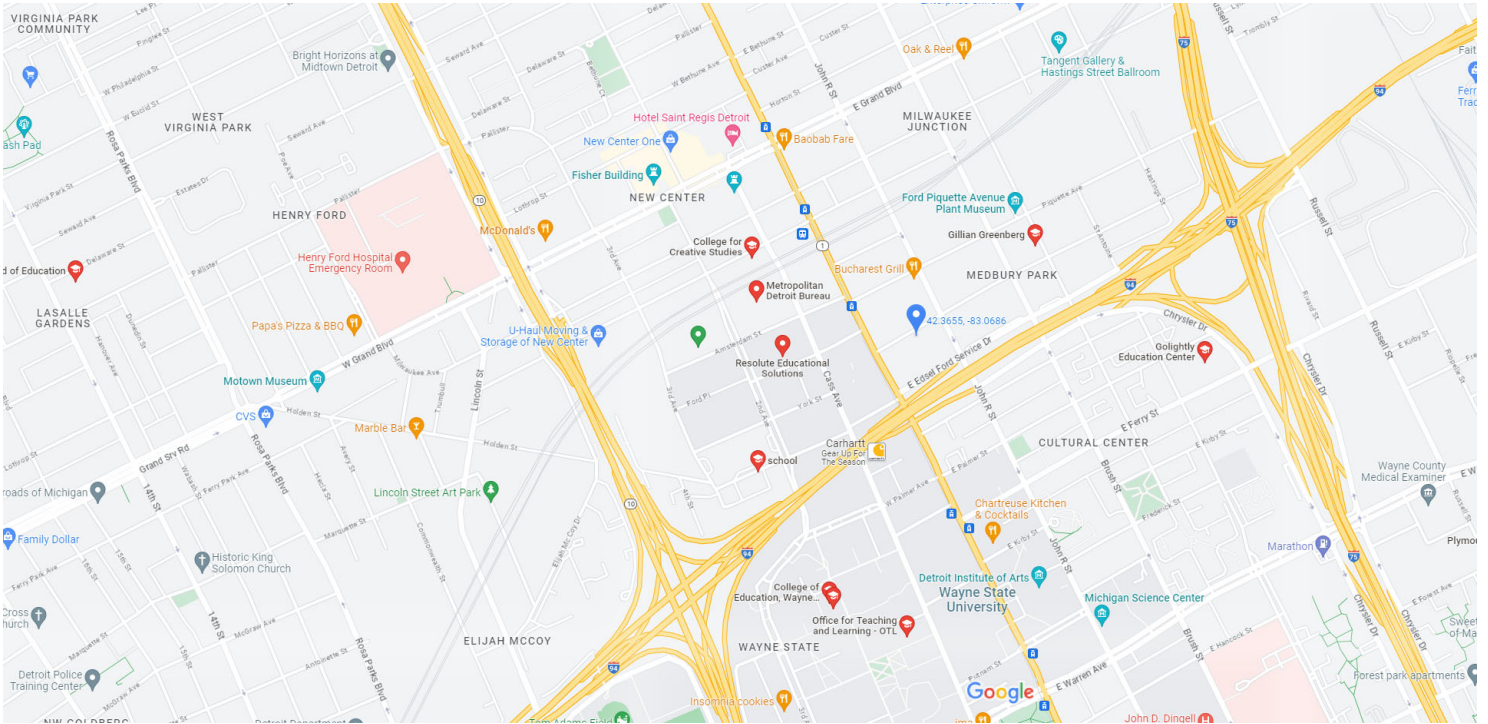
Ukrainian Cultural Center

4.5 (313)
Banquet hall · 26601 Ryan Rd
Open · Closes 5 PM · (586) 757-8130



"Nice facility for any large gathering, weddings, baby shower, etc."

education



Map data ©2023 Google 500 ft

Rating Hours All filters

Golightly Education Center
4.0 (9)
Middle school · 5536 St Antoine
(313) 870-1101



Website

Directions

Department of Teacher Education
No reviews
University · Education Building



Directions

College of Education, Wayne State University
4.9 (7)
University · Education Building, 5425 Gullen Mall
Open · Closes 10 PM · (313) 577-1620



Website

Directions

Resolute Educational Solutions
No reviews
Educational consultant · 440 Burroughs St Suite 160
Open · Closes 5 PM · (734) 707-1126
Onsite services · Online classes



Website

Directions

Board of Education
5.0 (1)
School · 7724 14th St
(313) 873-3111



Website

Directions

school
No reviews
School · 627 Antoinette St



Directions

Plymouth Ed Center
4.0 (5)
Education center · 1460 E Forest Ave
Open · Closes 5 PM · (313) 831-3280



Website

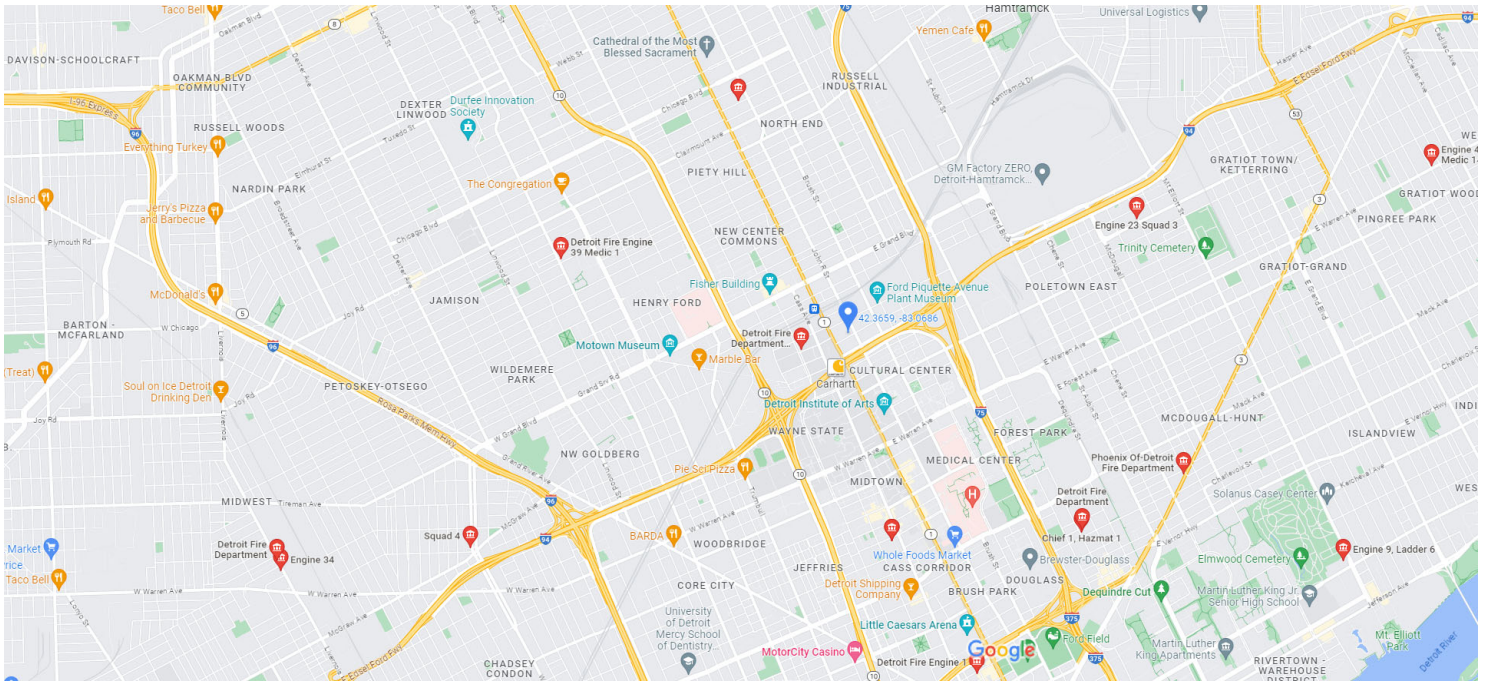
Directions

Office for Teaching and Learning - OTL
No reviews
Teacher college · 5265 Cass Ave
Open · Closes 5 PM · (313) 577-0001



Website

Directions




Map data ©2023 Google 2000 ft

Rating Hours All filters

- Detroit Fire Department Engine 17
Ladder 7 Chief 5**


4.3 (6)

Fire station · 6100 2nd Ave


[Directions](#)
- Detroit Fire Department**


No reviews

Fire station · 435 W Alexandrine St


[Directions](#)
- Engine 35**

No reviews


Fire station · 111 Kenilworth St


[Directions](#)
- Detroit Fire Department**

5.0 (1)

Fire station · 3050 Russell St



Open 24 hours


[Directions](#)
- Detroit Fire Engine 39 Medic 1**

5.0 (3)

Fire station · 8700 14th St


Open 24 hours



[Website](#) [Directions](#)
- Engine 23 Squad 3**

5.0 (3)

Fire station · 1818 E Grand Blvd



Open 24 hours · (313) 333-8926


[Directions](#)
- Detroit Fire Engine 1**

5.0 (1)

Fire station · 111 W Montcalm St



Open 24 hours



[Website](#) [Directions](#)
- Hamtramck Fire Department**

3.4 (5)


Fire station · 2625 Caniff St

Open 24 hours · (313) 305-4503



[Website](#) [Directions](#)
- Chief 1, Hazmat 1**


5.0 (1)

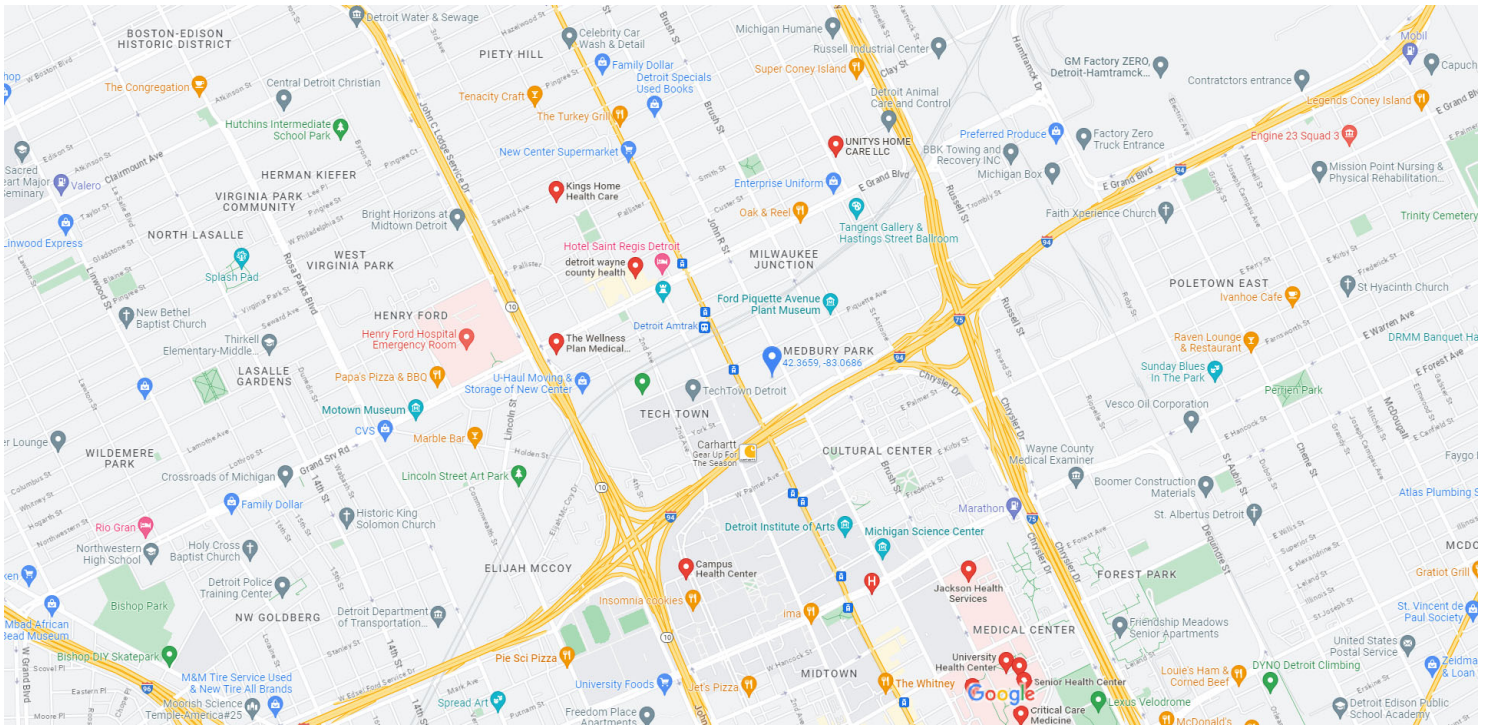
Fire station · 3080 Russell St


[Directions](#)
- Phoenix Of-Detroit Fire Department**

No reviews

Fire station · 2733 Gratiot Ave


[Directions](#)



Rating Hours All filters

Jackson Health Services
 2.5 (11)
 Medical clinic · 4727 St Antoine #3/202
 (313) 831-8805

[Directions](#)

Romancare Health Services
 No reviews
 Medical clinic · 9600 Dexter Ave
 Closed · Opens 10 AM Fri · (248) 218-1198

[Website](#) [Directions](#)

Wayne County Healthy Communities
 3.4 (24)
 Medical clinic · 9021 Joseph Campau Ave
 Open · Closes 5:30 PM · (313) 871-1926

[Website](#) [Directions](#)

● "Prompt, friendly, professional, caring"

Heart Care
 No reviews
 Medical clinic · 4160 John R St #827
 (313) 833-7233

[Directions](#)

Advantage Health Centers
 3.8 (26)
 Medical Center · 60 E Warren Ave
 Open · Closes 5 PM · (313) 416-6262

[Website](#) [Directions](#)

Senior Health Center
 No reviews
 Medical clinic · 4201 St Antoine
 (313) 993-0639

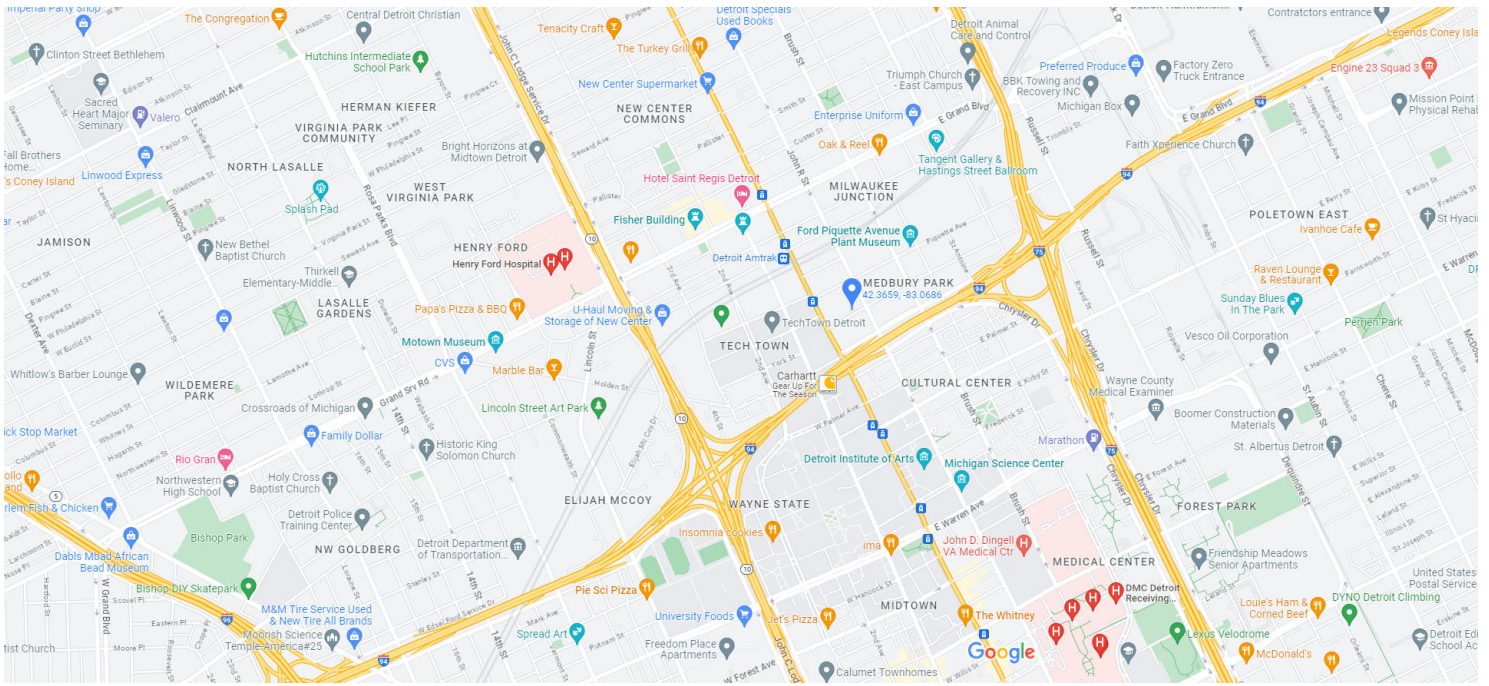
[Directions](#)

Kings Home Health Care
 No reviews
 Home health care service · 675 Seward Ave
 Open now

[Directions](#)

detroit wayne county health
 5.0 (1)
 Medical clinic · 3031 W Grand Blvd Suite 545

[Website](#) [Directions](#)



Map data ©2023 Google 1000 ft

Rating Hours All filters

DMC Harper University Hospital
 2.6 (279)
 General hospital · 3990 John R St
 Open 24 hours · (313) 745-8040
 "Only one CT technician or entire Hospital"

Henry Ford Hospital
 3.0 (1,329)
 General hospital · 2799 W Grand Blvd
 Open 24 hours · (313) 916-2600
 "That was not good but this is a hospital in the hood so that is expected."

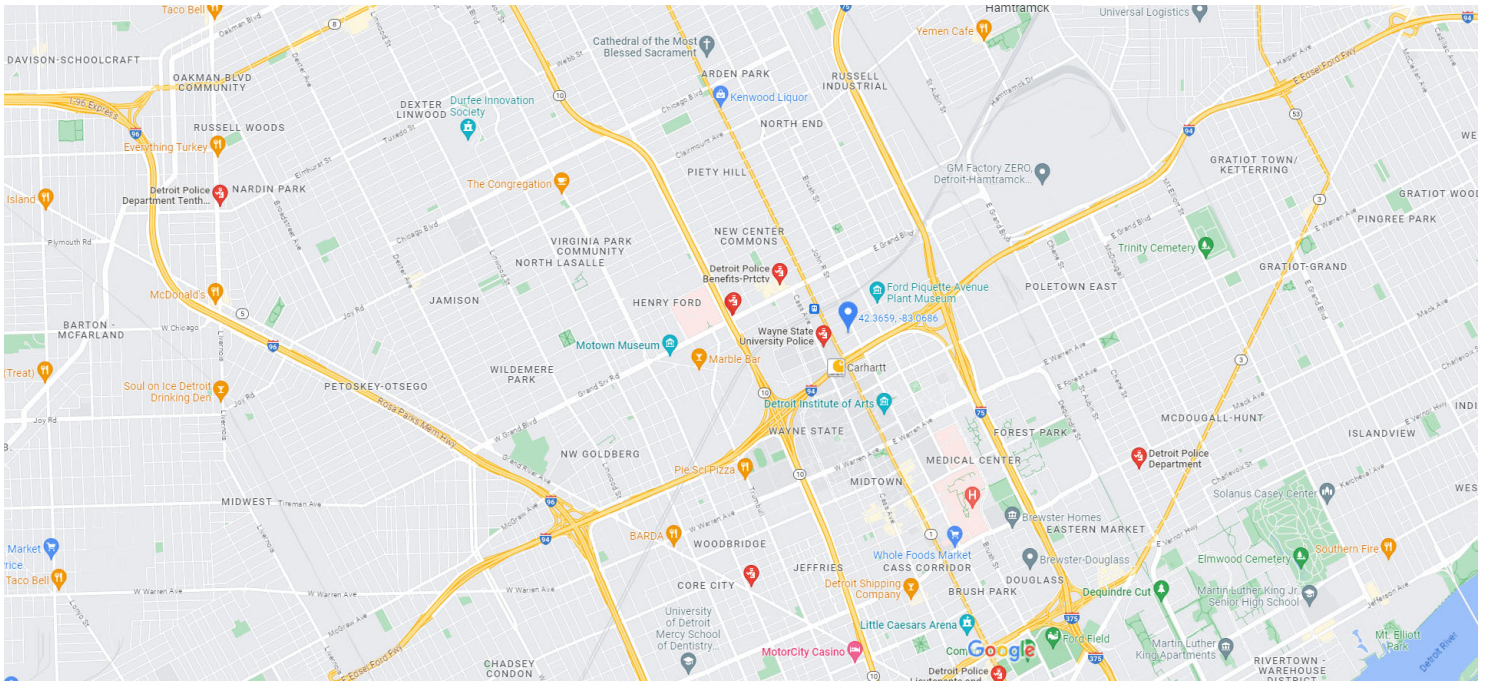
DMC Detroit Receiving Hospital
 2.4 (405)
 General hospital · 4201 St Antoine
 Open 24 hours · (313) 745-3000
 "But the rest of the Hospital is the best!"

Fresenius Medical Care at Henry Ford Hospital
 No reviews
 Hospital · 2799 W Grand Blvd
 (313) 916-2600

Windsor Regional Hospital - Ouellette Campus
 2.6 (287)
 General hospital · 1030 Ouellette Ave
 Open 24 hours · +1 519-254-5577
 "Understaffed hospital."

Detroit Medical Center Central Campus
 5.0 (1)
 Medical Center · 4201 St Antoine
 Open now · (313) 745-3000

DMC Sinai Grace Hospital
 2.0 (377)
 General hospital · 6071 Outer Dr W
 Open 24 hours · (313) 966-3300



Map data ©2023 Google 2000 ft

Rating All filters

Wayne State University Police
 4.5 (22)
 Police department · 6050 Cass Ave
 (313) 577-2222

[Directions](#)

3rd Precinct NPO
 3.6 (27)
 Police department · 2875 W Grand Blvd
 (313) 596-5300

[Directions](#)

Detroit Police Benefits-Prctct
 5.0 (2)
 Police department · 3031 W Grand Blvd #43/405
 Closes soon · 3 PM · Opens 7 AM Fri
 · (313) 870-9301

[Website](#) [Directions](#)

Eighth Precinct Police Station
 No reviews
 Police station · 4150 Grand River Ave

[Directions](#)

Hamtramck Police Department
 2.5 (41)
 Police department · 3401 Evaline St
 Open 24 hours · (313) 800-5281

[Website](#) [Directions](#)

Detroit Police Department
 No reviews
 Police department · Scott St

[Directions](#)

Detroit Police Department
 2.9 (20)
 Police department · 1301 3rd Ave
 (313) 596-2200

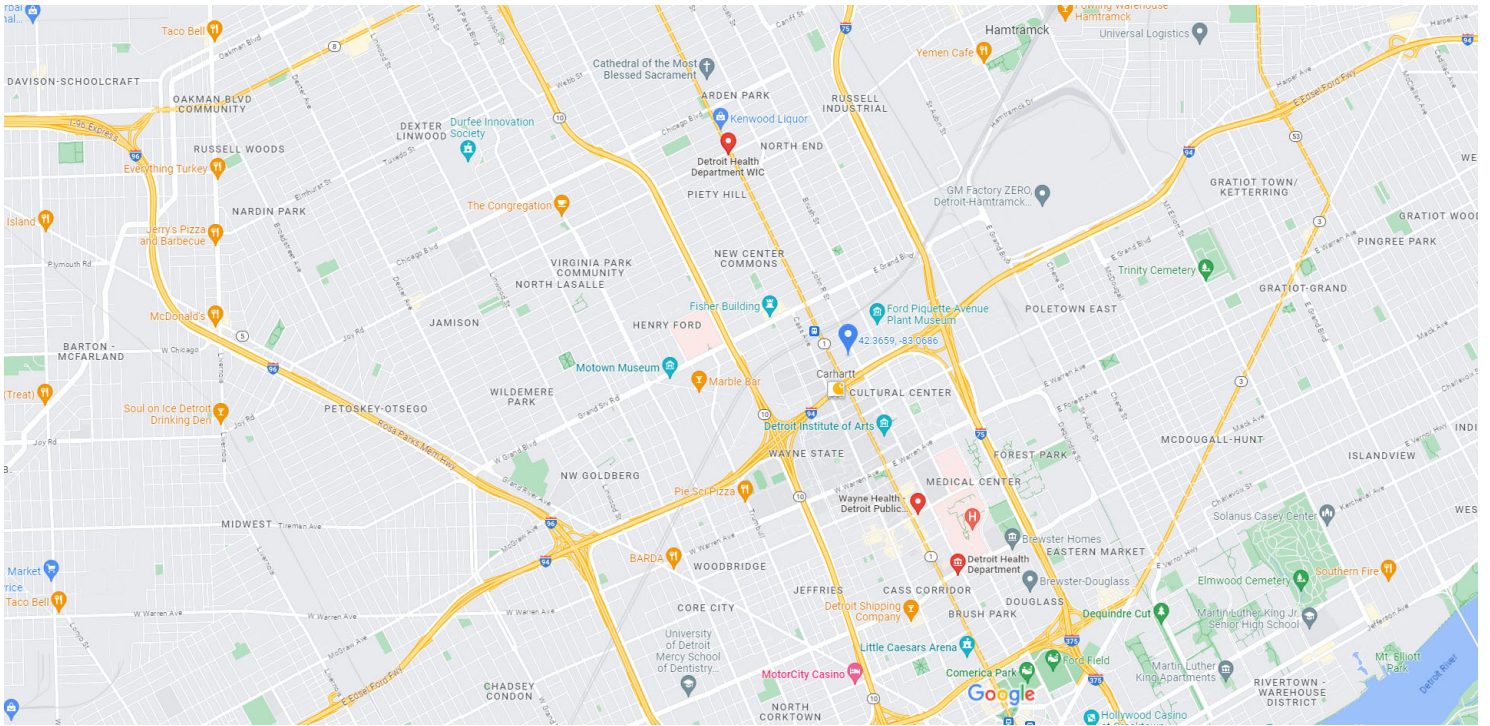
[Website](#) [Directions](#)

Detroit Police Department
 2.3 (3)
 Police department · 1434 Michigan Ave

[Website](#) [Directions](#)

Detroit Police Department
 2.0 (11)
 Police department · 20 Atwater St
 (313) 237-2850

[Directions](#)



Map data ©2023 Google 2000 ft

Rating Hours All filters

Detroit Health Department
 3.0 (24)
 City government office · 100 Mack Ave
[Website](#) [Directions](#)
 Open · Closes 4 PM · (313) 876-4000

Wayne Health - Detroit Public Health STD Clinic
 2.8 (42)
 Public health department · 50 E Canfield St
[Website](#) [Directions](#)
 Open · Closes 4 PM · (313) 577-9100

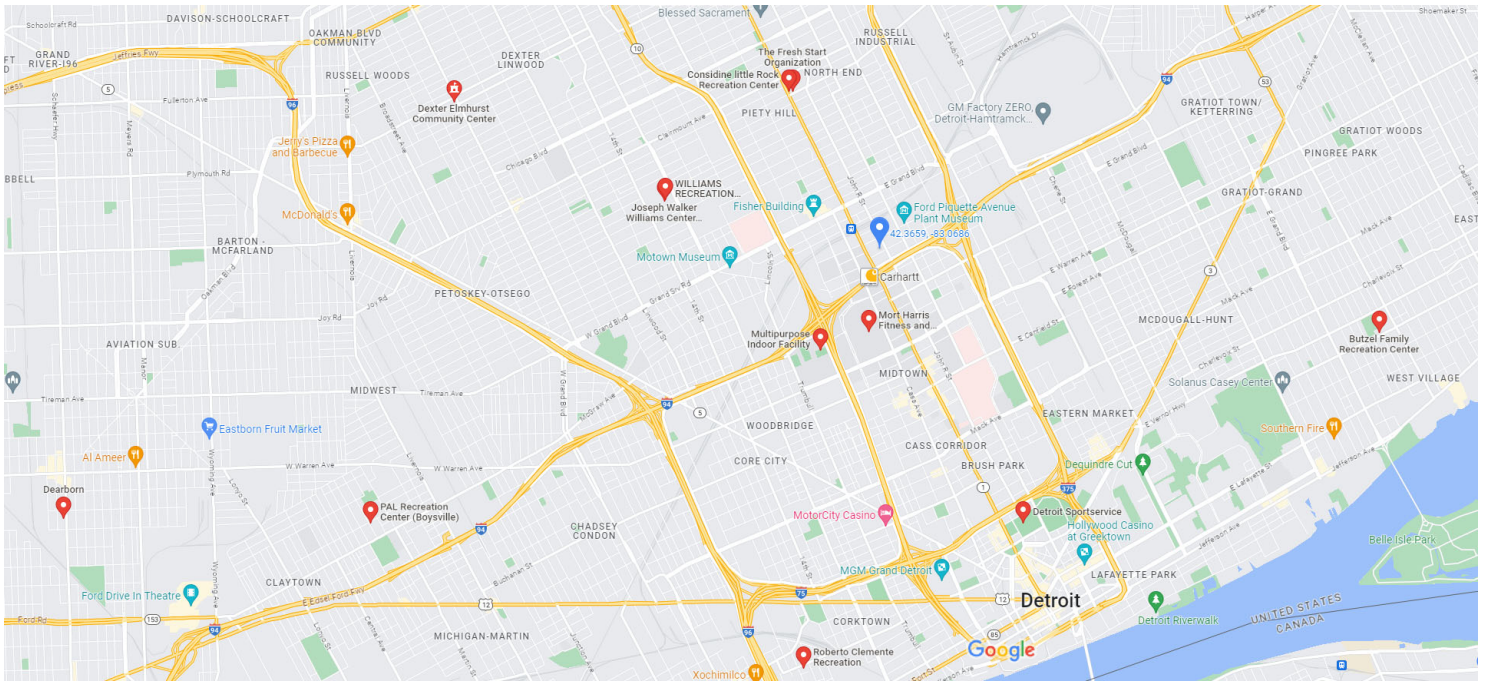
Institute for Population Health
 4.0 (20)
 Medical clinic · 9053 Woodward Ave
[Website](#) [Directions](#)
 Open · Closes 5 PM · (313) 309-9350

Windsor-Essex County Health Unit
 3.4 (106)
 Public health department · 1005 Ouellette Ave
[Website](#) [Directions](#)
 Open · Closes 4:30 PM · +1 519-258-2146

Macomb County Health Department
 4.1 (42)
 Public health department · 27690 Van Dyke Ave
[Website](#) [Directions](#)
 Open · Closes 5 PM · (586) 465-8090

Wayne County Dept of Health, Human & Veterans Services
 3.7 (3)
 Public health department · 500 Griswold St 10th floor
[Website](#) [Directions](#)
 Open · Closes 4:30 PM · (313) 224-0810

"This site is listed incorrectly and is not the Public Health Department!"



Map data ©2023 Google 2000 ft

Rating Hours All filters

Considine little Rock Recreation Center
 5.0 (2)
 Recreation center [Directions](#)

WILLIAMS RECREATION CENTER
 4.2 (6)
 Recreation center · 8431 Rosa Parks Blvd [Website](#) [Directions](#)

Multipurpose Indoor Facility
 4.6 (10)
 Recreation center (313) 577-2253 [Website](#) [Directions](#)

Joseph Walker Williams Center Swim Lessons
 4.5 (254)
 Recreation center · Virginia Park Community Plaza Shopping Center, 8431 Rosa Parks Blvd [Website](#) [Directions](#)
 Open · Closes 4:30 PM · (313) 628-2039

Roberto Clemente Recreation
 4.7 (67)
 Recreation center · 2631 Bagley St [Website](#) [Directions](#)
 Open · Closes 9 PM · (313) 224-0228

Hamtramck High School Community Center
 4.2 (38)
 Community center · 11350 Charest St [Website](#) [Directions](#)
 Open · Closes 10 PM · (313) 591-7402

Lasky Recreation Center
 4.2 (150)
 Recreation center · 13200 Fenelon St [Website](#) [Directions](#)
 Open · Closes 9 PM · (313) 628-2030

Butzel Family Recreation Center
 4.3 (150)
[Website](#) [Directions](#)

NO.	DATE	ISSUANCE / REVISION
1	04-12-23	DATE CDS
2	05-31-23	DATE CDS
3	04-14-23	DATE CDS
4	05-09-23	FOR PERMITS

60 HARPER APARTMENTS
 A MIXED USE DEVELOPMENT
 CITY OF DETROIT, MI 48202
 WAYNE COUNTY, MICHIGAN
 SITE PLAN

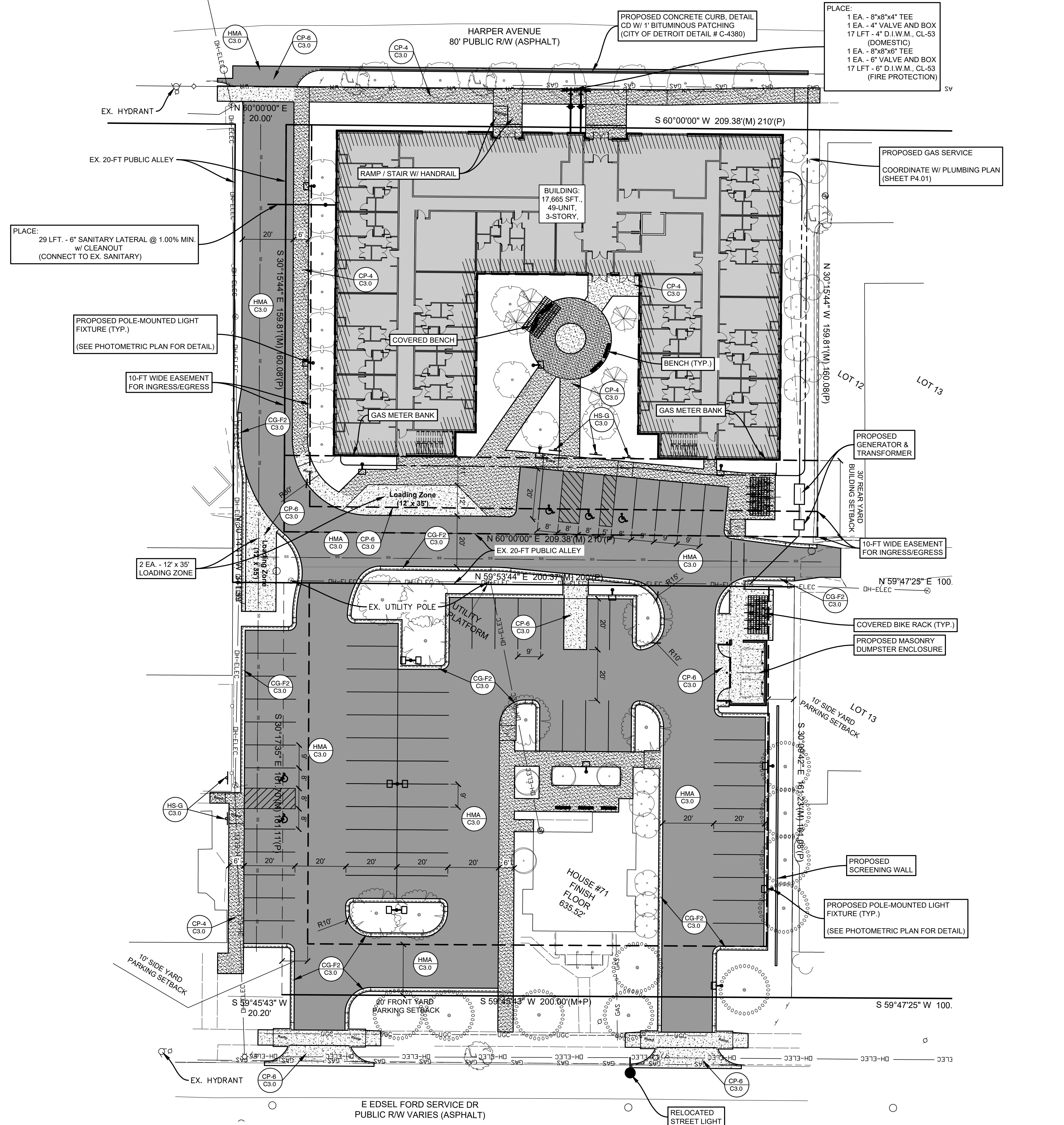
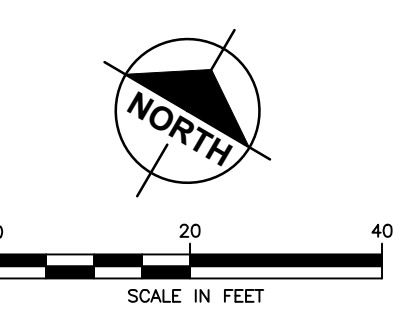
NO.	DATE	ISSUANCE / REVISION
1	04-12-23	DATE CDS
2	05-31-23	DATE CDS
3	04-14-23	DATE CDS
4	05-09-23	FOR PERMITS

PROJECT 022 60 HARPER
 SHEET NO. C1.0

- GENERAL NOTES**
- PROJECT INCLUDES CONSTRUCTION OF ONE (1) NEW THREE (3) STORY MIXED-USE BUILDING WITH 49 TOTAL RESIDENTIAL UNITS. RELATED IMPROVEMENTS INCLUDE PARKING, LANDSCAPING, SIGNAGE, LIGHTING, UTILITIES, AND STORM WATER DRAINAGE FACILITIES.
 - THE DEVELOPMENT SHALL BE SERVED BY PUBLIC WATER MAIN AND PUBLIC SANITARY SEWER.
 - ACCESS TO THE DEVELOPMENT SHALL BE VIA ONE (1) EX. CURB CUT ONTO HARPER AVENUE AND TWO (2) PROPOSED CURB CUTS ONTO E. EDSEL FORD SERVICE DRIVE.
 - ALL WORK IN THE CITY RIGHTS-OF-WAY SHALL BE ACCORDANCE WITH CITY OF DETROIT DPW STANDARDS.
 - STORM WATER SHALL BE DETAINED ON SITE, MEETING THE REQUIREMENTS OF THE CITY OF DETROIT GUIDELINES FOR STORM WATER MANAGEMENT.
 - PARKING AND DRIVE AREAS SHALL BE PAVED WITH BITUMINOUS AND CONCRETE PAVEMENTS.
 - PARKING:
 MULTI-FAMILY DWELLING:
 REQUIRED: 0.75 PARKING SPACES PER DWELLING UNIT.
 COMMERCIAL:
 REQUIRED: 1 SPACE PER 250 SFT. OF GFA. x 0.75 FOR PROXIMITY TO PUBLIC TRANSPORTATION.
- | | |
|----------------------|------------|
| DWELLING UNITS = | 49 UNITS |
| 0.75 * 49 = | 37 SPACES |
| GROSS FLOOR AREA = | 1,469 SFT. |
| 1,469 / 250 * 0.75 = | 5 SPACES |
- 45 SPACES**
- PROVIDED: **TOTAL = 64 SPACES, INCLUDING FIVE (5) BARRIER FREE SPACES.**
- ALL PAVEMENT MARKINGS AND SIGNS SHALL CONFORM TO THE STANDARDS SET FORTH IN THE CURRENT EDITION OF THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
 - PROPOSED SITE LIGHTING SHALL BE A COMBINATION OF PROPOSED WALL & POLE-MOUNTED FIXTURES, DIRECTED TO INHIBIT LIGHTING BEYOND PROPERTY LIMITS, MEETING THE REQUIREMENTS OF APPLICABLE SECTIONS OF THE CITY OF DETROIT ZONING ORDINANCE.
 - LANDSCAPING AND SCREENING SHALL MEET THE INTENT OF THE CITY OF DETROIT ZONING ORDINANCE.

PROPOSED FEATURES LEGEND

- NEW LIGHT DUTY CONCRETE SIDEWALK AREA
- NEW HEAVY DUTY CONCRETE PAVEMENT AREA
- NEW HMA PAVEMENT AREA



PLACE:
 1 EA. - 8"x8"x4" TEE
 1 EA. - 4" VALVE AND BOX
 17 LFT. - 4" D.I.W.M., CL-53 (DOMESTIC)
 1 EA. - 8"x8"x6" TEE
 1 EA. - 6" VALVE AND BOX
 17 LFT. - 6" D.I.W.M., CL-53 (FIRE PROTECTION)

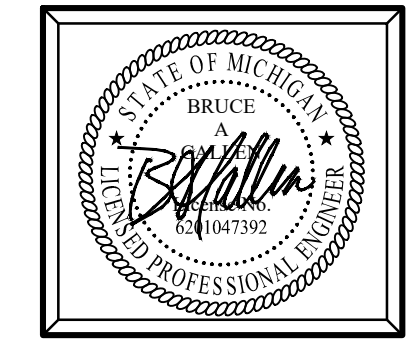
PLACE:
 29 LFT. - 6" SANITARY LATERAL @ 1.00% MIN. w/ CLEANOUT (CONNECT TO EX. SANITARY)

PROPOSED POLE-MOUNTED LIGHT FIXTURE (TYP.)
 (SEE PHOTOMETRIC PLAN FOR DETAIL)

2 EA. - 12' x 35' LOADING ZONE

10' SIDE YARD PARKING SETBACK

EX. HYDRANT

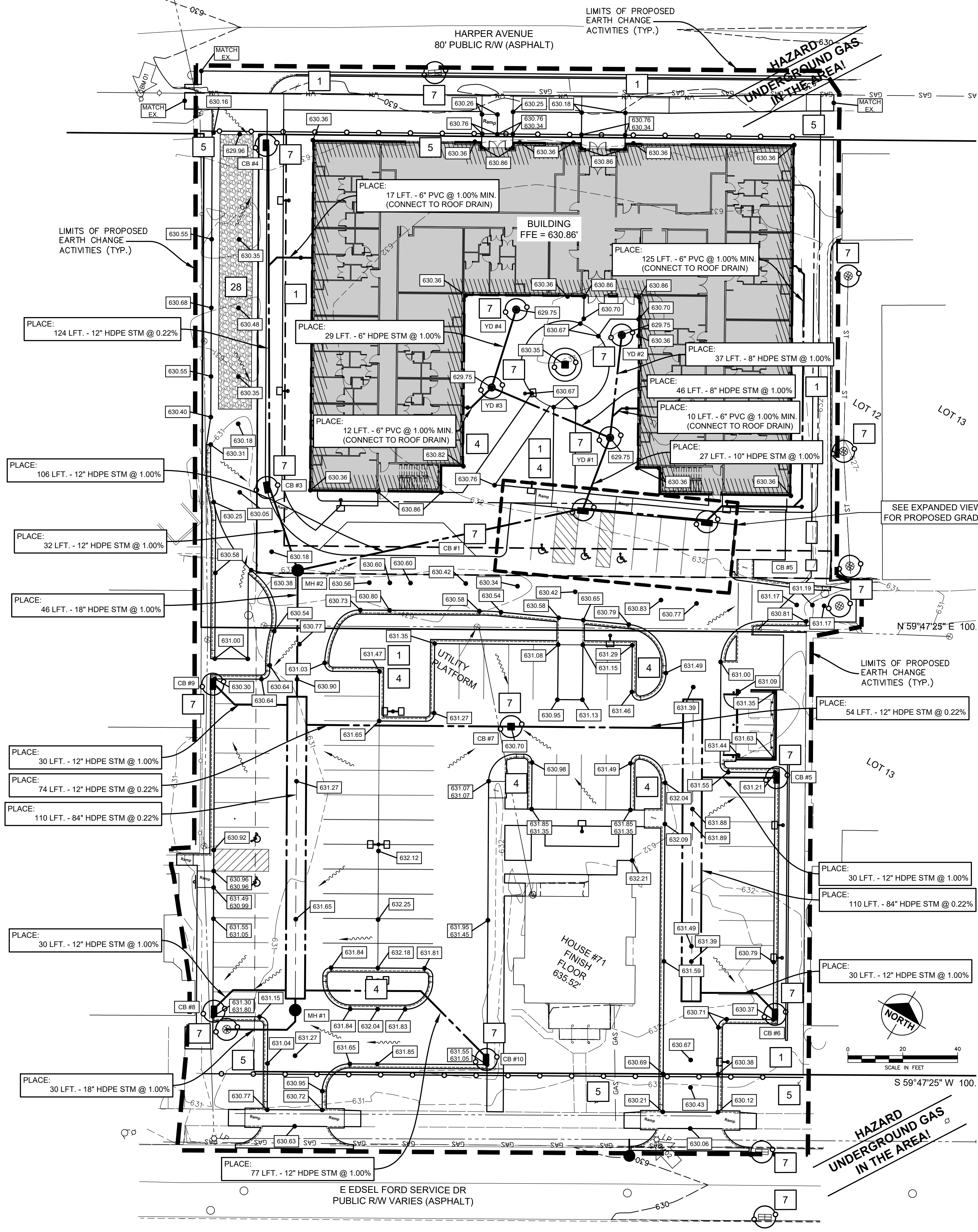


Plan Prepared By:
 Bruce A. Callen, PE
 Callen Engineering, Inc.
 108 E. Savidge St.
 Spring Lake, Michigan 49456
 Tel: 616-414-5260
 email: bcallen@callenengineering.com

For protection of underground utilities, the CONTRACTOR shall dial 1-800-482-7171 OR 811 a minimum of three working days, excluding Saturdays, Sundays and holidays, prior to excavation in the vicinity of utility lines. All "MISS DIG" participating members will thus be routinely notified. This does not relieve the CONTRACTOR of the responsibility of notifying the utility owners who may not be part of the "MISS DIG" alert system.

DATE OF PLAN: 05-05-23

PRELIMINARY - NOT FOR CONSTRUCTION



KEY	SESC MEASURE	SYMBOL	WHERE USED
1	Seeding		When bare soil is exposed, temporarily or permanently, to erosive forces from wind and/or water on flat areas, mild slopes, grassed waterways and spillways, diversion ditches and dikes, borrow and stockpile areas, and spoil piles.
2	Mulch		On flat areas, slopes, grassed waterways and spillways, diversion ditches and dikes, borrow and stockpile areas, and spoil piles when areas are subject to raindrop impact, and erosive forces from wind and/or water.
4	Trees, Shrubs, Vines and Ground Cover		When bare soil or recently vegetated slopes are exposed to erosive forces from wind and/or water.
5	Perimeter Sediment Control Measures (Silt Fence, Straw Wattles, etc.)		As a temporary measure used to capture sediment from sheet flow. May also divert small volumes of sheet flow to protected outlets.
7	Storm Drain Inlet Protection		Around the entrance to a catch basin or an inlet that will capture runoff from an earth change activity.
28	Stone Construction Access		At locations where construction equipment will enter and exit the drain easement and tracking of soil is anticipated.

EROSION CONTROL MEASURES

*SESC MEASURES SHALL COMPLY WITH MICHIGAN ASSOCIATION OF COUNTY DRAIN COMMISSIONERS SESC MANUAL SPECIFICATIONS

STORM STRUCTURE NOTES

STORM SEWER LENGTHS PROVIDED ARE MEASURED FROM CENTER OF STRUCTURE. RIM ELEVATIONS ARE MEASURED AT EDGE OF METAL.

STORM SEWER SHALL BE WATER-TIGHT, SOLID-WALL HDPE OR APPROVED EQUAL, UNLESS INDICATED ON THE PLANS.

MANHOLES AND CATCH BASINS SHALL BE IN ACCORDANCE WITH MDOT STANDARD SPECIFICATIONS. CATCH BASINS SHALL INCLUDE 2-FT SUMP.

ALL EXISTING CATCH BASINS TO REMAIN SHALL BE CLEANED AND INSPECTED TO VERIFY OPERABILITY.

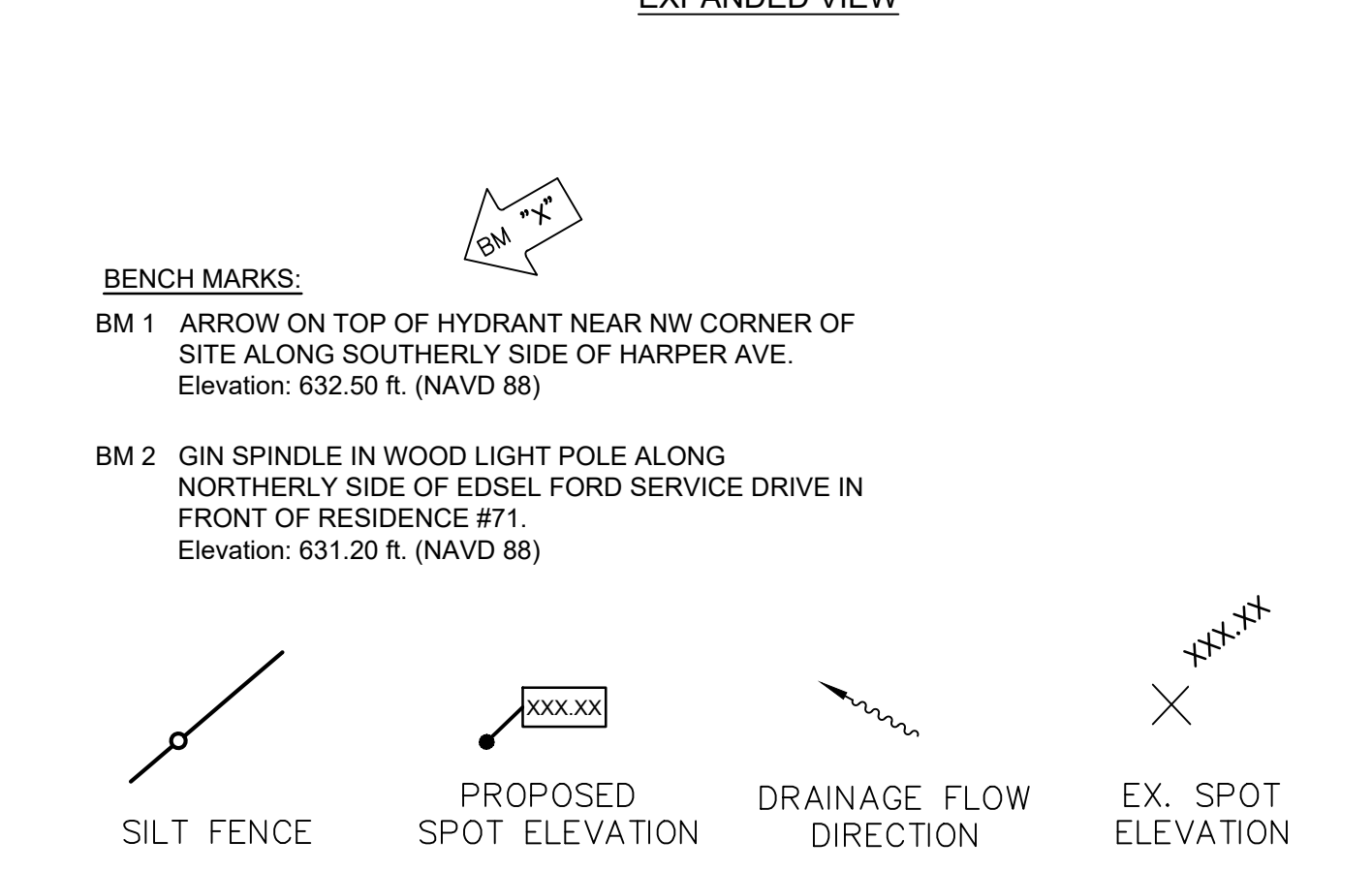
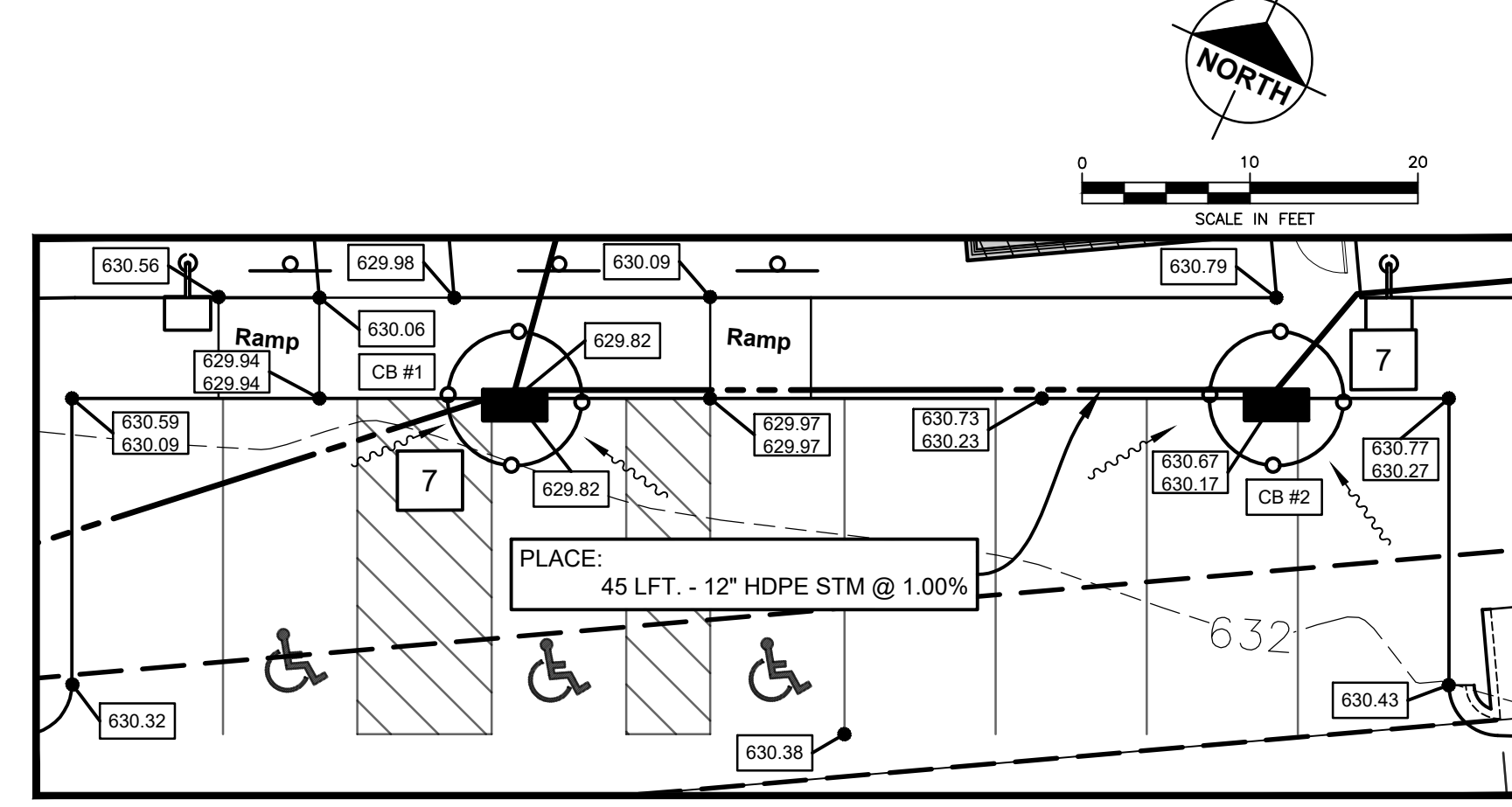
CONNECTIONS TO MANHOLES SHALL BE MADE WITH A RESILIENT CONNECTOR FOR PIPE DIAMETERS 24 INCHES OR LESS.

MANHOLE RIM ELEVATIONS SHALL BE SET 1/4" BELOW PLAN GRADE.

CATCH BASIN RIM ELEVATIONS SHALL BE SET 1/8" BELOW PLAN GRADE.

ALL CURB DIMENSIONS ARE MEASURED FROM EDGE OF METAL.

THE CONTRACTOR SHALL EXPOSE AND VERIFY LOCATION AND DEPTH OF EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. CONFLICTS IN GRADES SHALL BE REPORTED TO ENGINEER AND ADJUSTMENTS SHALL BE MADE AT NO ADDITIONAL COST TO OWNER.



HAZARD UNDERGROUND GAS IN THE AREA!

811 Know what's below. Call before you dig.

Subsurface Conditions

Contractor(s) shall familiarize themselves with the information and findings presented in the Geotechnical Evaluation Report prepared by SME dated Mar. 23, 2023. SME Project No. 092121.00

Contractor(s) shall also familiarize themselves with the information and findings presented in the Phase II Environmental Site Assessment prepared by Hamp, Mathews & Associates, Inc., dated Oct. 03, 2022.

SESC NOTES

TOTAL DISTURBED AREA: 91,234 SFT (2.09 ACRES)

ADDITIONAL EROSION CONTROL MEASURES NOT SHOWN ON THE SITE PLAN MAY BE NECESSARY AS SITE WORK PROGRESSES. PERMITTEE IS RESPONSIBLE FOR ALL MEASURES NECESSARY TO PREVENT OFFSITE SEDIMENTATION.

GEOTEXTILE SILT FENCE AND ALL CATCH BASIN PROTECTION MEASURES SHALL BE PROPERLY PLACED AS SHOWN ON PLANS AND AS NEEDED TO RETAIN SOILS ON-SITE. PERIODIC MAINTENANCE AND INSPECTION OF SESC MEASURES IS REQUIRED FOR PROPER EFFECTIVENESS.

STABILIZE SITE AS SOON AS POSSIBLE.

CONSTRUCTION IS ANTICIPATED TO BEGIN _____ 2023 WITH SUBSTANTIAL SITEWORK COMPLETION NO LATER THAN _____ 2023.

STORM STRUCTURE SCHEDULE (THIS SHEET)

MH #1 (4' DIA W/ PARTITION WALL) RIM = 631.44 FT (COVER: EJIW 1045 Z SERIES W/ SOLID COVER OR A.E.)
18" INV (SE) = 621.40 FT (SOLID WALL HDPE @ 1.00%)
18" INV (NW) = 621.60 FT (SOLID WALL HDPE @ 1.00%)
TOP OF WALL = 628.60 FT
X' ORIFICE = 621.50 FT (BANKFULL / FLOOD CONTROL)
MH#2 (4' DIA) RIM = 630.18 FT (COVER: EJIW 1045Z W/ SOLID COVER OR A.E.)
18" INV (S) = 621.40 FT (SOLID WALL HDPE @ 0.22%)
12" INV (NE) = 625.58 FT (SOLID WALL HDPE @ 0.22%)
12" INV (NW) = 626.12 FT (SOLID WALL HDPE @ 0.22%)
6" INV (N) = 626.72 FT (SOLID WALL HDPE @ 1.00%)
CB#1 (4' DIA) RIM = 629.82 FT (COVER: EJIW 5100 Z W/ ADA GRATE OR A.E.)
12" INV (SW) = 626.62 FT (SOLID WALL HDPE @ 1.00%)
12" INV (NE) = 626.72 FT (SOLID WALL HDPE @ 1.00%)
10" INV (N) = 626.72 FT (SOLID WALL HDPE @ 1.00%)
CB#2 (2' DIA) RIM = 630.17 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (S) = 627.17 FT (SOLID WALL HDPE @ 1.00%)
6" INV (NE) = 627.17 FT (SOLID WALL HDPE @ 1.00%)
CB#3 (4' DIA) RIM = 629.85 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 626.44 FT (SOLID WALL HDPE @ 1.00%)
12" INV (NE) = 626.54 FT (SOLID WALL HDPE @ 0.22%)
6" INV (N) = 626.54 FT (SOLID WALL HDPE @ 1.00%)
CB#4 (2' DIA) RIM = 629.81 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 626.81 FT (SOLID WALL HDPE @ 0.22%)
12" INV (NW) = 626.81 FT (SOLID WALL HDPE @ 0.22%)
CB#5 (2' DIA) RIM = 631.23 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 627.73 FT (SOLID WALL HDPE @ 1.00%)
CB#6 (2' DIA) RIM = 630.70 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 627.20 FT (SOLID WALL HDPE @ 1.00%)
CB#7 (4' DIA) RIM = 630.88 FT (COVER: EJIW 1045 Z W/ M2 GRATE OR A.E.)
12" INV (SW) = 622.28 FT (SOLID WALL HDPE @ 0.22%)
12" INV (NE) = 622.38 FT (SOLID WALL HDPE @ 0.22%)
CB#8 (2' DIA) RIM = 630.82 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 627.32 FT (SOLID WALL HDPE @ 1.00%)
CB#9 (2' DIA) RIM = 630.30 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 627.55 FT (SOLID WALL HDPE @ 1.00%)
CB#10 (2' DIA) RIM = 631.05 FT (COVER: EJIW MDOT R-15 W/ TYPE K COVER OR A.E.)
12" INV (SW) = 627.55 FT (SOLID WALL HDPE @ 1.00%)
YD#1 (2' DIA) RIM = 629.75 FT (COVER: EJIW 1045Z W/ TYPE 02 GRATE OR A.E.)
10" INV (S) = 626.99 FT (HDPE STM @ 1.00%)
8" INV (N) = 627.09 FT (HDPE STM @ 1.00%)
8" INV (W) = 627.09 FT (HDPE STM @ 1.00%)
YD#2 (2' DIA) RIM = 629.75 FT (COVER: EJIW 1045Z W/ TYPE 02 GRATE OR A.E.)
8" INV (S) = 627.46 FT (HDPE STM @ 1.00%)
YD#3 (2' DIA) RIM = 629.75 FT (COVER: EJIW 1045Z W/ TYPE 02 GRATE OR A.E.)
8" INV (SE) = 627.55 FT (HDPE STM @ 1.00%)
6" INV (N) = 627.55 FT (HDPE STM @ 1.00%)
6" INV (SW) = 627.55 FT (HDPE STM @ 1.00%)
YD#4 (2' DIA) RIM = 629.75 FT (COVER: EJIW 1045Z W/ TYPE 02 GRATE OR A.E.)
6" INV (S) = 627.84 FT (HDPE STM @ 1.00%)

Callen Engineering, Inc.
108 East Savidge Street
Spring Lake, MI 49456
T. 616.414.5260
www.callenengineering.com

callen
civil engineers

MHT HOUSING, INC.
32500 TELEGRAPH ROAD, SUITE 100
BINGHAM FARMS, MI 48025
248-833-0552

NO.	DATE	BY	ISSUANCE / REVISION
1	04-14-23	J.W.C.	DATE
2	04-14-23	B.A.C.	DATE
3	04-14-23	B.A.C.	DATE
4	05-05-23	B.A.C.	DATE

60 HARPER APARTMENTS
A MIXED USE DEVELOPMENT
CITY OF DETROIT, MI 48202
WAYNE COUNTY, MICHIGAN
GRADING, DRAINAGE, AND SESC PLAN

Plan Prepared By:
Bruce A. Callen, PE
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108 E. Savidge St.
Spring Lake, Michigan 49456
Tel: 616-414-5260
email: bcallen@callenengineering.com

811 Know what's below. Call before you dig.

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DATE OF PLAN: 05-05-23

PROJECT: 022 60 HARPER
SHEET NO. **C2.0**